

LEG. FINANCE - BILLS 1979 - 1980 1368

CSSB 450am cont. 1368

Original Sponsor: Fahrenkamp

Funding Information

General Fund	\$6,094,500
Other Funds	-0-
	<u>\$6,094,500</u>

1 IN THE SENATE

BY THE FINANCE COMMITTEE

2 HOUSE CS FOR CS FOR SENATE BILL NO. 450

3 IN THE LEGISLATURE OF THE STATE OF ALASKA

4 ELEVENTH LEGISLATURE - SECOND SESSION

5 A BILL

6 For an Act entitled: "An Act making a special appropriation to the Legisla-
7 tive Council for a study by the Special Senate Committee
8 on Transportation of options for local acquisition and
9 ownership or management of the Fairbanks International
10 Airport and the Anchorage International Airport; making
11 special appropriations for grants to the Municipality
12 of Anchorage, the City of Kodiak, and the City and
13 Borough of Juneau; and providing for an effective
14 date."

15 BE IT ENACTED BY THE LEGISLATURE OF THE STATE OF ALASKA:

16 * Section 1. The sum of \$175,000 is appropriated from the general fund to
17 the Legislative Council for funding the Special Senate Committee on Trans-
18 portation for a study of options for local acquisition and ownership or
19 management of the Fairbanks International Airport and the Anchorage Interna-
20 tional Airport.

21 * Sec. 2. The sum of \$519,500 is appropriated from the general fund for
22 payment as a grant to the Municipality of Anchorage for reimbursement of
23 extraordinary expenses incurred in repairing damages of the April 2, 1980,
24 wind storm.

25 * Sec. 3. The sum of \$3,500,000 is appropriated from the general fund for
26 payment as a grant to the City of Kodiak to repair damage to the city dock
27 resulting from the January 20, 1980, wind storm.

28 * Sec. 4. The sum of \$1,900,000 is appropriated from the general fund for
29 payment as a grant to the City and Borough of Juneau to repair damage to the

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Auke Bay School.

* Sec. 5. The unexpended and unobligated portion of the appropriation made in sec. 1 of this Act lapses into the general fund June 30, 1981.

* Sec. 6. This Act takes effect immediately in accordance with AS 01.10.-070(c).

Memo

To: Rep. Russ Meekins

From: Marcy Rehfeld

Subject: Back-up on Anchorage Wind Damage Monies

I called Anchorage on 5/26/80 and talked to Mr. John Franklin, the cities Fire Chief. He was able to give me the total break down of the monies spent by the Municipality of Anchorage in repairs after the April 2, 1980 wind strom.

Of the total \$519,476 spent, \$260,000 was spent on repair to local schools, especially Service High School, which had extensive roof damage. Several area playgrounds also had extensive damage.

The \$259,476 spent on Municipality damages are divided into three categories:

1. Streets, roads, and bridges - \$67,304
This was for restoration, repair and removal of debris.
2. Public buildings and facilities - \$178,347
This was for restoration and repair of public facilities such as parks, bus sheds, utility transmission poles and etc.
3. Other - \$13,825
This includes repair to emergency vechicles, as well as call back duty pay for city employees.

REPORT OF DAMAGE

On January 20, 1980, a storm hit St. Paul harbor with south to northwest winds in excess of 53 miles per hour and seas with estimated eight foot swells. The storm which lasted for less than 24 hours did considerable damage, especially at high tide. Approximately 8,000 cubic yards of fill material and 2,300 cubic yards of armor rock was washed away from the City Dock (Pier II) and from the Container Shipping Facility (Pier III). More than 600 cubic yards of the washed out fill material came from behind concrete retaining walls at the east and west end of Pier III.

Damage at Pier III was not limited to erosion. An 8-inch waterline separated when fill material supporting it washed away. Approximately 485 feet of underground electrical cable, 44 power risers, and one light pole were also undermined and damaged by the pounding seas.

The cost of repairs at Pier II and Pier III is estimated at between \$200,000.00 and \$350,000.00. Most observers agreed that this was the worst storm damage they had seen since Piers II and III were constructed.

Attached are preliminary cost estimates for repair of the damage, and three alternative plans for repairing the damage.

John B. Stafford

I. REPAIR OF STORM DAMAGE:

1.	Classified and unclassified fill	8,000 c.y. @ \$6.50	52,000.00
2.	Armor rock	2,325 c.y. @ \$35.00	81,375.00
3.	Electrical repairs, including 425 l.f. of elect. wire and conduit, 40 + power risers, light pole, and associated fittings, and approximately 75 c.y. of excavation and backfill	Lump Sum	35,000.00
4.	8" waterline repair (By Force Account)	Lump Sum	<u>1,260.00</u>
	TOTAL -----		169,635.00
	SAY -----		\$ 200,000.00

II. REPAIR OF STORM RELATED DAMAGE:

1.	Reconstruct east wing wall, Pier 3 (Displaced approximately 1-foot seaward during storm)	Estimate	140,000.00
2.	Weld steel straps across loose panels on west wing wall		<u>5,000.00</u>
	TOTAL -----		145,000.00
	SAY -----		\$ 150,000.00

III. CORRECT DEFICIENCIES IN PIER 3 RETAINING WALLS AND BULKHEAD:

1.	Design & construct a new retaining wall and bulkhead to withstand anticipated seismic as well as static loads	Estimate	\$1,000,000.00
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ALTERNATIVE PLANS

I. REPAIR STORM DAMAGE, ONLY:

1. Have Ecklund survey position of East wing wall.
2. Locate source for armor rock.
3. Request proposals for earthwork.
4. Award contract for earthwork.
 When earthwork contract is underway---
5. Request proposals for electrical repairs.
6. Award contract for electrical repairs.

II. REPAIR STORM DAMAGE AND STORM RELATED DAMAGE:

1. Have Ecklund survey position of East wing wall.
2. Hire a consulting firm to design new East wing wall.
3. Locate source for armor rock.
4. Request proposals for earthwork and strap welding.
5. Award contract for earthwork and strap welding.
 When earthwork contract is underway---
6. Request proposals for electrical repairs.
7. Award contract for electrical repairs.
 When East wing wall design is completed---
8. Request proposals for construction of East wing wall.
9. Award contract for East wing wall.

III. REPAIR STORM DAMAGE AND CORRECT DEFICIENCIES IN PIER III RETAINING WALLS & BULKHEAD :

1. Have Ecklund survey position of East wing wall.
2. Hire a consultant to design new wing walls and bulkhead.
3. Locate source for armor rock.
4. Request proposals for earthwork.
5. Award contract for earthwork.
 When the earthwork contract is underway---
6. Request proposals for electrical repairs.
7. Award contract for electrical repairs.
 When design of new retaining walls and bulkhead is finished---
8. Request proposals for construction of new retaining walls and bulkhead.
9. Award contract for new retaining walls and bulkhead.

COST FIGURES SUPPLIED BY

CITY OF KODIAK - FEB. 26, 1980

PIER II & III STORM DAMAGE AND REPAIR

- * 1) Furnish and place open graded, pit run shot rock including removal and disposal of damaged electrical facilities.

8000 cu. yd. @ 7.50/cu. yd. 60,000

- * 2) Furnish and place armor rock

2300 cu. yd. @ \$30/cu. yd. 69,000

- 3) Electrical Reparis: includes installation of one each:

Light pole with yd. lights. Furnish and install 22 power risers with associated junction boxes and panels and underground wiring.

70,600

- 4) Furnish and place classified fill (type 1-A)

300 cu. yd. @ 36.67/cu. yd. 11,000

- 5) Furnish and install wheel stops (12' x 12' treated timbers).

44 each @ \$200.00 8,800

SUBTOTAL 219,400

inspection

2% +

4,500

Administration 1% +

2,200

Contingencies 10% +

22,000

TOTAL

248,100

- * that work is contracted and underway

DESCRIPTION PIER III STORM DAMAGE REHABILITATION

1) Excavation behind bulkhead 17,000 cu. yds. @ \$7/yd. (Plan to dump near West Rear for ad- ditional staging area)	\$119,000
2) Removing bulkhead 56 piles and concrete panels	39,000
3) Armor rock placement on newly exposed slope 3200 cu. yd. @ \$40/cu. yd.	126,000
4) Imprc Ease wing slope stability	250,000
5) Utility relocation	
a) waterline 450 line ft. \$40/ft.	18,000
b) electrical modification lighting and crane power source	15,000
6) Wharf structure 50 ft. 25,340 sq. ft. @ 100/sq ft	<u>2,534,000</u>
SUBTOTAL	3,101,000
7% engineering and administration	217,000
15% contingency	<u>497,500</u>
TOTAL	3,815,500
Pier II & III storm damage repair	248,100 -
GRAND TOTAL	<u>4,063,600</u>

ROBERT V. KYCEK, P.E.
CONSULTING ENGINEER

XXXXXX

XXXXXX
XXXXXX
XXXXXX

1803 W. Sixth Street
Aberdeen, Washington 98520
(Tel. 206-533-0326)

CIVIL AND STRUCTURAL ENGINEERING
DESIGNS, REPORTS, AND INSPECTIONS
PORT DEVELOPMENT AND MARINE STRUCTURES

February 23, 1980

Mr. Clair Harmony
City Manager
City of Kodiak
P.O. Box 1397
Kodiak, Alaska 99616



Subject: Kodiak Container Wharf (Pier 3)

Dear Mr. Harmony:

A visit to the Kodiak Container Wharf was made on the 25th and 26th of January, 1980 for the purpose of observing damage to the facility which occurred as the result of storm-driven ocean swells on January 20, 1980. Observations of the damage are reported herein.

The attached series of photographs illustrate the resultant storm damage. Damage was most severe along the easterly portion of the container storage yard where several thousand cubic yards of rockfill and armor rock were washed away with corresponding operational loss of 35 to 40 parking stalls for container vans. Replacement of rockfill, armor rock, and repair of electrical and water service was estimated in the order of \$170 - 200,000.

Emergency repair had begun during my visit to the site with several truck-loads of rockfill dumped into the more severely eroded areas (Photo No. 11). The method of repair proposed by the City Engineer (Contract Specifications for Pier 2 & 3 Storm Damage Emergency Repair) was reviewed and found appropriate to bring the container storage area back into operational use.

Not included in the contract for emergency repair is the replacing, reconstructing, or reinforcing storm loosened concrete bulkhead panels.

Inspection of the concrete paneled soldier pile bulkhead revealed continued distress aggravated by bulkhead height and by storm waves. Washout of fill material from behind the bulkhead at the wingwalls, at loose panels, and at sinkholes behind the wharf continue to inhibit full operational useage of the facility. Settlement of bulkhead piles in excess of one-foot below plan elevation along the easterly one-half of the wharf, broken tie-rod ends, a rotated and cracked concrete bulkhead anchor (deadman), and translation and kickout of bulkhead piles leaves little doubt that a creeping structural failure has occurred.

Temporary repair work on the bulkhead must continue where piles have translated or moved sufficiently to endanger loss of supported concrete wall panels (Photo No. 18). Repair should be in the manner established and shown in Photo No. 16 wherein tension straps maintain both the relative positions of adjacent piling and act as bearing plates for the wall panels.

Consideration must further be given to long-term correction and solution of bulkhead deficiencies and problems through either constructing a replacement bulkhead or by elimination of the bulkhead completely. A reconstructed or new bulkhead with free height to 25-feet would not, however, ease other problems related to fill slope stability. (Fill slopes are now but marginally stable under static, or non-seismic conditions.) Elimination of the bulkhead in its entirety through excavation of the retained fill and replacement of lost operational area with approximately a 56-foot shoreward extension of the existing concrete-decked wharf structure, together with placement of a toe-berm and/or slope flattening of certain of the fill slopes adjacent to the wharf structure, would provide the stable work platform necessary for continued cargo and container handling operations.


The cost of removing the bulkhead, constructing the wharf widening, and improving adjacent fill slopes is estimated at \$3.6 MM, including a nominal 16% for contingencies, as follows:

Earthwork and bulkhead removal	\$ 614,000
Wharf widening and utility relocation	<u>2,952,000</u>
Total	\$3,566,000

It is recommended that full consideration be given to removing the bulkhead and widening of the wharf structure as described.

Please call me if you should have any question.

Very Truly Yours,


Robert V. Kycek, P.E.

Attach: Appendix A, Photographs.
cc. Mr. M. Sharon/ Anchorage

RVK/rvk

APPENDIX A

1. Photographs of Storm Damage.
(Photos dated January 26, 1980)

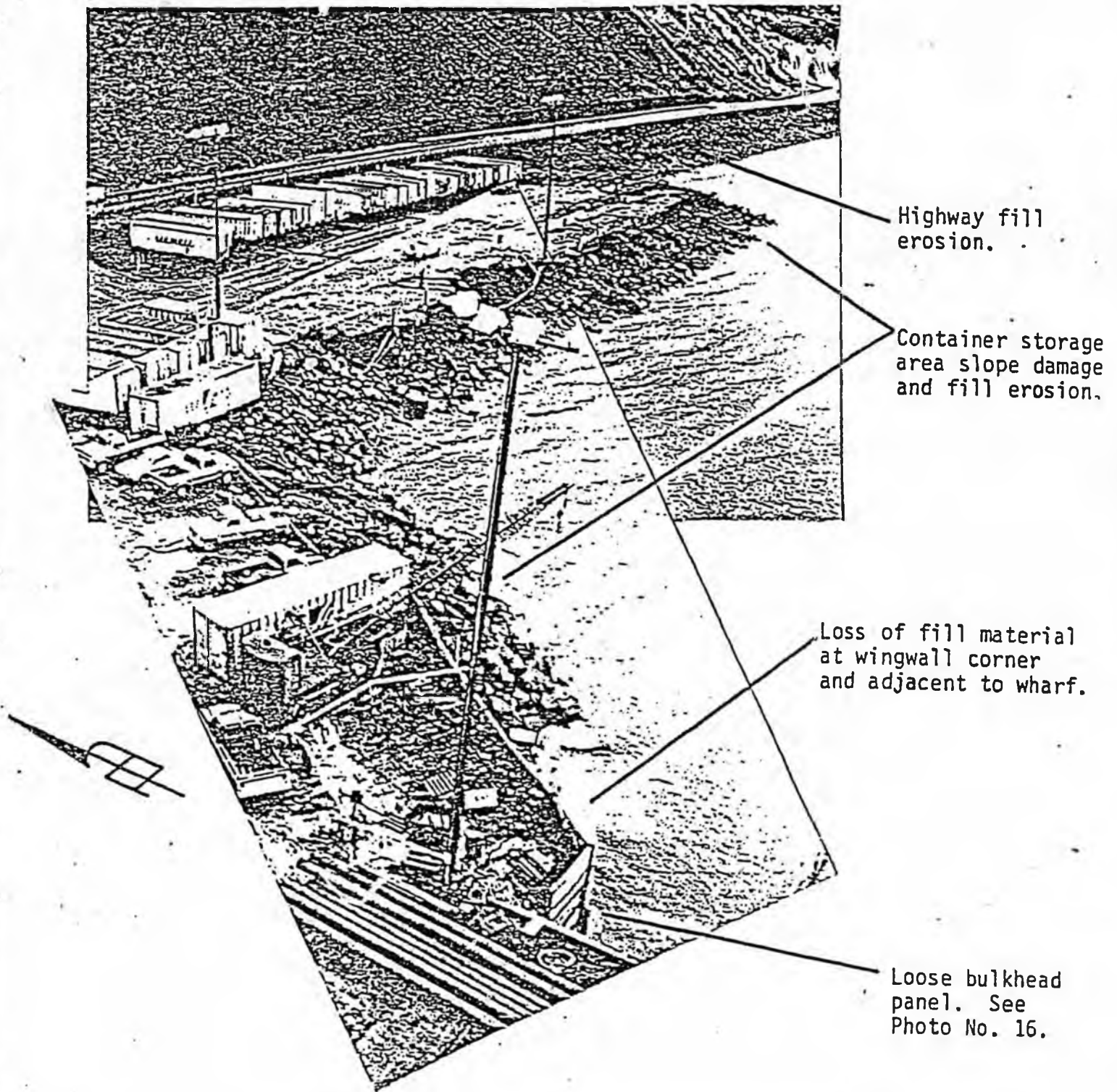


Photo No. 1: Overhead view of the easterly wingwall and of the easterly container storage area. Deterioration by storm wave erosion of the bankline along the container storage area is clearly visible as is erosion of the adjoining highway fill.

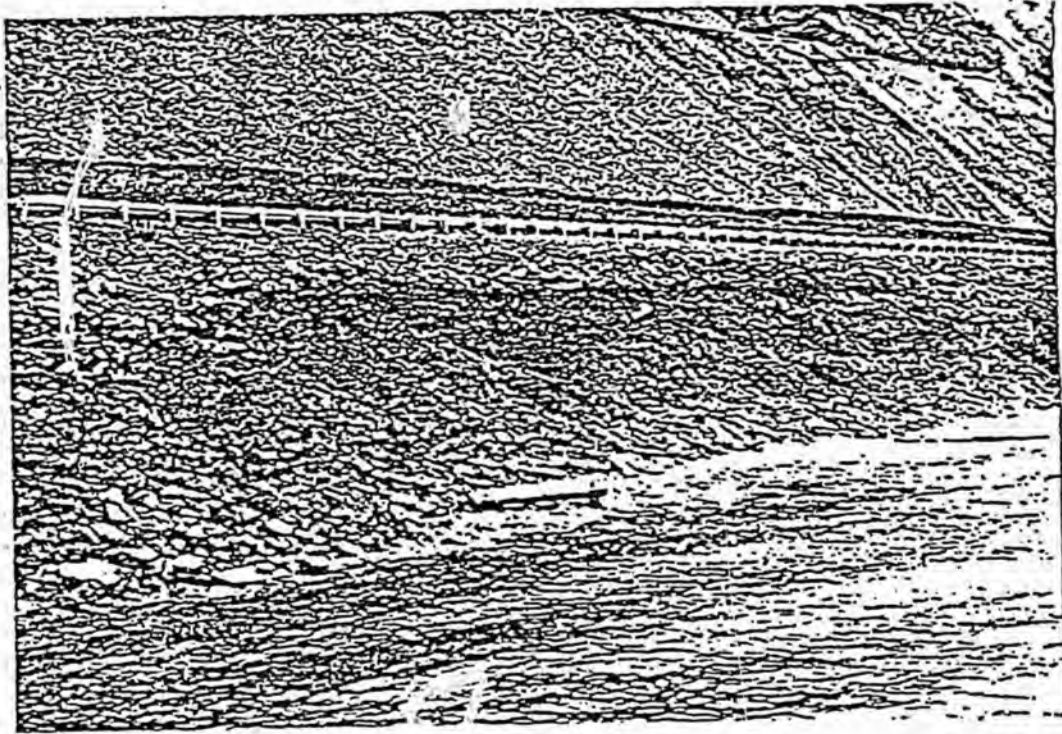


Photo No. 2: Erosion of highway fill adjacent to container storage area.

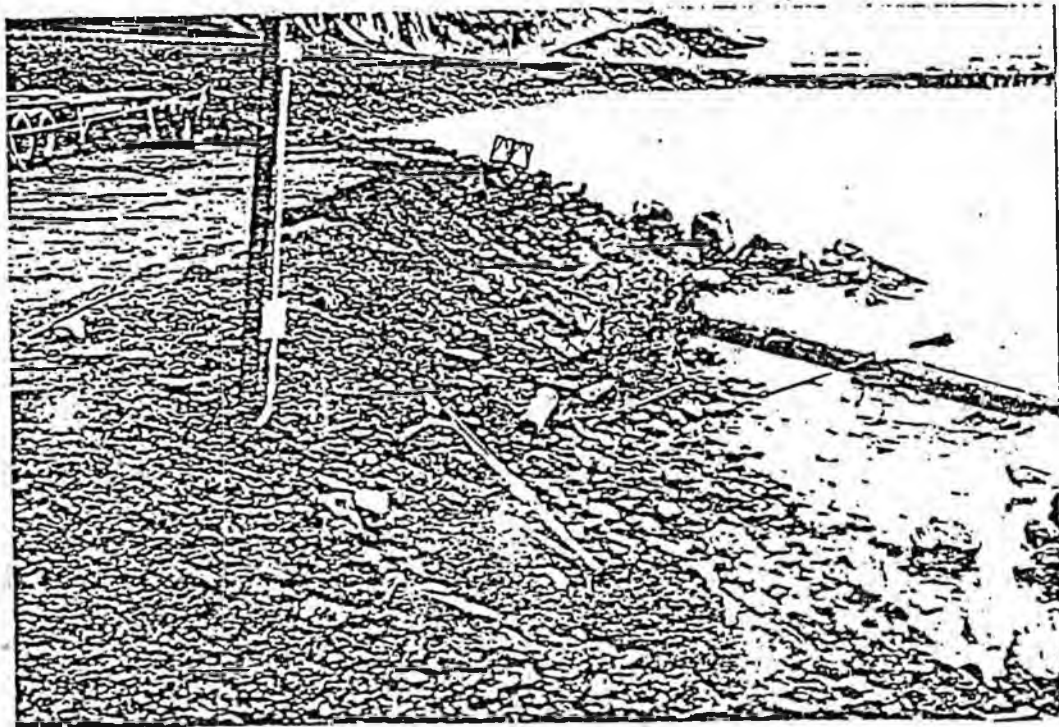


Photo No. 3: View of easterly corner of container storage area. Fallen lightpole visible at right of photo. Note location of concrete curbing relative to eroded bankline.

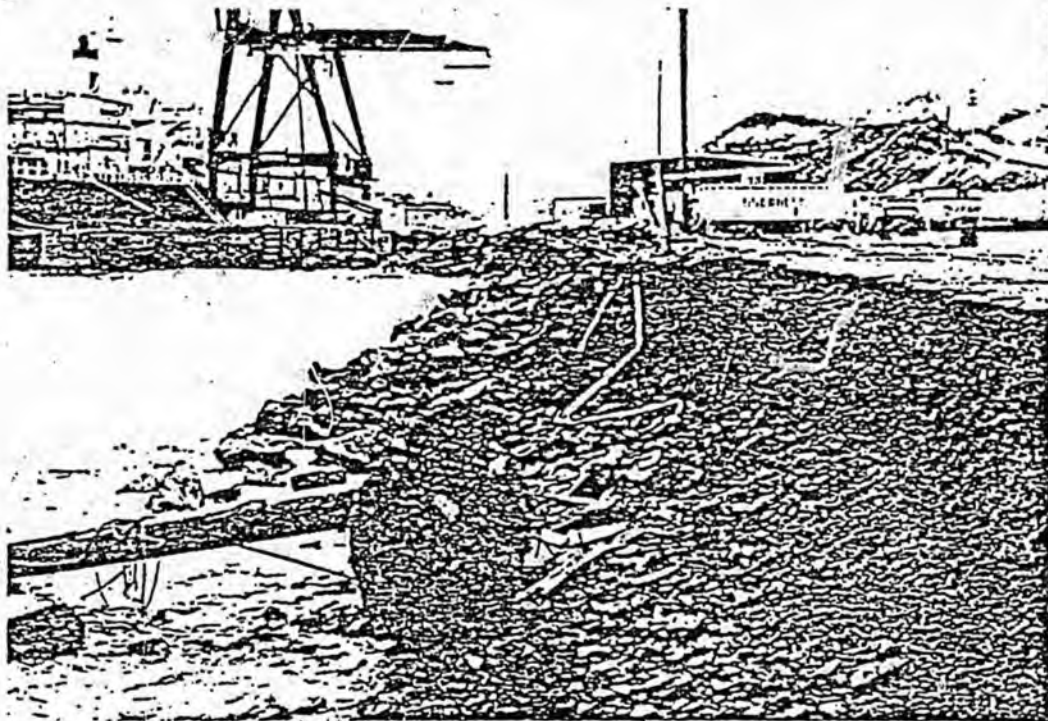


Photo No. 4: View from east end of container storage area looking west. Butt of lightpole visible in center of photo would indicate relatively little initial installation penetration.

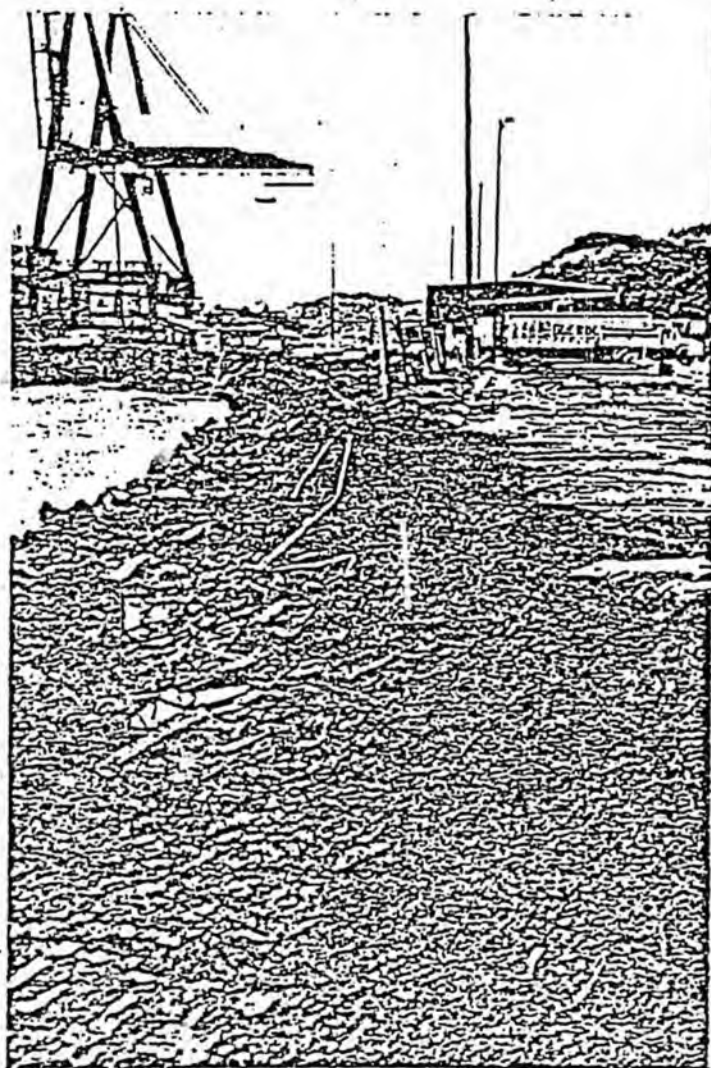


Photo No. 5: View as per Photo No. 4.



Photo No. 6: View looking west from east-central portion of container storage area. Lean of electrical receptacle posts initially caused by contact with parked container vans.

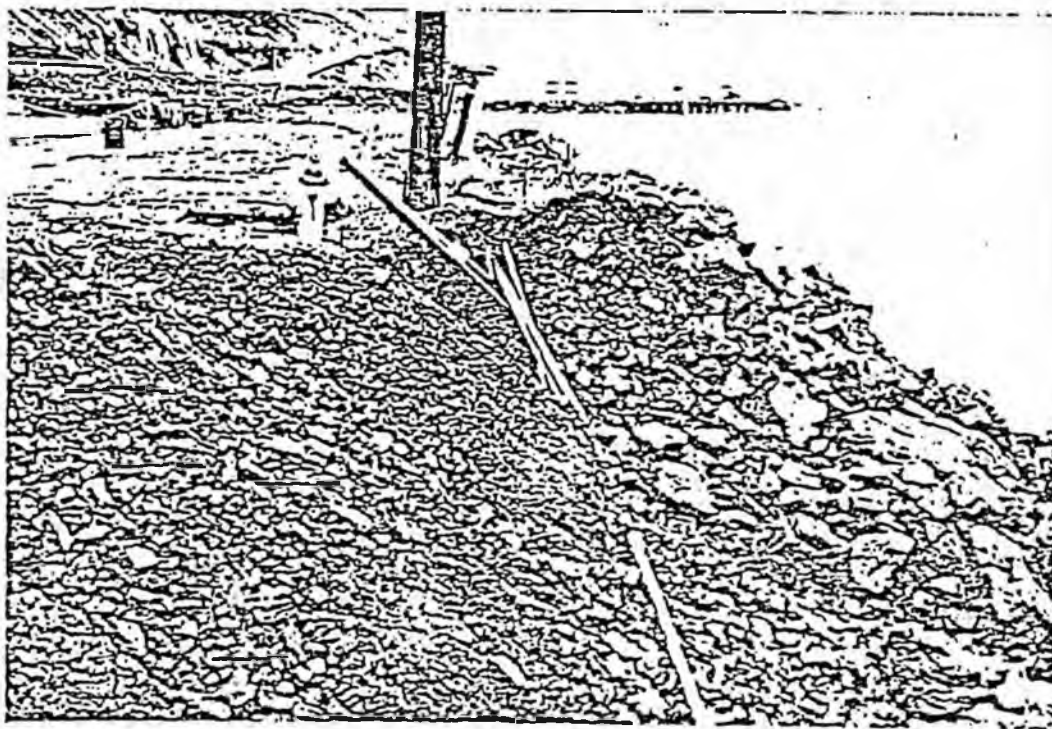


Photo No. 7: View looking east from west-central portion of container storage area. Fill in foreground placed by City of Kodiak.

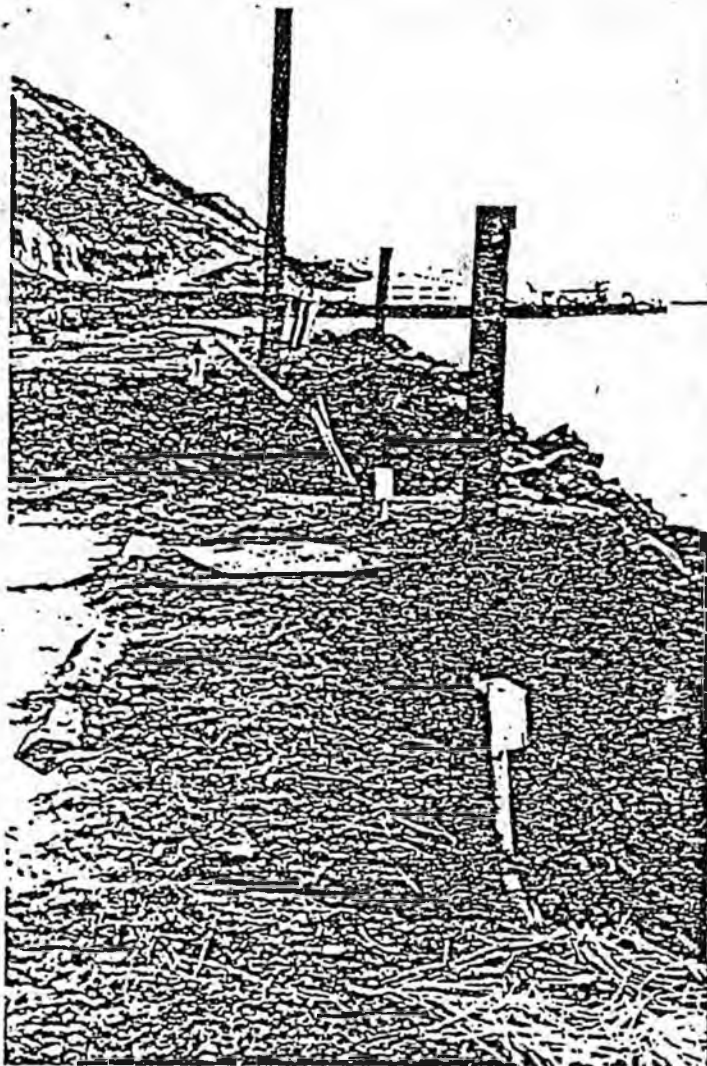


Photo No. 8: View looking east from west-central portion of container storage area.

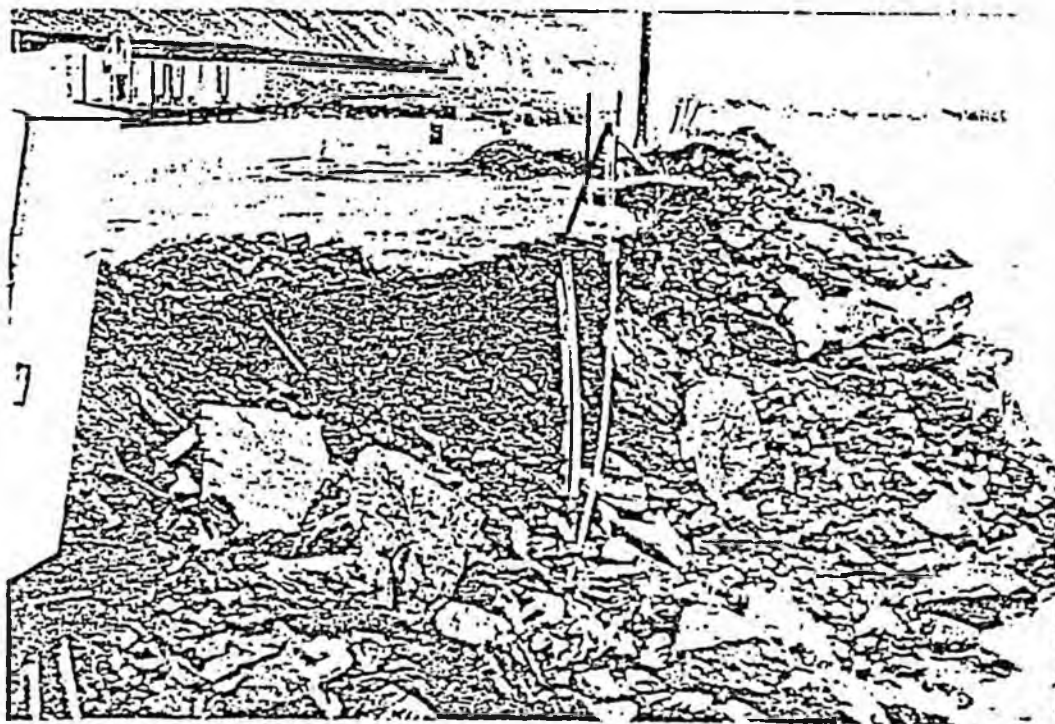


Photo No. 9: View as per Photo No. 8 above. Large riprap rock displaced downslope.

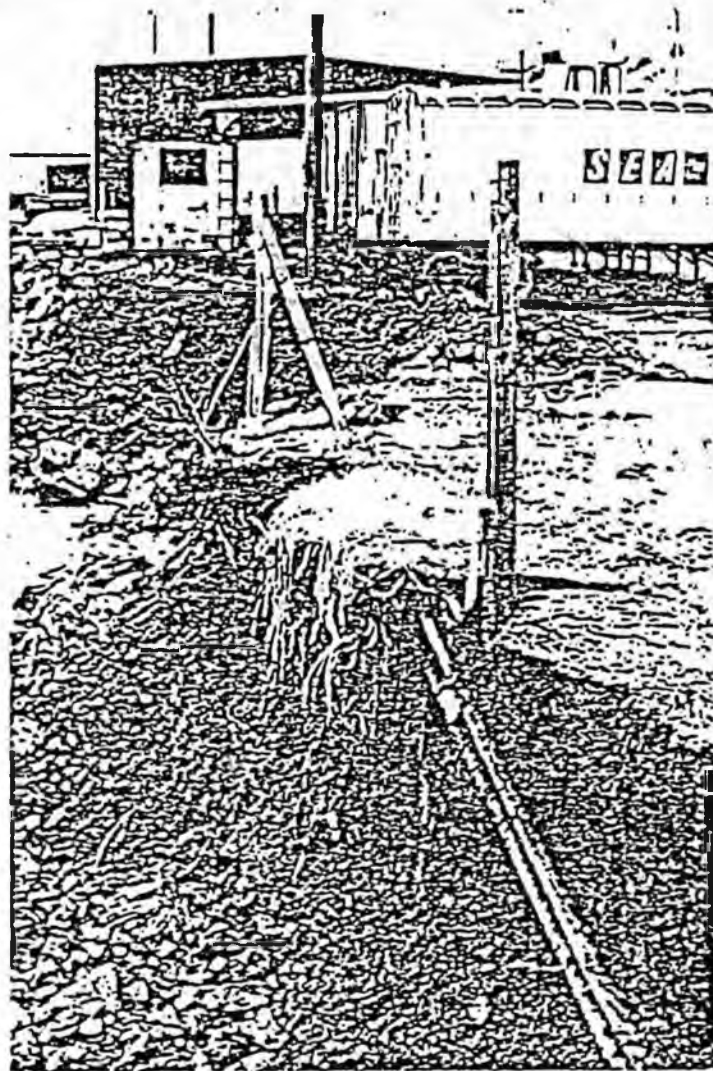


Photo No. 10: View looking west toward intersection of wingwall fill and container storage area. Waterline leak in vicinity of nearest container van in upper right of photo, not visible.



Photo No. 11: View as per Photo No. 10 above. Emergency rock fill being placed to prevent further spalling of bank.

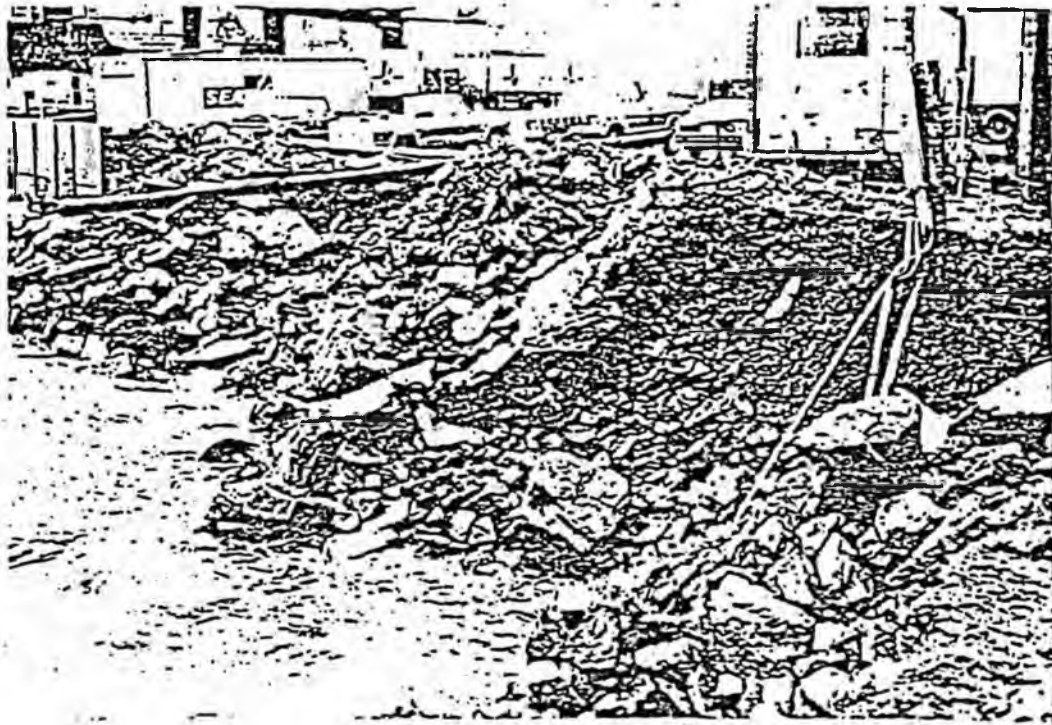


Photo No. 12: Slope erosion northerly of east wingwall. Note location of creosoted piling originally stored on top of bank.

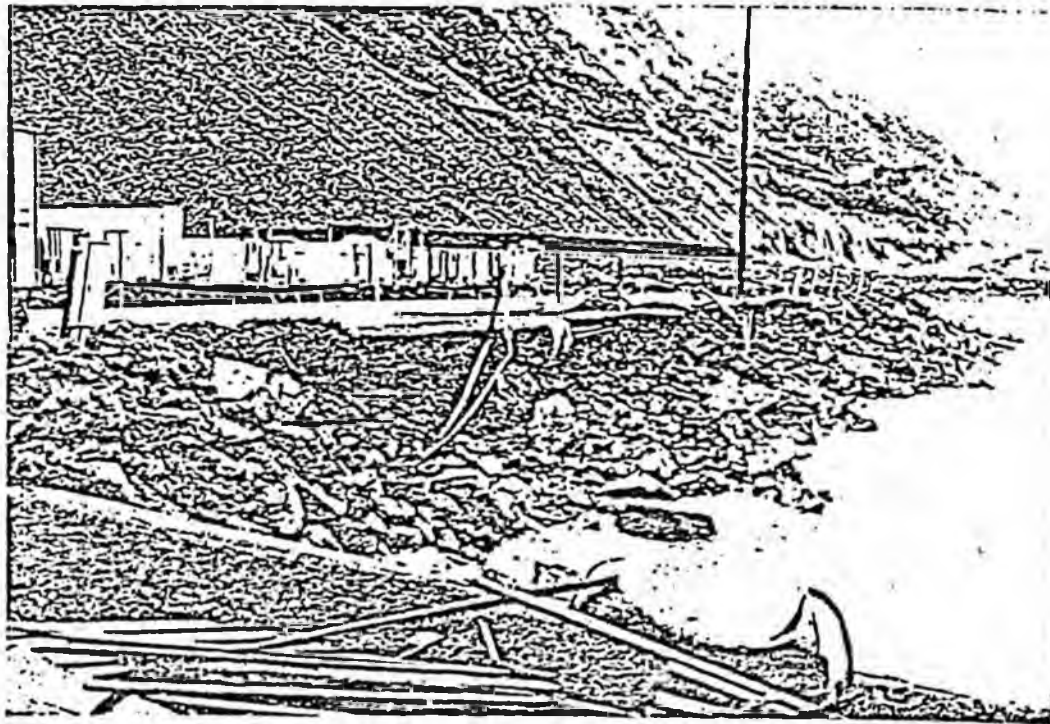


Photo No. 13: Overall view looking east of slope erosion along container storage area.



Photo No. 14: View looking northeasterly along top of east wingwall. Note irregular alignment of wall top.



Photo No. 15: View at corner pile, inside of east wingwall. Due to broken tierods, pile is retained in place by buried steel tension beam installed by City of Kodiak. Loss of fill material caused by wave action through loose panel below.



Photo No. 14: View looking northeasterly along top of east wingwall. Note irregular alignment of wall top.



Photo No. 15: View at corner pile, inside of east wingwall. Due to broken tierods, pile is retained in place by buried steel tension beam installed by City of Kodiak. Loss of fill material caused by wave action through loose panel below.

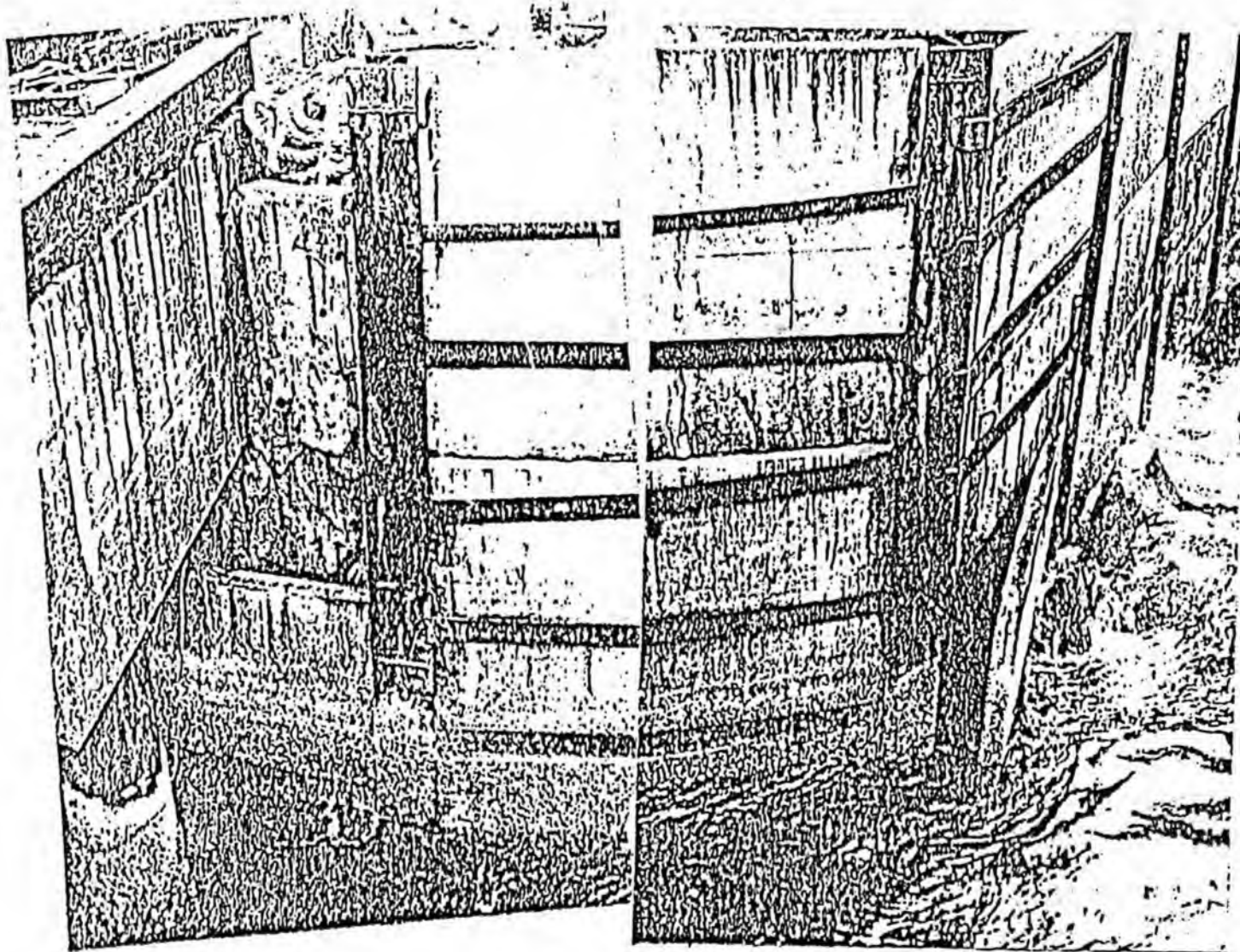


Photo No. 16: View of east end of main wharf bulkhead at junction with east wingwall
Movement of soldier pile adjacent to wharf structure (at left of photo) has broken
cast-in-place concrete panel section. Steel straps keep soldier piles from spreading.
Steel tierrod ends are broken, first three soldier piles.

CORRECTION

**THIS DOCUMENT
HAS BEEN REPHOTOGRAPHED
TO ASSURE LEGIBILITY**

CORRECTION

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TO ASSURE LEGIBILITY**

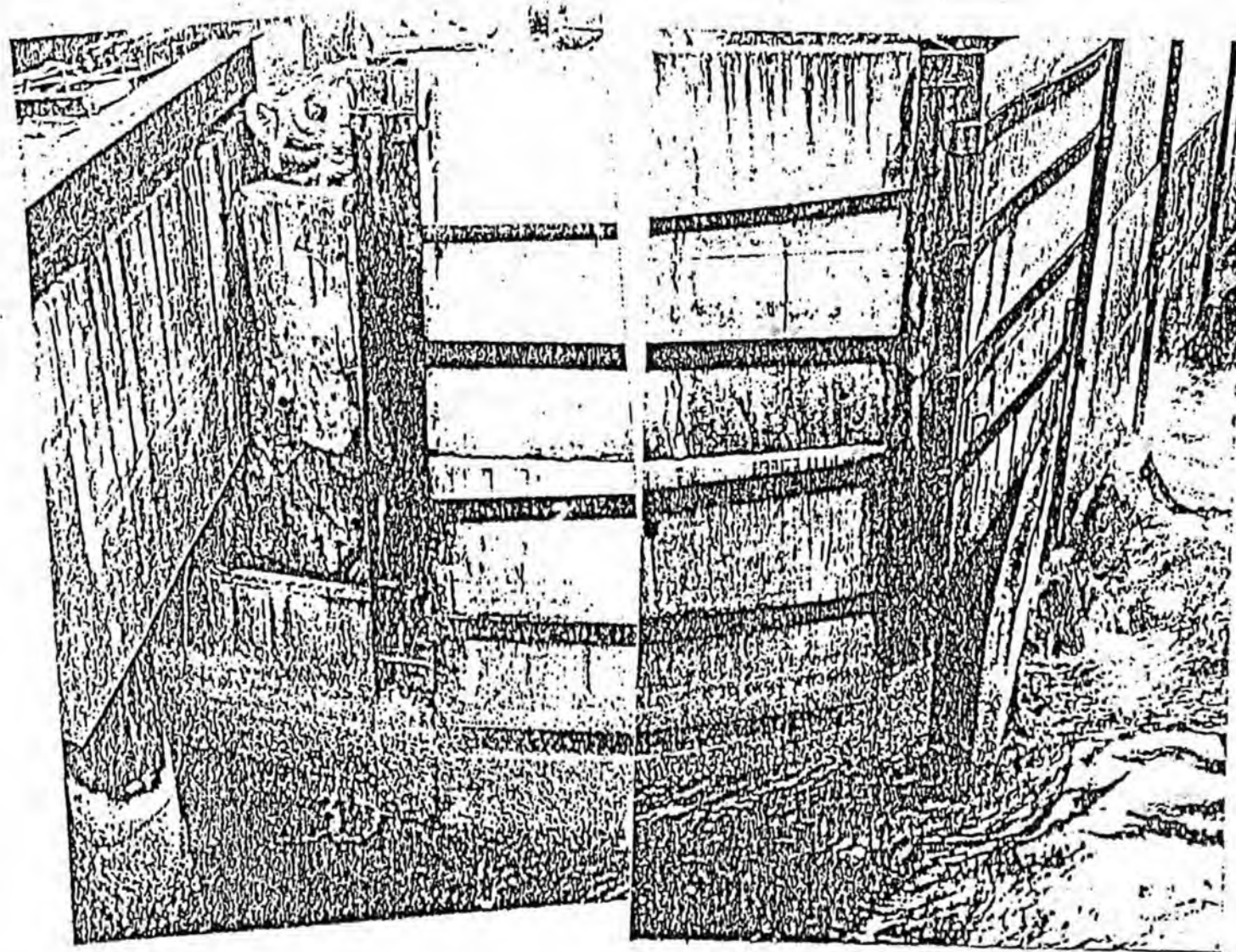


Photo No. 16: View of east end of main wharf bulkhead at junction with east wingwall. Movement of soldier pile adjacent to wharf structure (at left of photo) has broken cast-in-place concrete panel section. Steel straps keep soldier piles from spreading. Steel tierod ends are broken, first three soldier piles.

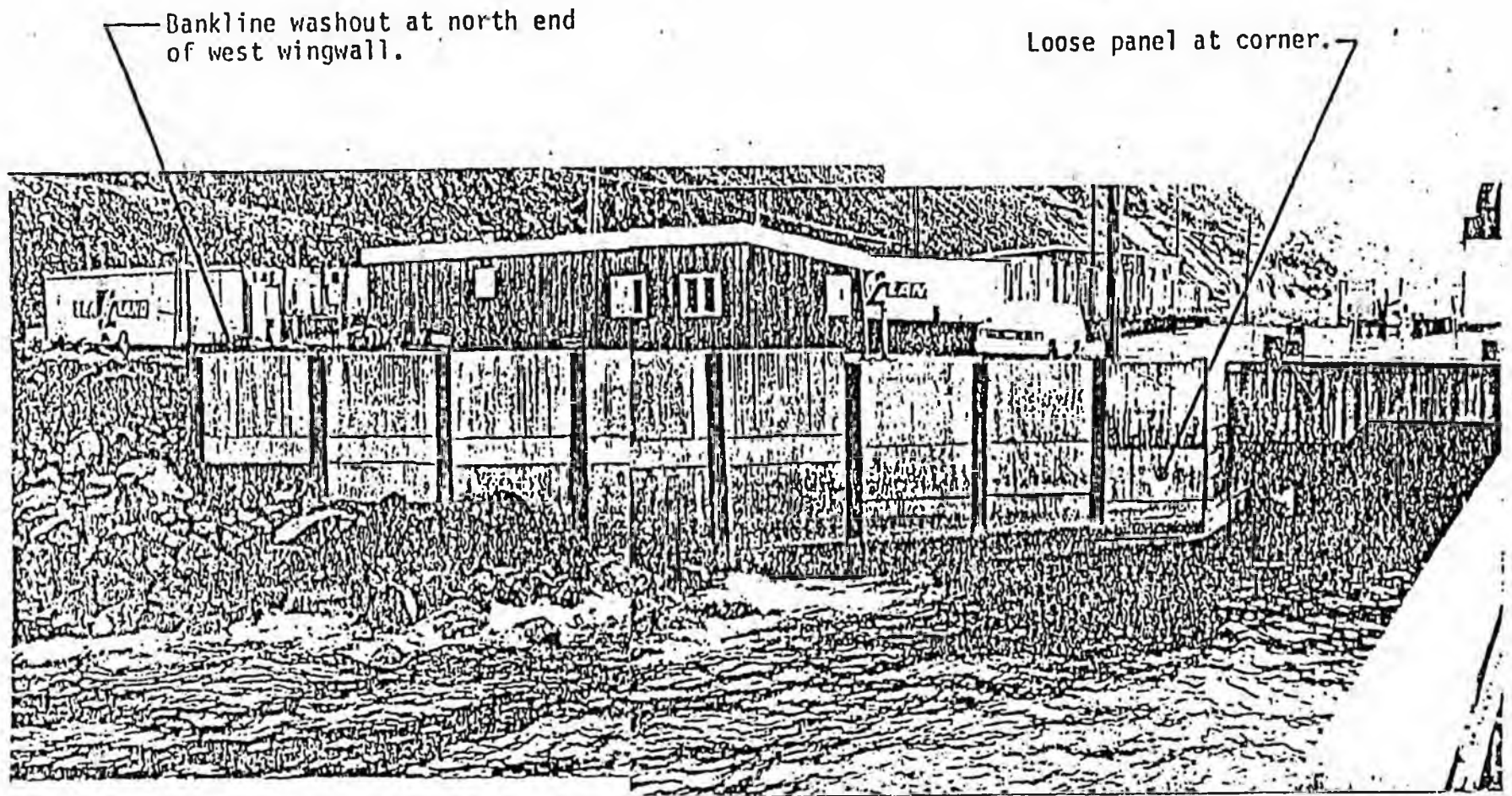


Photo No. 17: Overall view of west wingwall. Storm damage relatively minor in comparison to east container storage area and east wingwall.

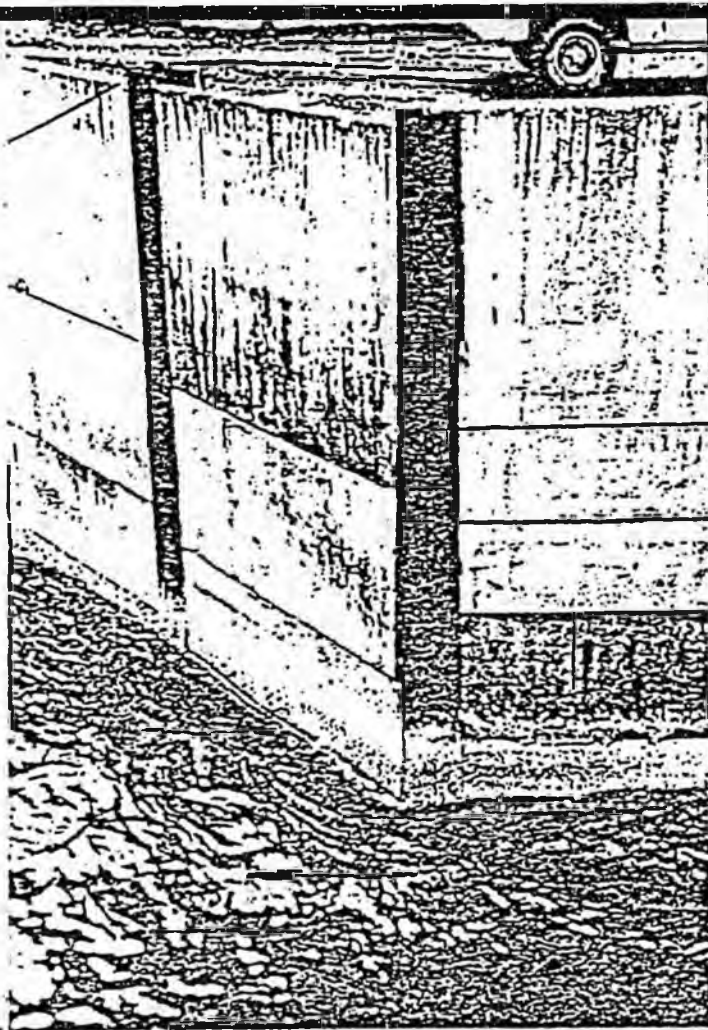


Photo No. 18: View of west wingwall corner. Concrete panel framing into flange of corner soldier pile in danger of falling out in fashion similar to that shown in Photos No. 14 and 16.



Photo No. 19: View of northern end of west wingwall looking north. Fill washed out from behind wall by storm waves.

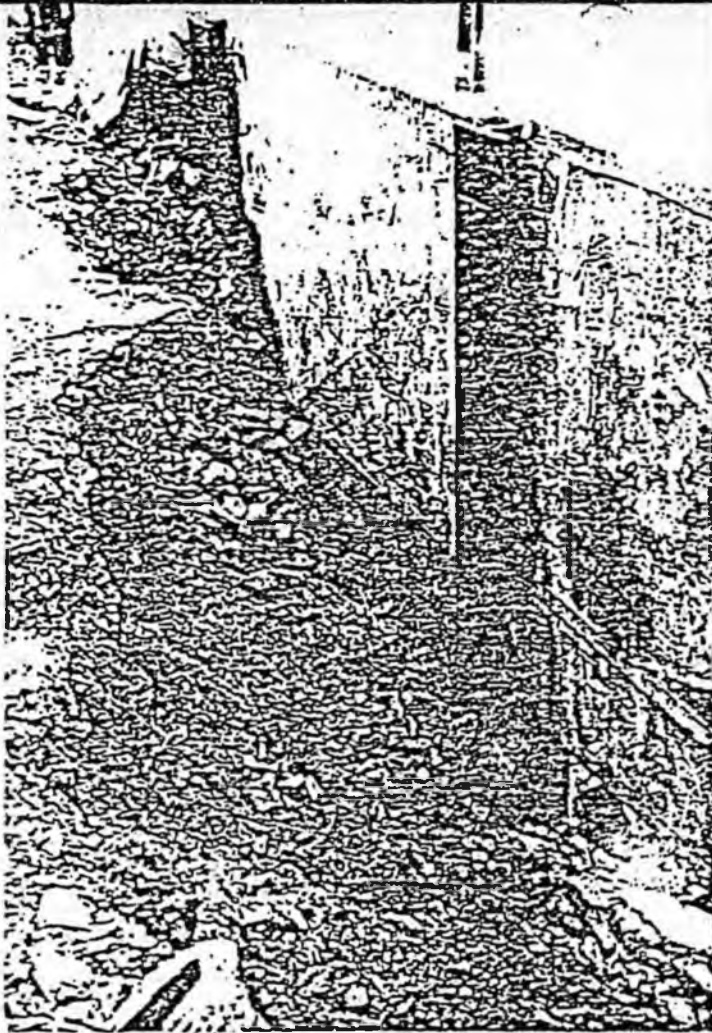


Photo No. 20: View of north end of west wingwall looking south. Eroded fill is daylighted under concrete panel.



Photo No. 21: Sinkhole in rockfill in back of wharf. Typical three places along easterly third of wharf.



STATE OF ALASKA
OFFICE OF THE GOVERNOR
JUNEAU

DECLARATION OF DISASTER EMERGENCY

WHEREAS, on the 20th day of January, 1980, an intense winter storm generating high winds with velocities in excess of 50 miles per hour and heavy sea waves coupled with high tides resulted in moderate to severe damages in the Kodiak Island Borough; and

WHEREAS, as a result of the storm, extensive damage was done to private and public properties; and

WHEREAS, resources of the local governments are inadequate to recover from the disaster effects which have caused damages to Kodiak City Port facilities, City of Port Lions dock, ramp and boat harbor facilities, City of Ouzinkie damage to boardwalks adjacent to the harbor, City of Old Harbor and Akhiok sustained extensive shoreline erosion to riprap, the City of Karluk, and the City of Akhiok sustained extensive damage to private housing, e.g. roofing, etc.

NOW THEREFORE, this 5th day of February, 1980, I have determined that the damages to the Kodiak Island Borough communities resulting from the storm of January 20, 1980, are of sufficient severity and magnitude to warrant a disaster declaration under Alaska Statutes, Section 26.23.020, and I hereby declare that such a Disaster Emergency exists in order to provide State assistance.

FURTHER, the Director, Alaska Division of Emergency Services, is hereby authorized to allocate from funds available for these purposes such amounts as considered necessary for State individual and public disaster assistance and for necessary administrative and disaster management expenses. The Director, Alaska Division of Emergency Services, is further authorized to task, as necessary, State departments and agencies to provide assistance to cope with the disaster effects as tasked in the State Emergency Plans.

BY


Jay S. Hammond, Governor

16

STATE OF ALASKA
OFFICE OF THE GOVERNOR
JUNEAU

February 5, 1980

The President
The White House
Washington, D.C. 20500

Through: Mr. Neal Chaney
Director, Region 10
Federal Emergency Management
Agency
Federal Regional Center
Bothell, Washington 98011

Dear Mr. President:

I respectfully request that you declare that a major disaster exists in the State of Alaska under the provisions of the Disaster Relief Act of 1974, Public Law 288, 93rd Congress.

The disaster situation resulted from an intense winter storm, generating high winds, heavy seawave, coupled with high tides, which struck the Kodiak Island, resulting in extensive damage to public and private properties in communities in the tidal areas of the Kodiak Island Borough of this State on January 20, 1980.

The port facilities at the City of Kodiak were topped by the high wind-driven water, causing extensive erosion and destruction to the public docks and shoreline roadways. The dock is the only container ship operation in the area and is the hub of transportation for domestic and foreign shipping. The area utilized for container parking and portions of the dock were rendered unusable and will require extensive repair/replacement. The roadway affected is the only highway to the airport, Coast Guard Base and the southern portion of the Island.

The City of Port Lions suffered damage to its harbor facilities, e.g., float, ramp and boat slips; and the City of Ouzinkie had damage to their harbor area boardwalks.

The communities of Old Harbor and Ahkiok had breakwater damage and extensive erosion; the villages of Karluk and Ahkiok sustained extensive damage to private housing, e.g., roofing, etc.

Total damage estimates at this time are Public \$475,000, and Private \$70,000.

On February 5, 1980, I directed the execution of the State Emergency Plan in accordance with Section 301 of the Law. State and local efforts in response to this disaster situation have been as follows:

The City of Kodiak has: repaired broken water mains to maintain fire protection capability, hauled in immediate essential backfill to provide limited utilization of the dock facility, performed the necessary engineering and have prepared bid documents to proceed with the emergency backfill and rip rap work. The Kodiak Island Borough has dispatched building official personnel to survey damages and estimate dollar amounts in the villages of Port Lions, Ouzinkie, Old Harbor, Akhiok, and Karluk. State response will be directed in support of the Kodiak Island Borough efforts in the area of private housing and individual losses.

I find that the situation is beyond the reasonable capabilities of the State and the affected local governments to effectively alleviate the existing situation.

The magnitude of repair/replacement costs of the public damages would exceed the capabilities of the local government, thereby, having a drastic impact to on-going community programs essential to the people's needs. The State has, within the past fifteen months supported, without federal assistance, five (5) Governor declared Disaster Emergencies with expenditures exceeding 4.5 million dollars. Therefore, the needs of the people would best be served with Federal funding support.

Pursuant to Section 301 of the Law and Federal Disaster Assistant Administration Regulation, I certify that the total of expenditures and obligations for this disaster for which no Federal reimbursement will be requested are expected to exceed \$95,000 in accordance with the following table:

Kodiak Island Borough.....	\$15,000.
(Engineering, assessment, project management, etc.)	
State of Alaska.....	\$80,000.
(To provide assistance to individual/private sector and the necessary highway repairs.)	

Individual Assistance:

Housing.....	\$70,000.....
Individual & Family	
Mass care.....	
Other.....	

Total.....\$70,000.....

Public Assistance:

Debris & wreckage clearance.....	\$2,000.....
Protective work.....	\$8,000.....
Restoration of Public facilities....	\$460,000.....
Public Safety.....	
Other engineering.....	\$5,000.....

Total.....\$475,000.....

Grand Total.....\$545,000.....

I have made direct requests for assistance under other statutory authorities on the following Federal agencies with their response as indicated. Corps of Engineers provided one technical person, to assist the State and borough in initial damage assessments in the City of Kodiak dock area.

I further request other Federal assistance of the following type: restoration of facilities, authorized under the law. Attached is a table of estimated costs broken out by category of public and individual assistance.

Public Assistance:

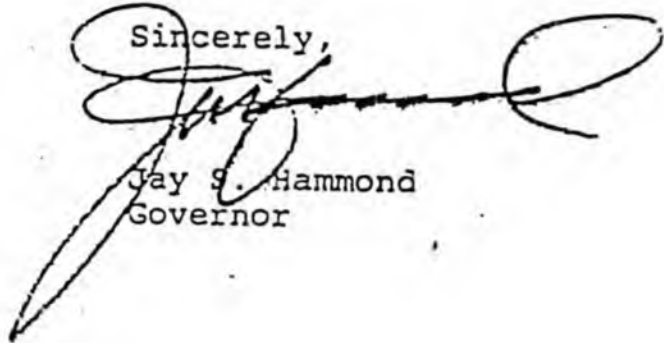
City of Kodiak dock/roadway.....	\$460,000
City of Port Lions harbor facilities.....	\$50,000
Old Harbor erosion/rip rap.....	\$15,000
Ouzinkie harbor boardwalks.....	\$7,500

I request that the following (boroughs and political subdivisions) be designated as eligible for this Federal assistance under your declaration:

1. Kodiak Island Borough
2. City of Kodiak

Additional consideration which support the necessity for assistance in this disaster situation are; this is an economically depressed area, due to seasonal economy, e.g., fishing.

Sincerely,

A handwritten signature in black ink, appearing to read "Jay S. Hammond". The signature is highly stylized with large loops and a long horizontal stroke extending to the right.

Jay S. Hammond
Governor

TO:

Lloyd I. Turner
Deputy Director
Alaska Division of Emergency Services

DATE

February 6, 1980

FILE NO.

TELEPHONE NO.

FROM:

Harold E. Wolverton *filed*
Southcentral District Supervisor
Alaska Division of Emergency Services

SUBJECT

Trip Report Kodiak Storm
Damage to dock facility

I. PURPOSE

For an on-site inspection and to survey damage to Kodiak dock facility due to storm disaster.

II. ACTIVITIES

At 8:00 a.m. on January 28, proceeded to Kodiak accompanied by John Chapman, Corps of Engineers. Arriving at Kodiak, we were met by John Stafford, City Engineer, Kodiak. Mr. Stafford provided maps of the dock area, pictures of the associated storm damage, specifications for bids to be accepted January 30 for emergency repairs entailing backfill, and armor rock to three areas adjacent to Pier Number 3 and one small area adjacent to Pier Number 2. We then proceeded to Pier 3 to survey the damage. The main damage consisted of wash away of armor rock and backfill erosion to the south edge of the east trailer parking area. This was some 450 to 500 feet in length and ranged from 6 to 15 feet in width of erosion area. In addition, in this immediate vicinity some extensive electrical powerline and one 8-inch waterline were affected by the pounding waves and erosion of the supporting fill material. Located at the east and west ends of the center portion of the pier and dock facility are the wing walls which retain backfill. The waves had washed fill rock and some armor rock from behind and under these wing walls, making it moderately dangerous to maneuver or park van trailers in the vicinity of the fill area. In the center portion of the dock facility which is created by "H" pile and concrete panel bulkhead retaining wall and fill material behind. There was evidence of wash out below as new fill had been placed in several holes. Two incidents of a cave-in resulted in a taxicab dropping through the surface and in another incident a worker dropped through the surface to above the knee on one leg. Mr. Chapman took numerous pictures and some estimative measurements as evidence was apparent that seaward shifting of the lower portion of the concrete panels comprising the bulkhead was clearly evident. The armor rock and fill material that wash away at Pier 2 was very minor by comparison, but is included in the emergency work proceeding by the city since further high tides and wave action would attack the roadway serving Pier 2.

Lloyd I. Turner
Page 2
February 6, 1980

In addition to city facility damage the state highway between Piers 2 and 3 was impacted considerably by the storm, and armor rock and fill material had been washed away in several spots. The city engineer informed the DOT representative in Kodiak of this activity but had not heard back at the time we were there. It appears that any additional wave or storm activity of moderate proportion could undermine the highway at this location. We then proceeded to the city/borough office facility and met with the City Manager Clair Harmony and the newly appointed Borough Manager Gary Havanec. Later we were joined by the City Mayor Alan Beardsley. Copies of the city disaster declaration and borough disaster declaration were provided. In addition, Mr. Havanec provided damage and dollar estimates to the outlying villages of Port Lions where approximately \$50,000 of damage to harbor facilities was sustained. The communities of Akahiok and Karluk sustained damages, primarily roof failures to the recently built rural native housing units, consisting of lost shingles, etc. The community of Old Harbor sustained damage by erosion in the vicinity of their oil supply tanks. The community of Ouzinkie sustained damage to its boardwalk. Estimated damages by community are listed as follows: Port Lions to the dock facility \$50,000; Old Harbor fuel tank erosion \$15,000; Akahiok housing roof damage \$28,000; Karluk housing roof damage \$42,000; and Ouzinkie boardwalk \$7,500.

The city manager and mayor, as well as the borough manager, inquired as to the disaster emergency declaration process which was explained to both of them from a State Declaration standpoint, and a possibility of proceeding to the Federal government with a Presidential Emergency Declaration request. Considerable discussion ensued as to the Pier 2 and 3 damages and options for repair. Mr. Chapman and Mr. Stafford discussed the probable damage to the center section, as evidence by the cave-in and wash out at both sides, where the wingwalls do some retaining. In addition, engineering studies and analysis have been done recently. The impact of cave-out in the lower section of the bulkhead retaining wall panels is such that undoubtedly it is impacting on the scissor piles which support the outer section of the pier supporting the main moving crane. Recent engineering indicates that these scissor piles are strained to the near maximum and further pressure in movement of the bulkhead panels could begin buckling the scissor piles with total failure of the crane support; and these results would be catastrophic to say the least.

It was pointed out in our discussion that this (one only) container handling dock facility provides for the city of Kodiak, the entire borough, and some of the Aleutian Chain and Prince William Sound areas. The fisheries industry for a considerable portion of the north pacific area, as is conducted by American Fishing Interests, is heavily supported by the Kodiak area and virtually all marine transportation support is provided by Kodiak harbor. Pier 3 is the only container-ship capable dock facility in this location. In addition, normal commerce, military support, and other marine transportation movements are heavily supported by this one facility.

Naturally its loss or serious damage to it delaying all or seriously affecting this marine transportation activity is a serious concern.

We returned to the Pier 3 area to further investigate the electrical repairs necessary and discussed some of them with one resident electrical contractor who had done most of the original installation. Mr. Chapman obtained some material size and took a few additional measurements to be certain he had sufficient facts for his report and damage survey confirmation. We met with the Sealand dock manager briefly and confirmed a few additional facts regarding the storm damage as it had ensued, and a few historical notes as he has been the facility manager about six years.

Mr. Chapman obtained a few additional engineering prints, and we were able to return on the afternoon flight to Anchorage.

Mr. Chapman will rapidly prepare his trip report analysis and estimates of emergency work quantities, dollar amounts, and some analysis of the damage to the center portion behind the crane supporting pier, indicating what further engineering studies may be necessary for total repair determination. In addition, details of electrical repairs to the trailer parking facility which was most seriously impacted by the storm will be prepared.

PROBLEMS:

NONE

RECOMMENDATIONS:

Recommend we maintain liaison with Kodiak as to further details of outlying village damage. Recommend we prepare disaster emergency declaration for the Governor for the entire borough area citing specific villages. Recommend we draft Governor's request to the President for an Emergency Declaration citing total damage in the neighbor of \$2 million dollars which should cover one or two options for the total restoration of Pier Number 3 in Kodiak.

DISPOSITION FORM

For use of this form, see AR 340-15; the proponent agency is The Adjutant General's Office.

REFERENCE OR OFFICE SYMBOL

SUBJECT

NPAEN-DB-ES

Investigation of the Storm Damage to the Kodiak City Dock Facility

~~THRU~~ ^{via} NPAEN-DB ^{12/20}
NPAEN ^{10/10}

FROM NPAEN-DB-ES

DATE 5 Feb 80

CMT 1

Mr. Chapman/lsc/2-2713

TO NPASA-EM

1. The purpose of the field trip to Kodiak on 28 January 1980 was to obtain information necessary for the preparation of an estimated cost of the storm damages to the City docking facility.

2. The inspection was made in the company of Mr. John Stafford, the City Engineer, and Mr. Hal Wolverton of the Alaska Division of Emergency Services.

3. The major area of damage was an area approximately 500' long to the east of the container cargo dock. The bank eroded into the staging area. The electrical system used to furnish power to the cargo containers was damaged to an extent that it will require replacement.


Scope of work to be done:

- a. Remove concrete wheel stops.
 - b. Remove electrical distribution panels, conduit, wire, outlet pedestals, poles and floodlights.
 - c. Repair broken water line to fire hydrants.
 - d. Grade bank prior to rebuilding.
 - e. Remove large rock that would interfere with the consolidation of new fill.
 - f. Reconstruct storage area and slope bank 1 vertical to 2 horizontal.
 - g. Apply filter rock.
 - h. Apply armor stone.
 - i. Replace electrical outlet pedestals, provide 6 new pedestals that are missing or badly damaged.
 - j. Replace light poles and flood lights. Provide one new pole and 4-1000 watt mercury vapor flood lights.
 - k. Replace distribution panels.
 - l. Run new conduit and wire to panel boards, outlet pedestals, and light poles.
4. There are three other small areas that were eroded by the storm. Reshape and rebuild banks, apply filter and armor rock.

NPAEN-DB-ES

5 Jan 80

SUBJECT: Investigation of the Storm Damage to the Kodiak City Dock Facility

5. There is a potential problem with the dock. The retaining wall along the back-side of the dock has kicked out at the bottom and is resting against the batter piles that support the back side of the dock and the inside crane rail. The fill behind is washing out, causing cave-ins on the surface. The problem with the fill could be much more serious than the surface cave-ins would indicate. This area is essential to the continued use of the dock. 

6. The east wing wall is in bad condition and could possibly fail during a bad storm. The city has entered into legal action against the designer for design deficiencies. The failure of this wall would cause erosion of the staging area and possible loss of vans and cargo. The wing wall is approximately 10 to 20' high and 100+ feet long. To remove the existing wall, redesign and construct a new wall is estimated to cost between 150 and 200 thousand dollars.

1 Incl
1. Cost Estimate


JOHN W. CHAPMAN
Chief, Estimates Section



February 20, 1980

Representative Fred Zharoff
Pouch V
Juneau, Alaska 99811

Dear Fred:

As you know, the storm damage of January 20 was substantial at the City's dock facilities. It was somewhere in the neighborhood of \$350,000 for simple restoration.

However, as was experienced in Nome with reconstruction and relocation of their City Hall, simple restoration will not be cost effective. In order to repair the dock facility at Pier 3, we have been advised by professional port engineers that the bulkhead and wing walls must be either reconstructed or replaced with a 50 foot wharf addition.

We are advised that design and construction could be adequately budgeted for \$2.7 million, plus another \$300,000.00 for storm damage repairs to the container storage areas and temporary activities at the container dock itself. The total would be \$3.0 million.

Senator Clem Tillion has indicated his willingness to support State funded port facilities in Kodiak. He termed our harbor as the "premier port" for development with the coming bottomfish industry and we suspect for the impending offshore oil development. In any event, it behooves not only the Port of Kodiak, but also the State of Alaska to secure the existing dock structure and to give consideration for additional future docks.

A recent study by CH2M-Hill indicated the design of the structure is marginally stable and is inadequate to meet the seismic load factors of 6.0 points on the Richter scale to which this area is subject. In the event of such an earthquake it is perceived that the bulkhead holding back fill material behind the dock would give way. This would leave a stable dock facility without vehicle access. Obviously repeated storms will merely repeat the recent damage which will in turn need to be repaired during the years ahead. This is a continuing problem that makes the dock facilities costly and beyond the local agency's means.


It is my understanding that Alaska Emergency Services is forwarding their detailed findings, which have been backed up by Army Corps of Engineers' observations, directly to your office for appropriate legislative initiative.

Representative Fred Zharoff
February 20, 1980
Page 2

The City of Kodiak has hired the lobbying services of Robert Hartig on major projects. He has been directed to assist you concerning this appropriation should you decide to sponsor the bill.

Most sincerely,

\ CITY OF KODIAK

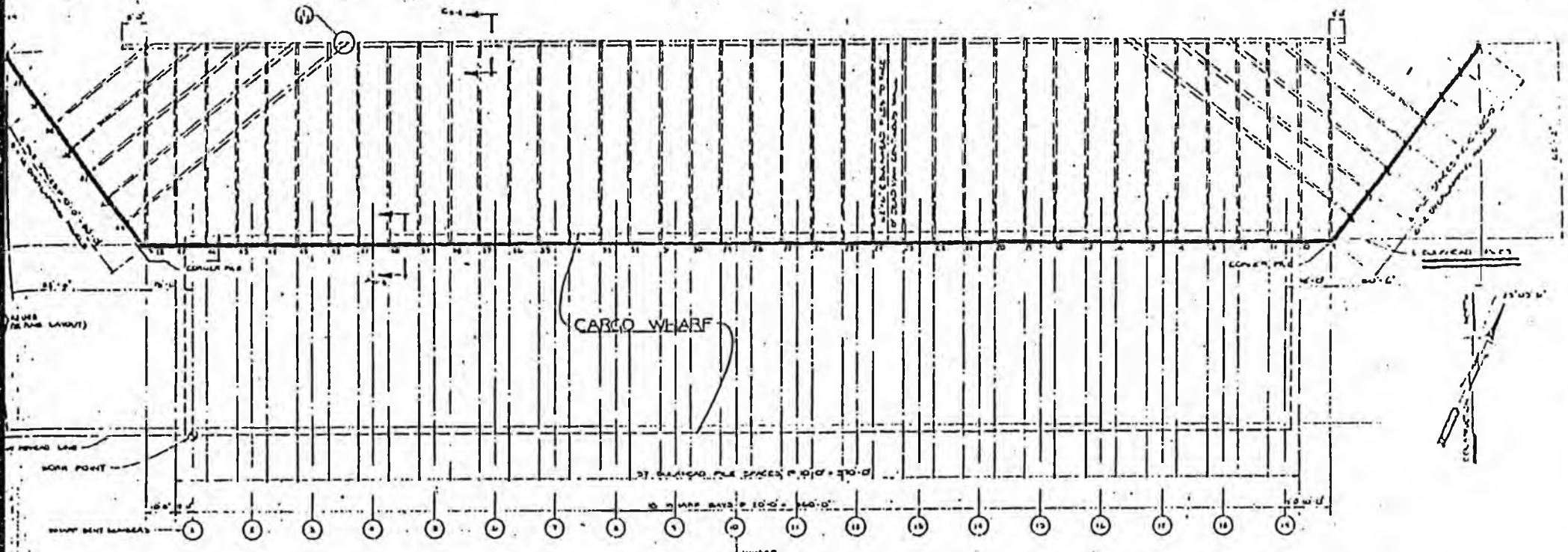


Clair W. Harmony
City Manager

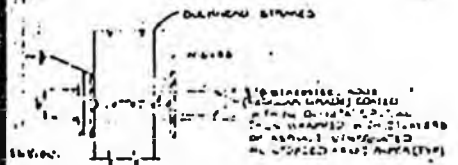
CWH/yb

cc: Robert Hartig, Esq.
Kodiak City Council
John Stafford, City Engineer
Alaska Emergency Services
Senator Bob Mulcahy

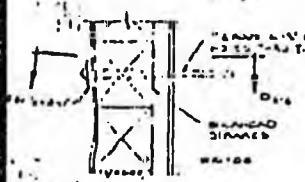
Enclosures



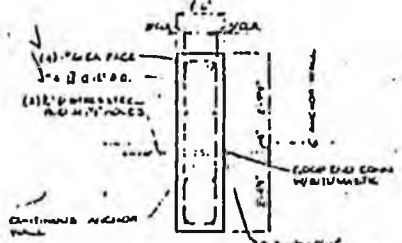
BULKHEAD PLAN



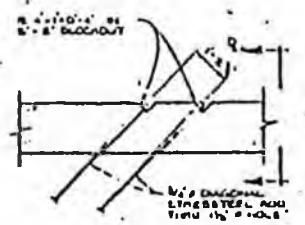
SECTION B (TYP)
1/4" = 1'-0"



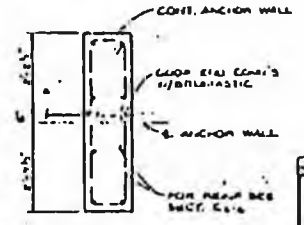
SECTION A (TYP)
1/4" = 1'-0"



SECTION C
TYP. OF PERP. TIE RODS
1/4" = 1'-0"




DETAIL 1
1/4" = 1'-0"



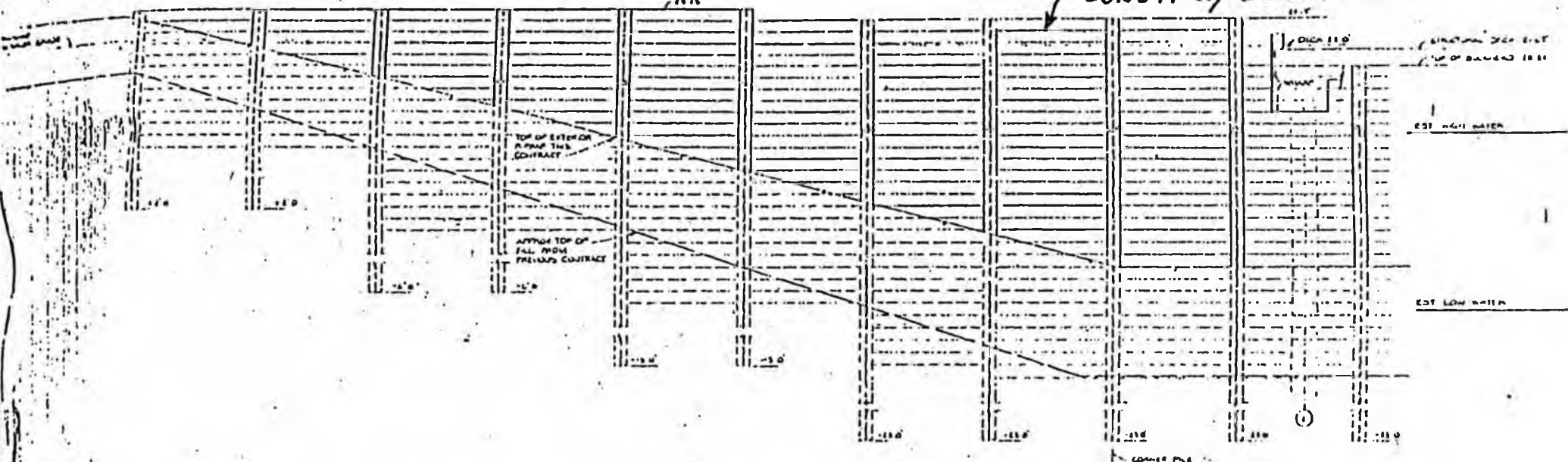
SECTION D
1/4" = 1'-0"

BULKHEAD PILE SCHEDULE

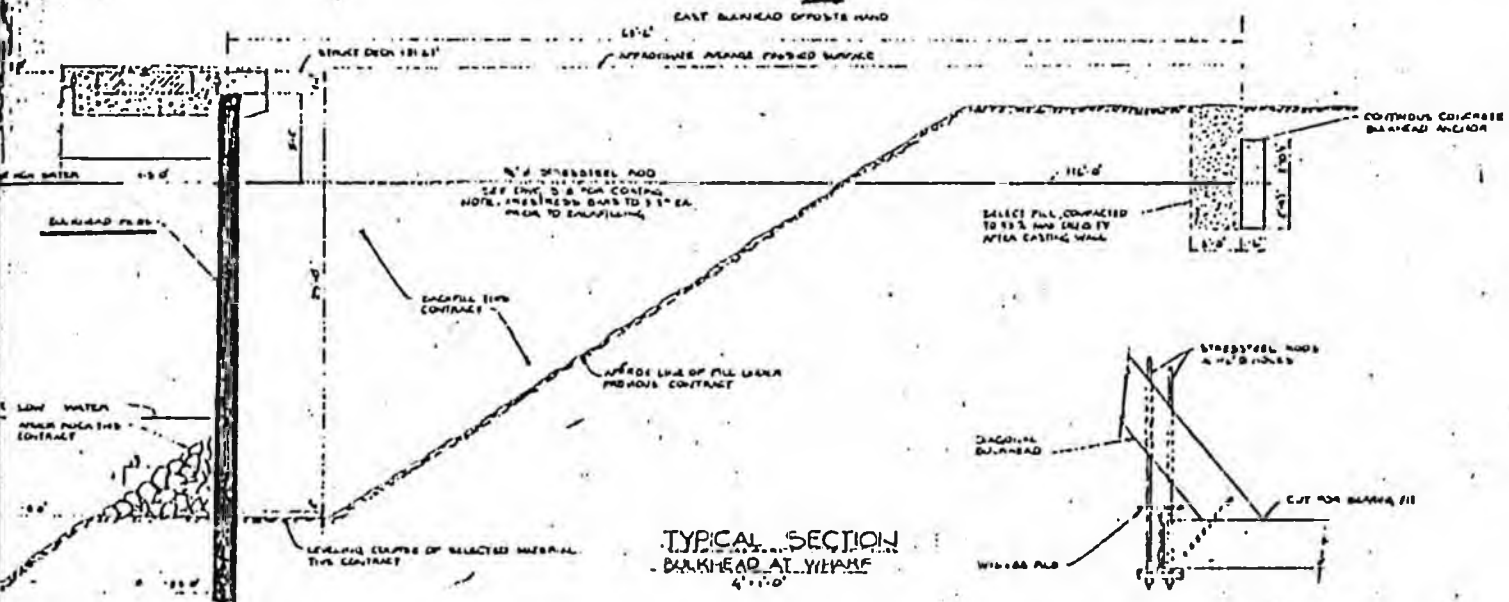
NO.	TYPE	LENGTH	WGT.
1	HP 14	20'-0"	1150
2	HP 14	20'-0"	1150
3	HP 14	20'-0"	1150
4	HP 14	20'-0"	1150
5	HP 14	20'-0"	1150
6	HP 14	20'-0"	1150
7	HP 14	20'-0"	1150
8	HP 14	20'-0"	1150
9	HP 14	20'-0"	1150
10	HP 14	20'-0"	1150
11	HP 14	20'-0"	1150
12	HP 14	20'-0"	1150
13	HP 14	20'-0"	1150
14	HP 14	20'-0"	1150
15	HP 14	20'-0"	1150


CITY OF KODIAK, ALASKA
CONTAINER SHIPPING FACILITY
GENERAL CONTRACT
BULKHEAD PLAN
 SHEET NO. 10 OF 10
 DATE: 07-1-00/52

CONST. W/ CONC. PANELS



EXTERIOR ELEVATION OF WEST BULKHEAD - SECTION A



TYPICAL SECTION BULKHEAD AT WHARF

PLAN OF BULKHEAD CONNECTION AT WEST CORNER PILE

EACH CORNER CONNECTION OPPOSITE WHARF

	100 600	CITY OF KODIAK, ALASKA CONTAINER SHIPPING FACILITY GENERAL CONTRACT BULKHEAD DETAILS
	100 400	
	100 200	
	100 100	

REPRESENTATIVE
 FRED F. ZHAROFF
 P.O. BOX 405
 KODIAK, ALASKA 99818
 (907) 486-5254



WHILE IN JUNEAU
 POUCH V
 JUNEAU, ALASKA
 99811
 (907) 485-4986
 485-4951

House of Representatives

March 25, 1980

M E M O R A N D U M

DISTRICT 14:

BELLSFLATS
 CHINIAK
 KODIAK
 OUZINKIE

To: Representative Mike Miller, Chairman
 House State Affairs Committee

From: Fred F. Zharoff *FZ*
 District 14 Representative

Re: Special Appropriation to the City of Kodiak for storm damage

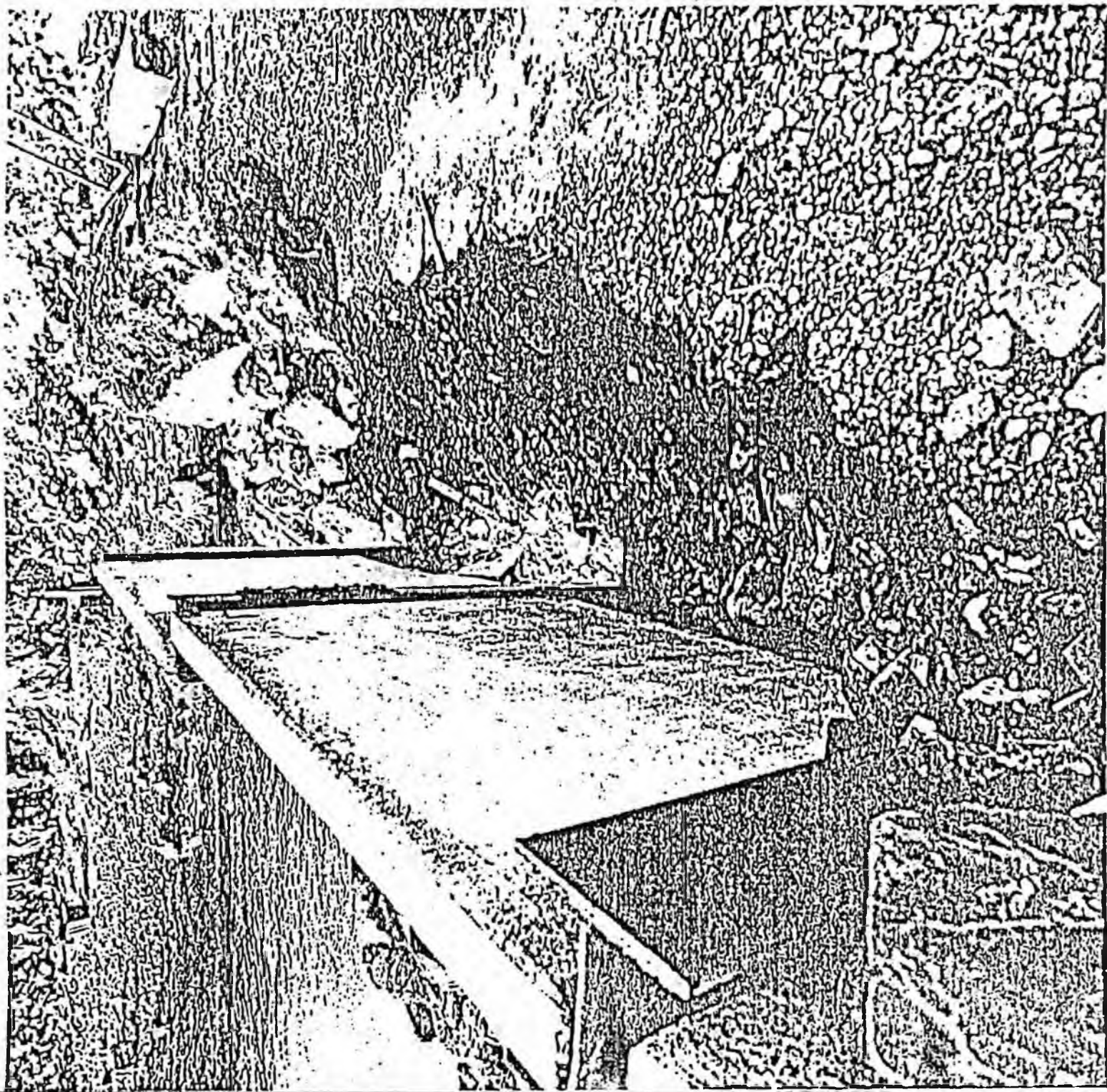
On January 20, 1980, an intense winter storm coupled with high tides and heavy seawaves struck Kodiak Island, resulting in extensive damage to public and private properties in communities in the tidal areas of the Kodiak Island Borough. On February 5, 1980, Governor Hammond determined the storm damage sufficient to warrant a disaster emergency, and directed the Division of Emergency Services to allocate such funds as considered necessary to cope with the disaster. Governor Hammond also asked for Federal assistance, which was subsequently turned down.

The storm caused significant damage to the City of Kodiak port facilities, City of Port Lions, City of Ouzinkie, City of Old Harbor, City of Akhiok, and the City of Karluk.

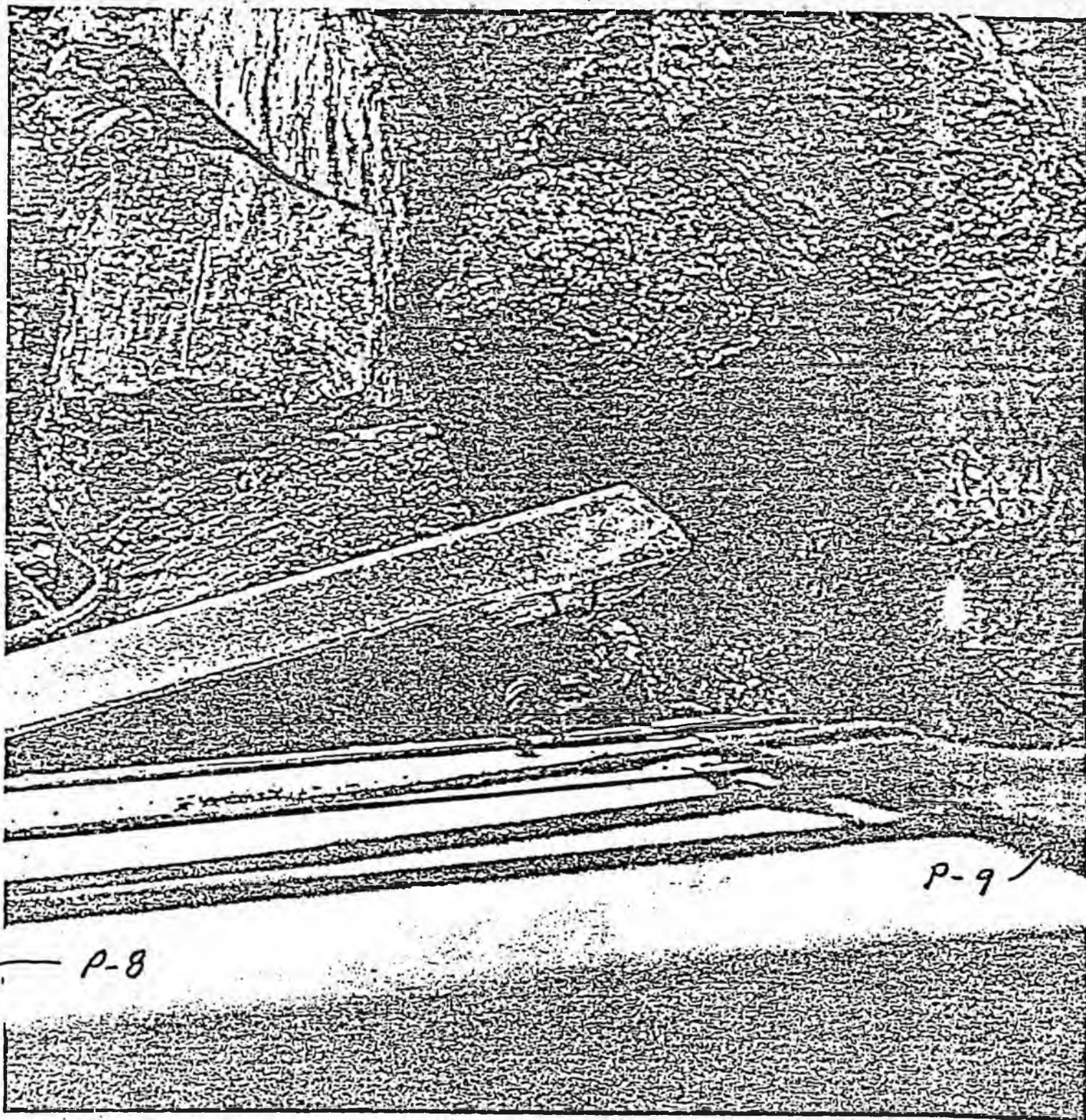
The Division of Emergency Services, after an on-site inspection, determined sufficient funds were available to provide the necessary assistance for most of the storm damage in the effected communities, however, the damage to the City of Kodiak port facilities was so extensive as to be well beyond the capabilities of the Division. It is at their suggestion that this special appropriation is being sought.

Inspection by the Division of Emergency Services and the Corps of Engineers, as well as a consulting engineer hired by the City of Kodiak, indicate that reconstruction of the port facilities to its prior state would not correct the severe built-in construction deficiencies and problems found.

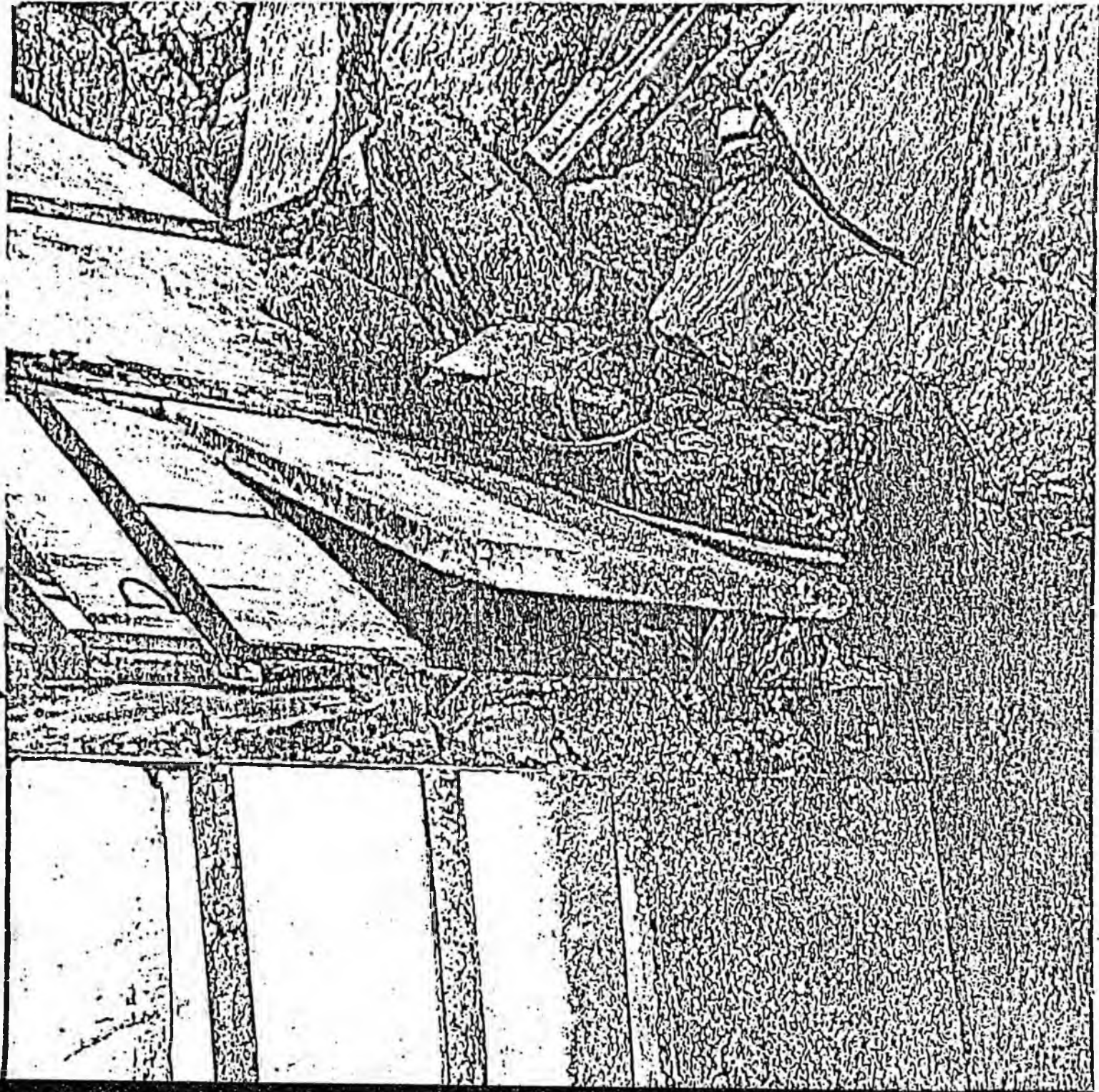
The passage of this bill would reimburse the City of Kodiak for cost of the emergency repairs they are currently undertaking, and provide funding for the permanent correction of the design deficiencies and problems.



Kodiak, Alaska
Pier 3, west wing wall
on or about 1-28-80



Kodiak, Alaska
Pier 3, East wing wall
en or about 1-28-80



Kodiak, Alaska
Pier 3, East wing wall
... on or about 1-28-80

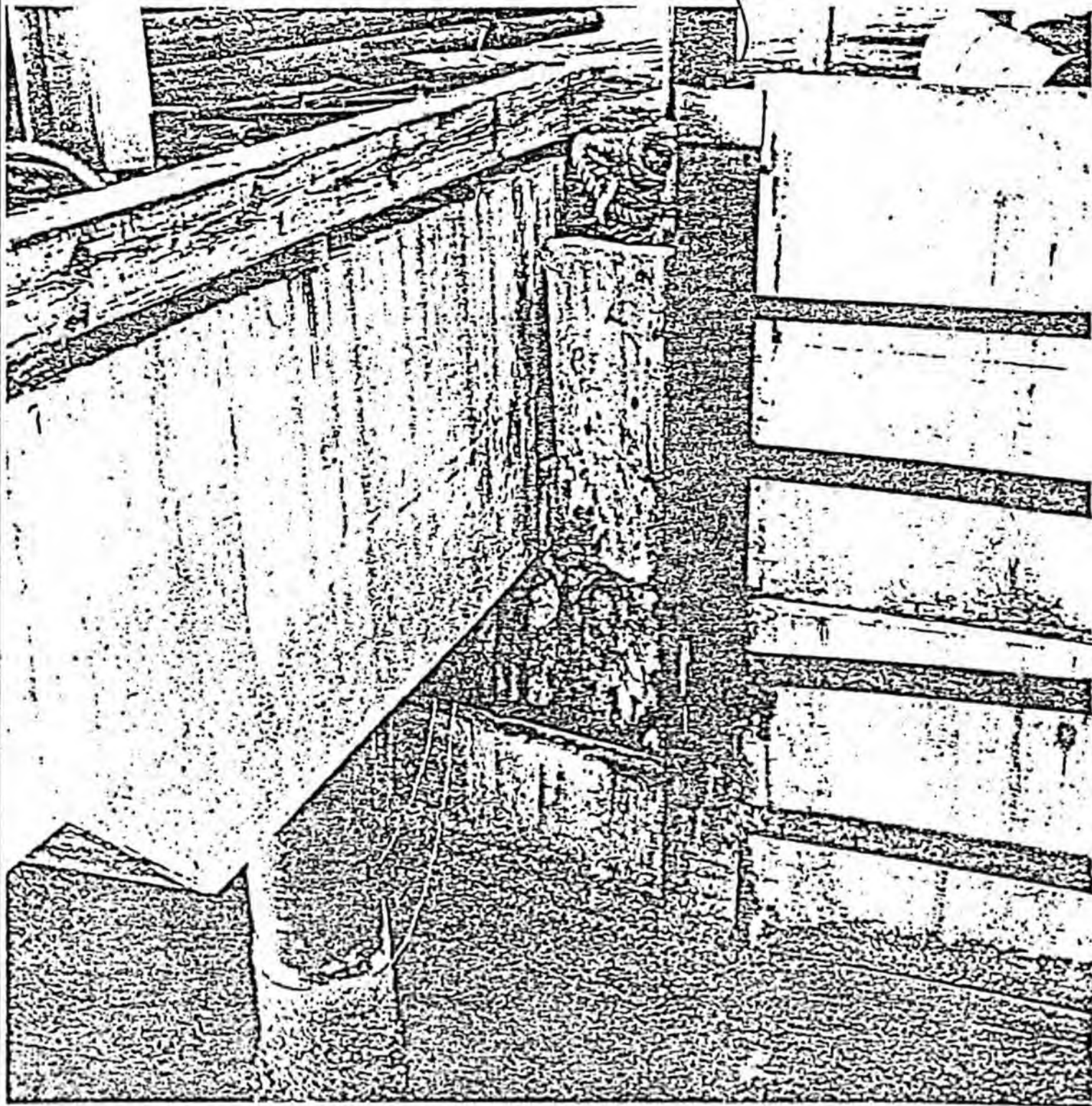


Kodiak, Alaska
Pier 3, Near East end of wharf
Em on about 1-28-33

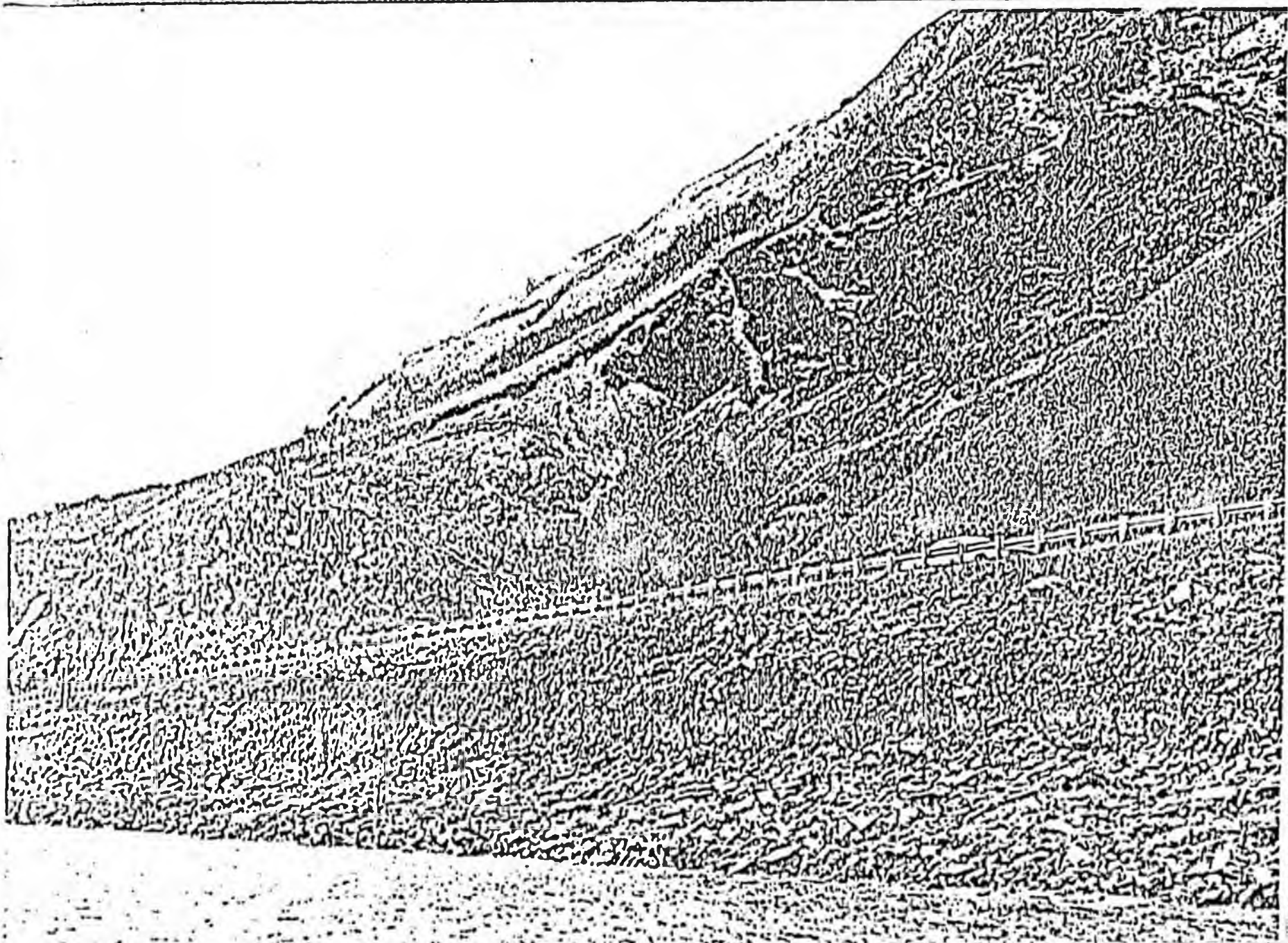
Kodiak, Alaska
Pen 3, East wing wall
E or absent 01-28-80

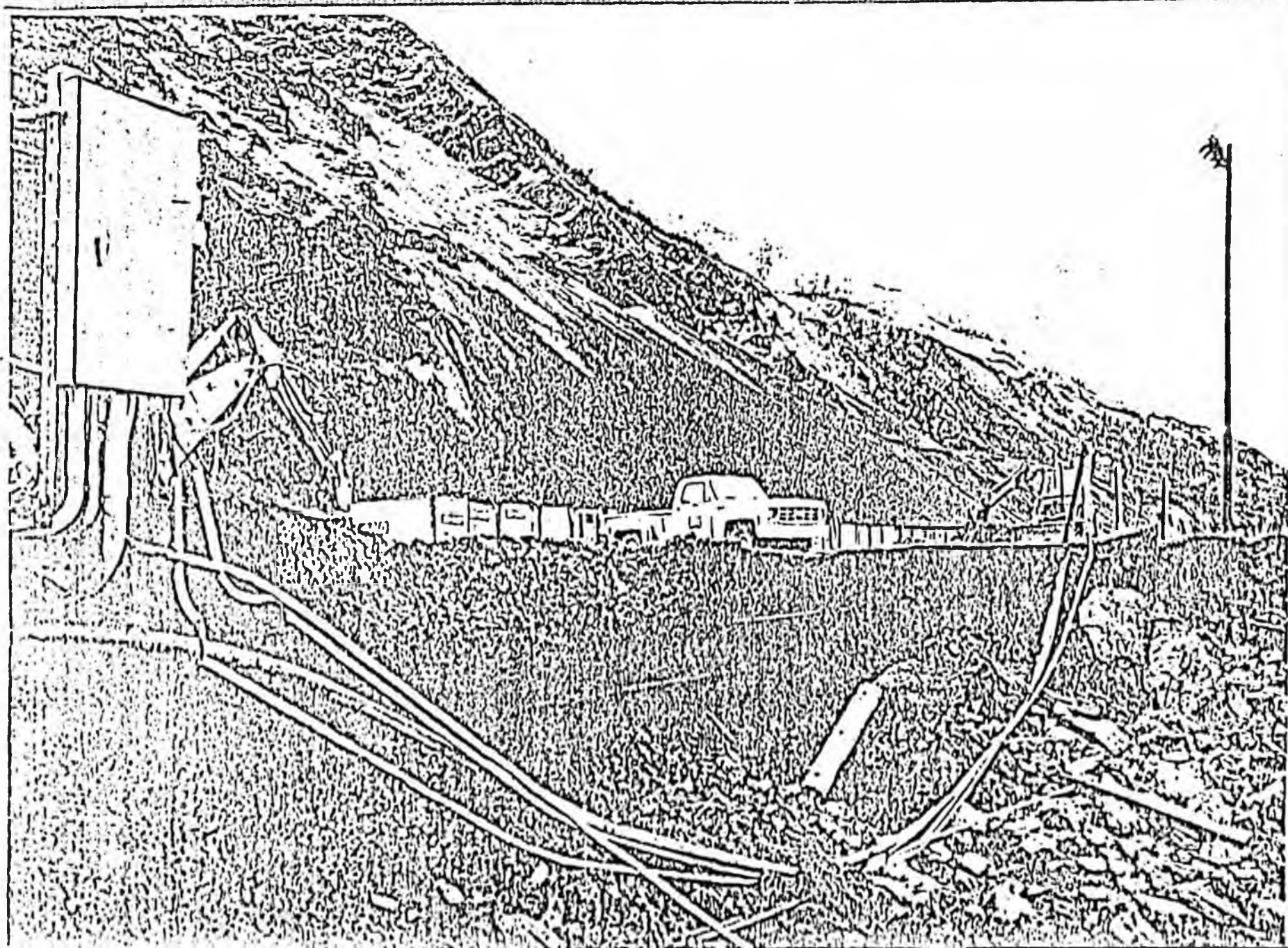


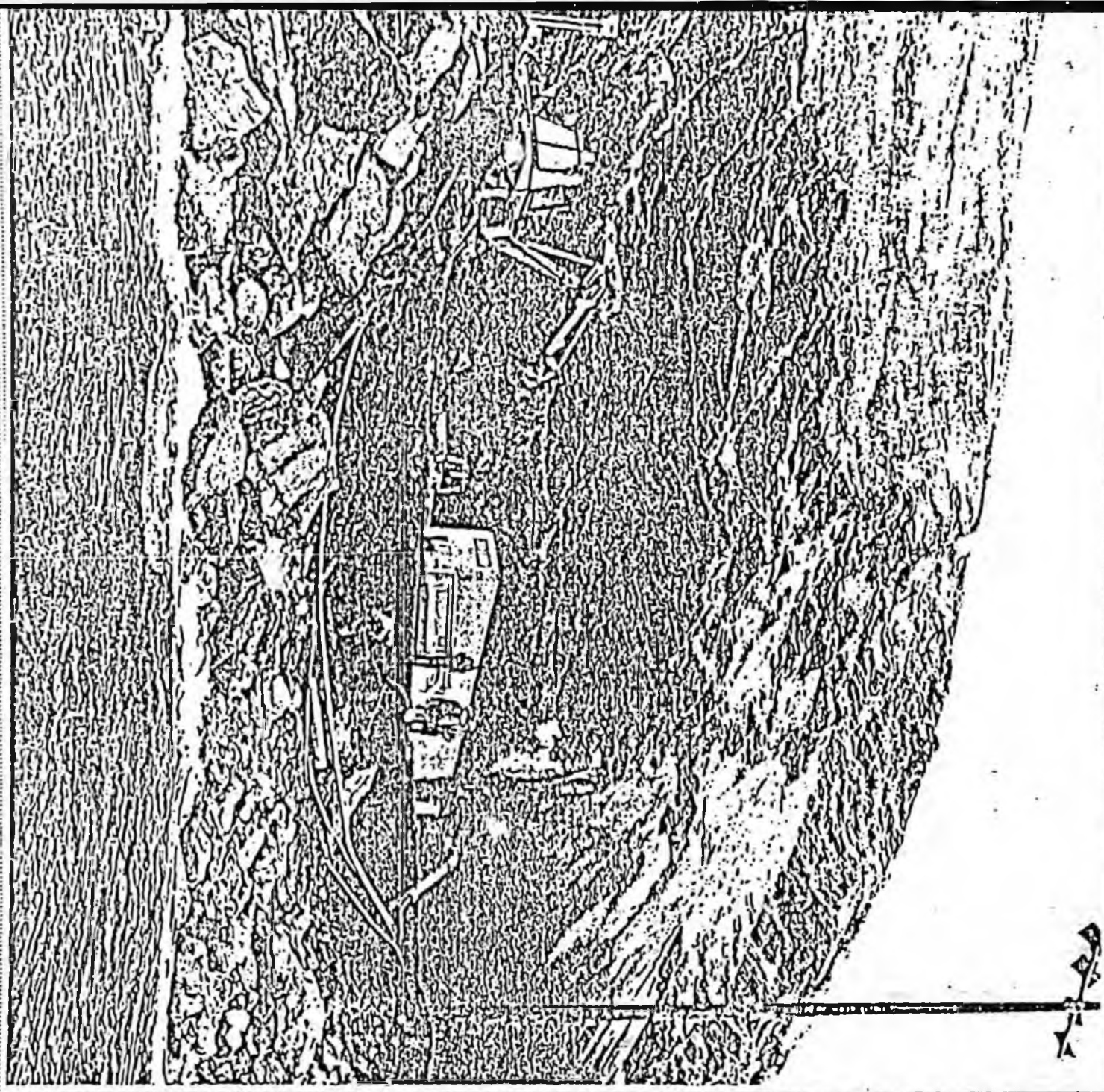
SP-10

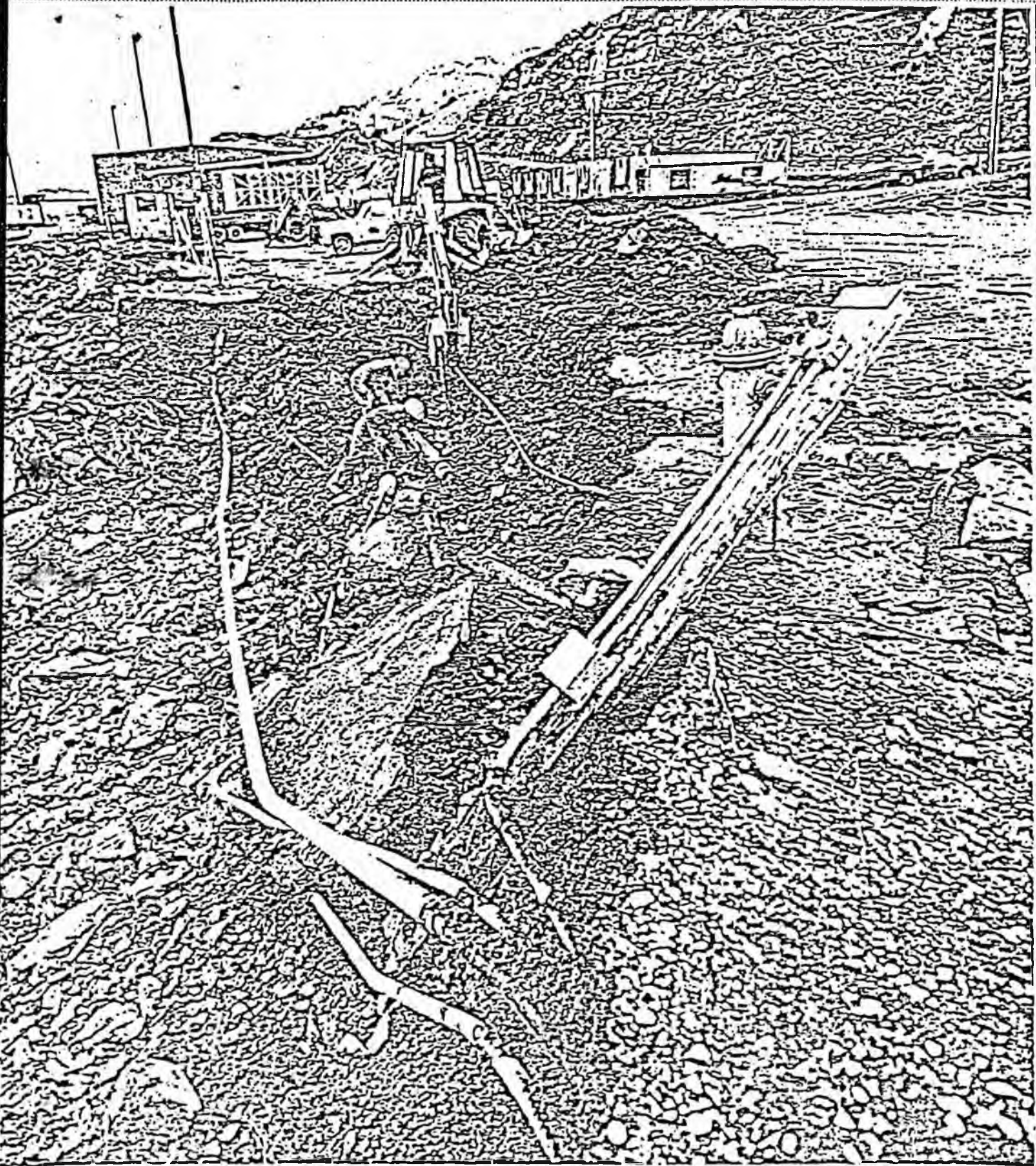


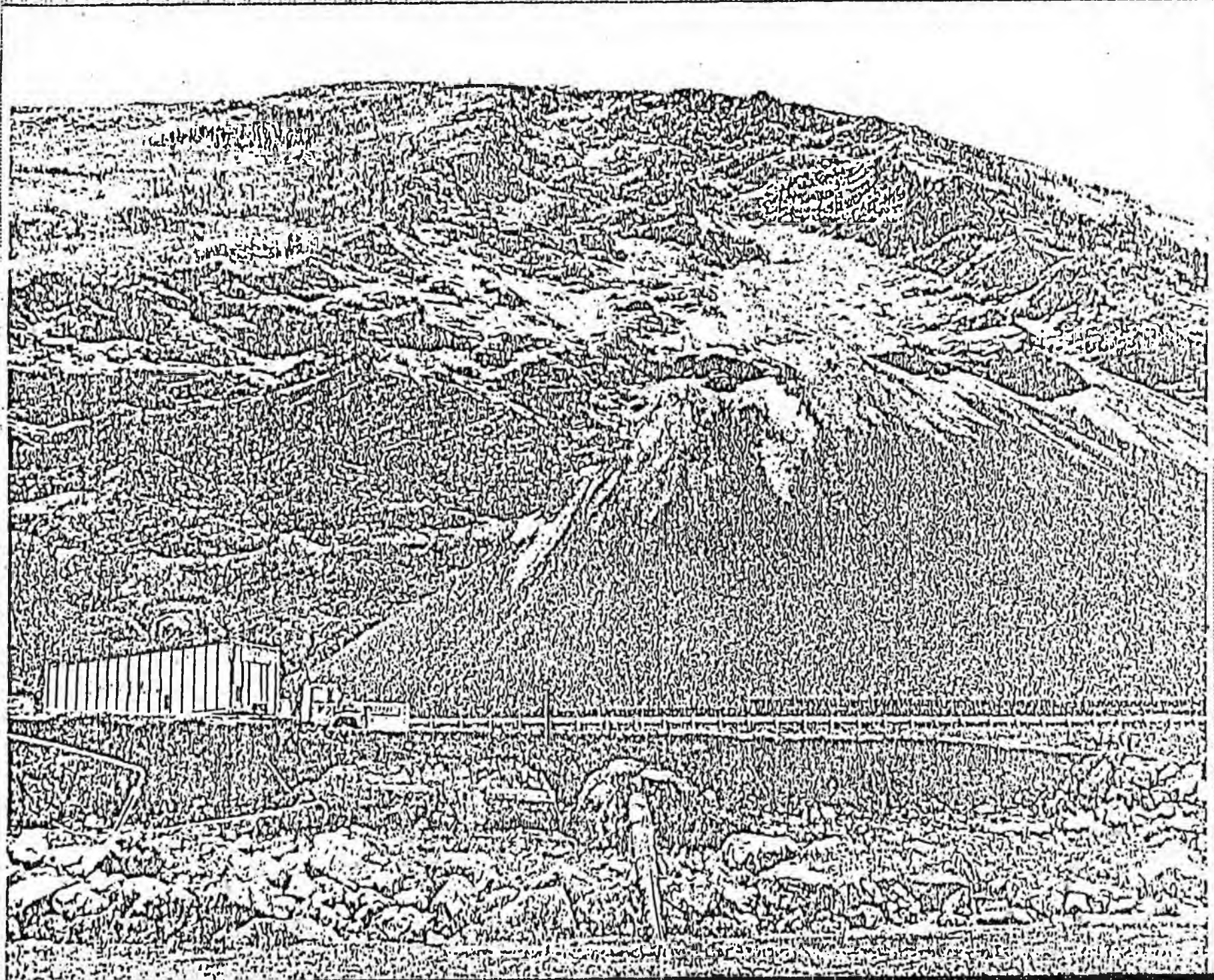
Kodiak, Alaska
Pier 3 East wing wall & wharf
on or about 1-28-80











Settlement was reported to be noticed during construction and continued after construction to a point, wherein enough concern was expressed to warrant a post-construction subsurface investigation by Dames & Moore, soils engineers of Seattle, Washington. Five test borings were conducted by that firm. The general conclusions of this study are as follows:

- "Upon removal of certain sand and gravel fill (west and east wings), settlement will stabilize at a point approximately 1" below the existing condition".
- "A vertical survey should be conducted monthly, immediately after removal of the soil and then at least semi-annually thereafter to assure that the site has stabilized".

Movement was noted periodically and in 1976, and again in 1978, structural reviews were conducted by R. Zagars, structural engineer, with subsequent corrective actions. During this time interval, levels were once again observed of the building movement.

USER VIEWS

During the period of the present subsurface investigation and site review, interviews were conducted with teachers, the school administrator, and maintenance personnel. A summary of their comments is as follows:

- Since the elementary school principal, Mr. Fred VanWallinga, has assumed his post at Auke Bay Elementary School in 1976, the building

has always had "active" structural distress. The school's activity has had a rapid increase during this past school year with noticeable physical changes, as well as leaks in the roof. These changes were even noticeable for individuals that have been accustomed to the "apparent movement" of the school site.

- April 1, 1980: The water from the school well turned black, clearing after several days.
- March/April, 1980: Window cracks (pressure?) appeared in northwest corner of building (fire door) and on library entrance glass wall.
- In Room 22, the fire doors would not open for 1 1/2 months past, and then suddenly they opened and stayed open and could not be closed.
- Every door in the school has been shaved at least once, and often, several times.
- Periodically the principal's office doors would not open or close.
- Most recently, Room 18 (the southwest corner) door would not open.
- A huge aufeis formation developed in 1976/1977 which led to apparent accelerated activity building movement. The ice formation was a glaciated condition that occurred during periods of extreme dry and cold weather conditions.

•1980 was the first year that the columns and beams have twisted after observations.

•A crack in the concrete wall/window opening on the west side of the structure opened approximately four to five weeks past to a state that it was readily viewable through, from the outside in and/or the inside out. The crack has since closed.

•During one day of the investigation period the windows in the northeast corner classroom and the door to the fire exit would not open. Several days later all windows/doors opened.

SUBSURFACE INVESTIGATION

A total of four test borings, four tests pits, and a piezometer observation well were placed at the project site. The location for each were surveyed and is reflected on the site topographic map. The summary of the test borings and pits are included within the appendix of this report.

The test borings ranged in depth from 35' to 44' below ground surface and were extended to that depth with a Mobile B-40, truck-mounted, drill rig equipped with a hydraulic cathead for drilling and sampling operations. The borings were put down using 6" hollow-stem, continuous flight auger and representative samples were obtained from the bore hole in accordance with ASTM specifications D-1586-64T. In this procedure, a split-barrel sampler (1.4" I.D.) is driven into undisturbed soil with a 140 pound hammer, having a 30" free fall. The number of hammer blows

origin was found in the depth interval 3' to 6' and was described as a wet, brown, gravelly sand with traces of organics and silt. For all other test holes and Test Hole 4 (below 6'), a soft gray to blue gray silt with some clay was found as shallow as 2' below the ground surface and extending to an average of 34' below the ground surface. Underlying this is a dense, sandy gravel (glacial till) which varies in thickness from several tenths to 1.5'.

Bedrock underlies the site at a relatively uniform depth and is a fairly massive, black slate. A bedrock core was taken in Test Hole 2 as shown in the Log of Test Borings.

Water Table Conditions

Due to the method of soils sampling, ground water observations could not be measured during drilling operations. A piezometer in Test Hole 4 has a summary range shown on Drawing 6. In addition, ground water was measurable in Test Pits B, C, and D. Ground water in Test Pit A filled the test hole within a matter of 20 minutes after excavation.

The water table at the site exists in a "perched" condition, being unable to percolate downward beyond the upper surface of the impervious gray to blue gray, clayey SILT.

SITE SURVEYS (VERTICAL MOVEMENT)

In 1959, vertical surveys were made along the perimeter and internally within the building for an approximate six month period. These vertical

surveys are reflected on Sheet Nos. 2 and 3, as well as by the graphs of vertical movement for specific building locations. Levels were largely discontinued in 1971 and accomplished again in 1978 and 1979, even though the building was reported by its occupants to be "active."

In reviewing the movement and the leveling procedure, as well as the bench mark utilized for such observations, it is the writer's conclusions that the observations are only accurate to $\pm 0.01'$. This conclusion is based on the uncertainty of the various bench marks used to observe vertical movement. For this reason, and while the drill rig was in the vicinity, two black iron pipe bench marks were drilled and driven to bedrock refusal.

As can be seen by the attached settlement charts, the building is, indeed "active." Generally, from the time period of September 1968 to June 1970 the building appears to be oscillating as much as $(+)0.31'$ (3 7/8"). Since that time, it appears that both settlement and rise have occurred in various areas of the building at a computed average rate of $\pm 0.005'$ year. The most massive differential settlement has occurred in the southwest corner of the building where it rose 0.14' and then settled, 0.175', only to rise 0.05' and settle again 0.02'.

PRELIMINARY

CONCLUSIONS

Vertical Movement

These are the following conclusions based on site engineering surveys of vertical movement for the Auke Bay Elementary School.

- The building has continued to be active from its time of construction to present time.
- The maximum vertical movement in the building has been in the southwest corner (0.12' downward) and in the boiler room (0.12' downward).
- The south center of the building has, within an area of 42' from the southwest corner of the building and 70' from the southeast corner of the building, had a differential settlement of 0.215' (2 9/16") (settled 0.10' and rose 0.11', respectively). The northeast corner of the building has risen 0.07', while the southeast and northwest corners have risen 0.04' and 0.3', respectively.
- Through all the differential movement, the building appears to have a twisting movement.

The building remains to be "active" to this date based on the user's views, as well as control surveys conducted. This activity, reportedly, has made the structural component work to an extent where (reportedly) our local building official (City and Borough building inspector) has most recently considered closing the building (vacating the building) due to the harm these movement cycles have done to the structural ingredients of the building.

footing and the independent column load spread footings could move in independent directions due to their "non-tied" condition as the sublying soil moves independently, i.e., a wave action.

It could be said that the building, in its present condition, is "floating" on its present sublying soil.

Other Considerations

The sublying soils are extremely sensitive to any changes in internal or environmental conditions. The "users" have spoken of doors closing and staying closed and windows closing and staying closed. In this condition, the safety of the building's inhabitants must be a prime consideration. The possibility of earthquake occurring with devastating results to the structural integrity of the building is highly possible. Also highly possible are other disasters such as fires. If the building were used in its present condition and the occurrence of some changes in the ground water table, loading of the building, loading of the site, etc. happen and should a fire break out for any reason (man-made disaster rather than Act of God), could the building in this condition be counted on to allow its occupants to discharge safely? This situation may possibly be the worst condition that the occupants of the building could encounter.

RECOMMENDATIONS

It is our opinion, upon reviewing the sublying soils conditions of the Auke Bay Elementary School, and the building's primary use, that the building, itself, should not be used in its present condition as an

elementary school. This recommendation is not based on the probability of an earthquake rendering devastating damages to the structure and its occupants only, but other forces and occurrences that could and are happening which endanger the occupants of this building. We believe the soil is too sensitive to continue to allow the use of Auke Bay Elementary School as a school, having its present foundation system.

There are several options available to local government. These options are;

1. Rehabilitate the structure:

Tie the foundation system together as an integral unit acting as one to assure structural response to settlement to be similar throughout the building. The present structure, as it exists, cannot withstand the probable range of ground displaced during seismic occurrence and particularly, in its present structural condition. Nor do we believe that the building can withstand its present "active" movement by applied environmental conditions (site loading, ground water table fluctuation, etc.). Retro-fit the above and below ground structural members so that complete failure of the building would not occur during seismic activity. This is a practice accomplished to older structures in the high seismic zone areas.

2. Construct a new foundation system underneath the present building supporting the present building on piles or other stabilizing

materials. Supporting the building on piles would be necessary to eliminate the present soil ground movement effects on the building due to the probability of;

- a. ground water level variations
- b. snow/ice load
- c. frost heave (perimeter)
- d. present differential site loading of embankment sands and gravel
- e. combined differential settlement of all the above to extend up to as much as 10"
- f. earthquake loadings

This probability differs from the rehabilitation of the structure in that a complete new foundation system would be placed underneath the building, plus the retro-fitting of the structural components of the building. In the rehabilitation of the structure, only the retro-fitting of the structural stability of the building would be considered and the present site settlement would be allowed to continue with annual maintenance occurrences. With this option, settlement would be eliminated.

3. Relocate Structure:

The relocation of the present Lake Bay Elementary School to a new site within the general area must assure that all of the

previously presented concerns and considerations are met. This would amount to a complete, new foundation system and then moving the school onto it. It must be noted, however, that the school is, in essence, a tri-level structure and any movement of the school, either in a north or south direction would entail extensive new construction to make the school completely usable (or additions/deletions to the school building).

4. As a last recommendation, it would be to design and build a complete, new school on this site with the foundation and structural engineering for the building taking into account the site sensitivity of the soil and modern state-of-the-art earthquake engineering analysis. The present building could be utilized by local government for other means, such as a warehouse, but in doing so, it must be realized that the building will continue to settle especially if high dead loads are placed within the building. The use of the building for occupational purposes would have to be questioned.

Within the appendix of this report, we have attempted to place a construction cost range of all of the above recommendations.

CLOSURE

It is our conclusion, based on all past and present soils reviews, that both the sensitive subsiding soils and the man-made fills have led to the deterioration of this public facility. The building has, and will continue

to deteriorate, due to the sensitive sublying soils. "Acts of God", such as earthquakes could complete devastate Auke Bay Elementary School.

Any solution to the school's present condition must take into consideration the sensitive sublying soils. The building must be made to act as a unit under dynamic load conditions. This is a condition that does not presently exist.

ESTIMATED COSTS

1. BUILDING REPLACEMENT

Building Size: 206'x141.5' = 29,149 square feet/level

First Floor Level: 29,149 square feet (\$110/square foot)	\$3,206,390
Second Floor Level: 29,149 square feet (\$80/square foot)	2,331,920
Total	\$5,538,310
Architecture and Engineering (4.6%)	254,762
Legal, Administrative & Contingencies (2.5%)	138,458
TOTAL ESTEIMATED COST	\$5,931,530

2. UNDERPIN FOUNDATION AND TIE TOGETHER

Tear out existing floor/foundation and shore-up	\$ 728,725
Piles (purchase)	94,208
Piles (splice and drive)	128,000
Concrete Grade Beams	64,865
Floor Slab	100,762
Reconstruction of Interior	874,470
Total	\$1,991,030
Architecture and Engineering (5.1%)	101,543
Legal, Administrative & Contingencies (3%)	59,731
TOTAL ESTIMATED COST	\$2,152,004

3. MOVE BUILDING

City & Borough of Juneau estimate \$1,750,000

4. CHEMICAL STABILIZE SOILS

?

Original Sponsor: Fahrenkamp

Offered: 3/19/80
Referred: Finance

Funding Information

General Fund \$175,000
Other Funds -0-
\$175,000

BY THE COMMUNITY AND
REGIONAL AFFAIRS COMMITTEE

1 IN THE SENATE

2

CS FOR SENATE BILL NO. 450 am

3

IN THE LEGISLATURE OF THE STATE OF ALASKA

4

ELEVENTH LEGISLATURE - SECOND SESSION

5

A BILL

6

For an Act entitled: "An Act making a special appropriation to the Legisla-
7 tive Council for a study of options for local acquisi-
8 tion and ownership or management of the Fairbanks
9 International Airport and the Anchorage International
10 Airport; and providing for an effective date."

11 BE IT ENACTED BY THE LEGISLATURE OF THE STATE OF ALASKA:

12

* Section 1. The sum of \$175,000 is appropriated from the general fund to
13 the Legislative Council for funding the Senate Special Committee on Trans-
14 portation for a study of options for local acquisition and ownership or
15 management of the Fairbanks International Airport and the Anchorage Interna-
16 tional Airport.

17

* Sec. 2. The unexpended and unobligated portion of the appropriation
18 made by this Act lapses into the general fund June 30, 1981.

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* Sec. 3. This Act takes effect immediately in accordance with AS 01.10.-
20 070(c).

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