

**ALASKA
CLASS
FERRIES
MATERIAL**

<TARGET><BILL></BILL><SUBJECT>ALASKA CLASS FERRIES
MATERIAL</SUBJECT><COMM>STRA28</COMM></TARGET>

Dana Owen

From: Rep. Jonathan Kreiss-Tomkins
Sent: Thursday, January 24, 2013 11:24 AM
To: Tully McLoughlin; Stephanie Scott (sscott@haines.ak.us); Dana Owen; elibbw@gmail.com; campbell@cityofsitka.com
Subject: FW: Sponson Confusion

Not sure if you all got this.

-----Original Message-----

From: hopkins@kpunet.net [mailto:hopkins@kpunet.net]
Sent: Thursday, January 24, 2013 11:18 AM
To: venables@aptalaska.net
Subject: Sponson Confusion

Dear MTAB:

I read in yesterday's Ketchikan Daily News about the meeting MTAB had with DOT. In the article, a sponson was described as a "stabilizing protrusion." A sponson has nothing to do with stability. It serves as a resting platform for the bridge span that is lowered from ferry terminal docks to the car deck so vehicles can pass from ship to shore or shore to ship. A sponson is hollow inside; it does not have any barite mud or water for stability. The interior of the sponsons is open to the void tanks below the car deck.

In seas over seven feet high, a sponson will slam loudly and violently. A physical property of water is that it will not compress, and when trapped under a sponson it has no place to go and the corresponding energy reverberates throughout the hull.

It is necessary to have a sponson when the side-loading door openings are forward where the hull begins to taper toward the point of the bow.

Without a sponson to rest upon, the bridge span would not hold safely to the car deck, and may slip off the side of the ship, especially during a falling tide.

In 1990, the very same Pat Eberhardt of Coastal Corp, served as the AMHS Port Engineer. He had signed a maintenance contract with the Seward Shipyard that year for the Aurora. We had to deliver the vessel to Seward in late October by crossing the Gulf of Alaska. We started out with Small Craft Advisory weather, and it was a miserable ride, with seas slamming under the sponsons at a rate of six hits per minute. Some of these hits were incredibly violent, and my concern as the captain was rupturing a sponson. Should that occur, water would flow into the void tanks below the car deck. Nearing Yakutat, the weather changed for the worse, and Storm Warnings were issued. We stayed at the Sitka Sound Seafood dock in Yakutat for four days. Knowing that I was faced with returning the Aurora from Seward in late November, the height of the storm season and prevailing southeast wind, I cancelled the voyage. When a window of better weather opened, we dashed back to Cape Spencer and returned to Ketchikan, and entered into the Ketchikan Shipyard for our maintenance.

I began to second guess my decision to return, and wrote a letter to Captain Harold Payne with regard to the voyage. Payne was perhaps the greatest captain AMHS has ever had. He told me with brevity, "A sponsoned ferry vessel should never be out there [in the Gulf of Alaska]." With regard to Mr. Eberhardt and his advice, I say "Buyer Beware."

Yours truly,
Captain William M. Hopkins, AMHS Ret



Alaska Department of Transportation & Public Facilities

Design Concept Report Day Boat Alaska Class Ferry

February 28, 2013



Mission Requirements

- Design & Construct the Next Generation of Ferry
- Bow/Stern Roll On-Roll Off Passenger Ferry
- Capable of Operating as a 12-hour Day Boat
- Primary Service in Lynn Canal (Juneau, Haines, Skagway)
- Fast Mooring and Loading Operations
- Seakeeping Similar to the *Taku*
- 99% Sailing Frequency



Vessel Requirements

- Approximate Length
 - 280ft Overall
 - 260ft at Waterline (40ft > *LeConte*)
- Schedule Speed: 15.5 Knots
- Loading/Unloading
 - Bow & Stern Doors
 - Bow Designed to Minimize Spray
 - Aft Port/Starboard Side Car Loading Openings
- Capacity
 - 300 Passengers
 - Minimum of 53 Alaska Standard Vehicles



Lynn Canal Day Boat Schedule

Juneau - Haines

Ferry Speed: 15.5 kn

	Terminal	Time	Underway Time	Load / Unload	Startup / Shutdown	Cumulative Time
Crew Start		6:45				0:00
Load	Juneau	7:00			0:15	0:15
Depart		7:30		0:30		0:45
Arrive	Haines	12:07	4:37			5:22
Depart		13:07		1:00		6:22
Arrive	Juneau	17:44	4:37			10:59
Unload		18:14		0:30		11:29
Crew End		18:29			0:15	11:44

Haines - Skagway

Ferry Speed: 15.5 kn

	Terminal	Time	Underway Time	Load / Unload	Startup / Shutdown	Cumulative Time
Crew Start		8:40				0:00
Load	Haines	8:55			0:15	0:15
Depart		9:25		0:30		0:45
Arrive	Skagway	10:16	0:51			1:36
Depart		11:16		1:00		2:36
Arrive	Haines	12:07	0:51			3:27
Depart		13:07		1:00		4:27
Arrive	Skagway	13:58	0:51			5:18
Depart		14:58		1:00		6:18
Arrive	Haines	15:49	0:51			7:09
Unload		16:19		0:30		7:39
Crew End		16:34			0:15	7:54

- Vessel arrival times are coordinated for faster transfer times
- HNS-SGY vessel schedule allows for 4 hours of maintenance by day crew



Roadmap Design

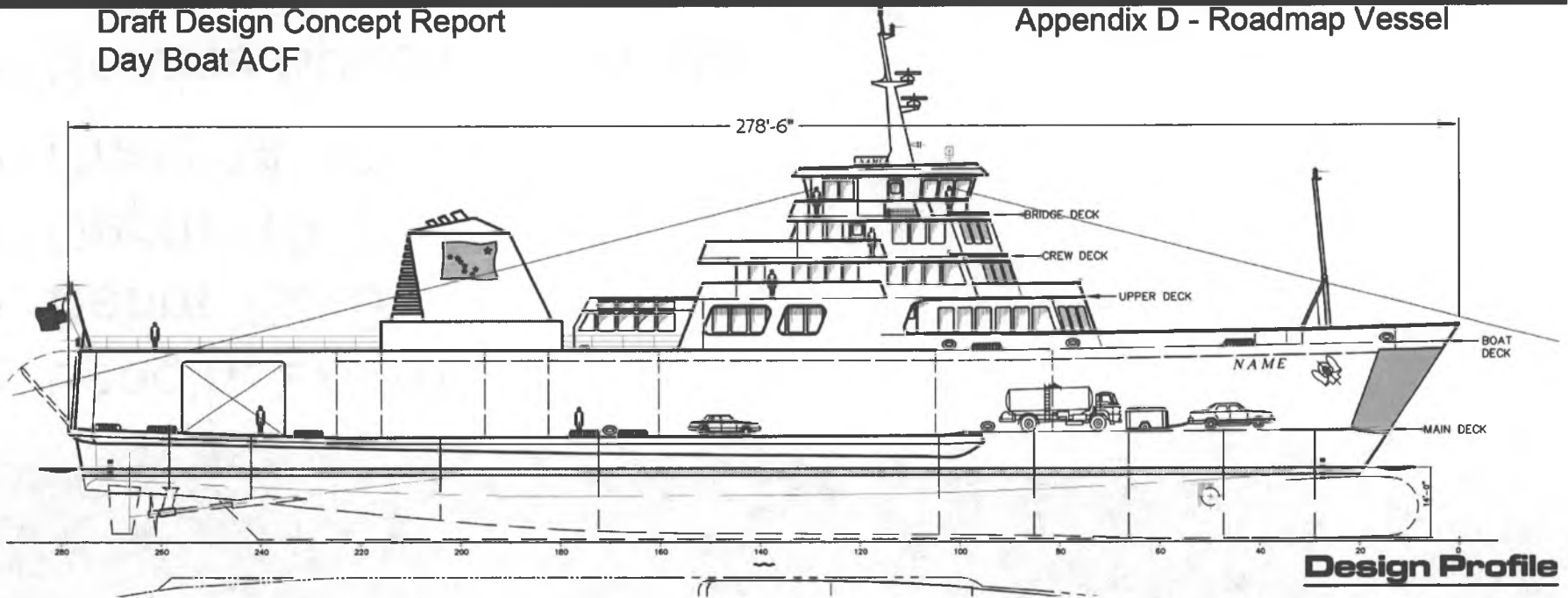
- Length: 278'-6"
- Beam: 62'-6"
- Depth: 19'-7"
- Draft: 14'-4"
- Service Speed: 16 Knots
- Vehicle Capacity: 53 Alaska Standard Vehicles
- Passenger Capacity: 300 persons (280 Interior Seats)
- Main Engines: 2 x 3000 hp
- "Clam Shell" Style Bow & Stern Center Doors
- Partially Open Aft Roof w/ Full (18') Height Bulwarks



Day Boat ACF "Roadmap" Vessel

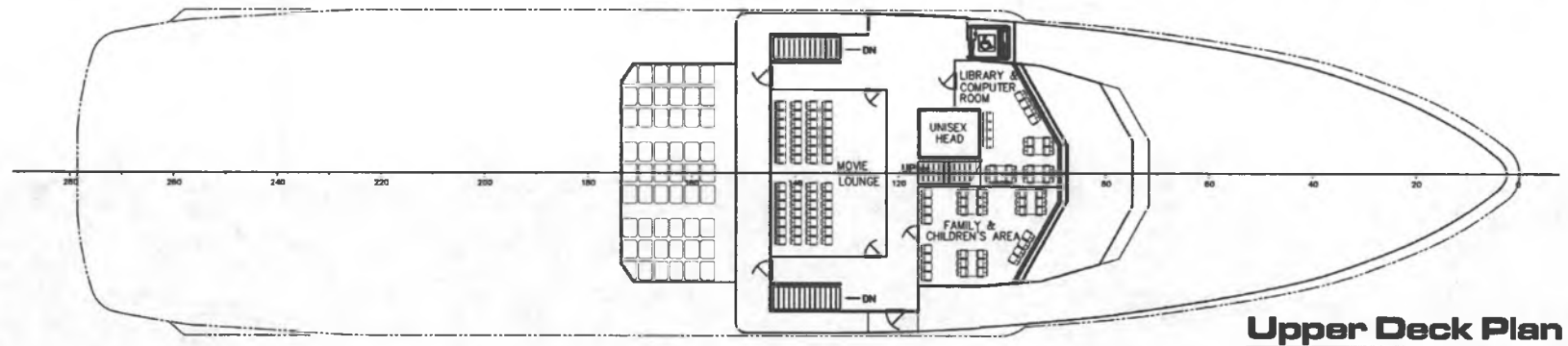
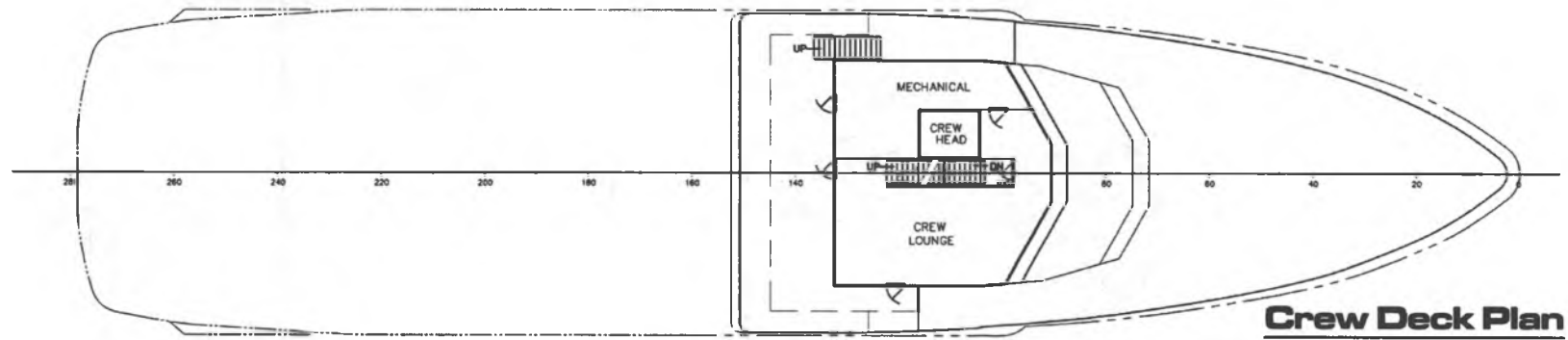
Draft Design Concept Report
Day Boat ACF

Appendix D - Roadmap Vessel



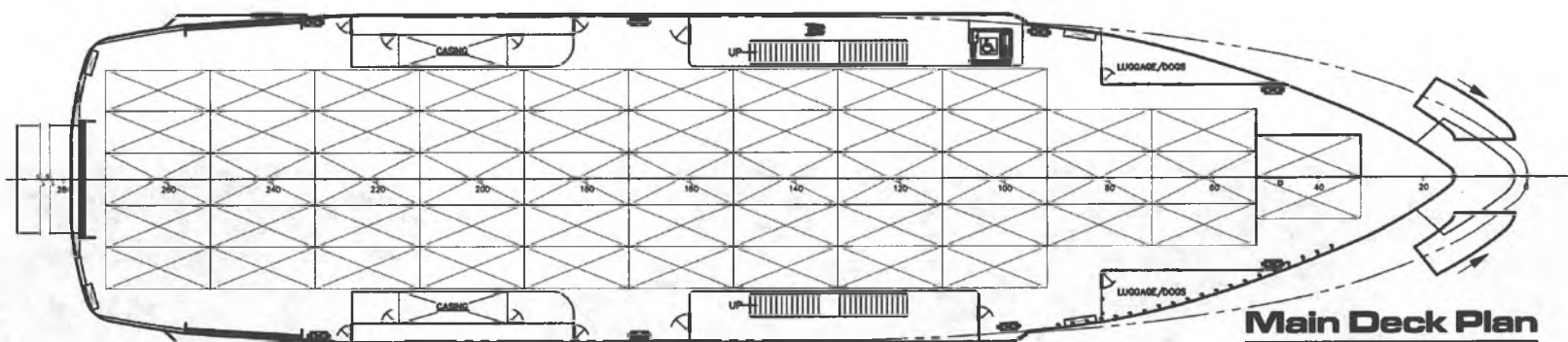
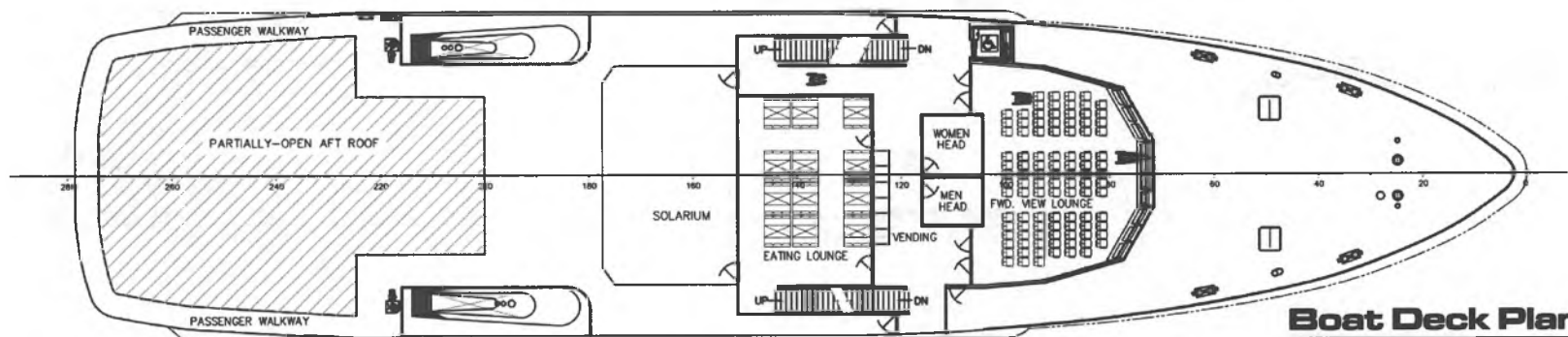


Day Boat ACF "Roadmap" Vessel





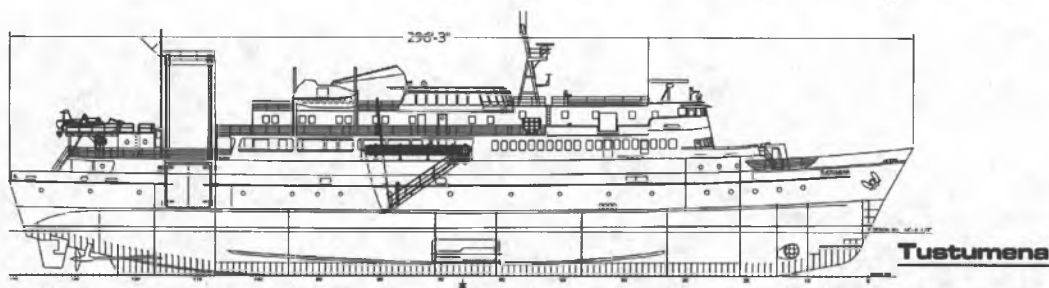
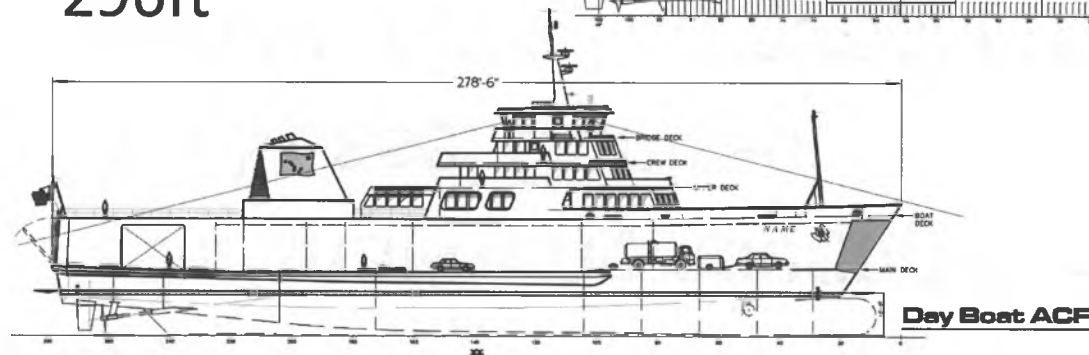
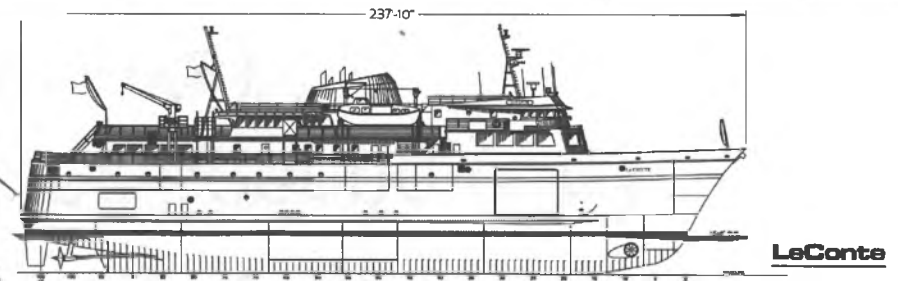
Day Boat ACF "Roadmap" Vessel





Vessel Comparison

- LeConte – 237ft
- Day Boat ACF – 278ft
- Tustumena – 296ft





Preliminary Cost Estimates

- \$3.2M - Preliminary Engineering (PE)
- \$49.2M - First Vessel Contract
- \$44.3M - Second Vessel Contract (10% Less)
- \$5.6M - Construction Engineering (CE)
- \$4.9M - ICAP (Both Vessels)
- \$107.2M - Total
 - Leaves \$9.8M for contingency from the \$117M remaining in the ACF budget.



Next Steps: Tentative Timeline

- February 25 – March 8, 2013
 - Design Concept Report & Public Review
- April 1, 2013 – Design Study Report & Concept Design
- May 1, 2013 – Preliminary Design
- November 1, 2013 – Detailed Design
- January 1, 2014 – Keel Laying First Vessel



Alaska Department of Transportation & Public Facilities

Alaska Class Ferry Change in Direction

Commissioner Pat Kemp

January 17, 2013



ACF History

- June 2006 DOT&PF issued a statement of services for a “Southeast Shuttle Ferry” design concept
 - Goal was to build a stern/bow roll on-roll off vessel similar in concept to the M/V Bartlett
 - Original design specs included: 255ft to 305ft length, 48-60 vehicle capacity, and bow, stern & side load capabilities
 - \$25-30 million - Estimated cost to build a vessel of this concept and size in 2006
- During the development process changes were made that diverged from the original concept
 - This included: elimination of the bow door, addition of crew quarters and lengthening the vessel to 350’
 - These changes created a new class of ferry between an Aurora Class and a mainline vessel.
- The ACF Design Study Report was completed in 2009 and the cost estimate grew to \$120 million



Funding

- The 2010 Alaska State Legislature appropriated \$60 million
 - Also in 2010, Governor Parnell “defederalized” the ACF project and the department transferred approximately \$1.5 million that had been expended for design to other state transportation projects. Defederalizing the ACF project allowed the state more flexibility to choose where and how the ACF would be designed and constructed.
- The 2011 Alaska State Legislature appropriated \$60 million



University Study

- DOT&PF hired UAF to independently analyze AMHS, the study was published in Fall 2011
- It found that there was no improvement in the overall efficiency of the AMHS by replacing the M/V Malaspina in Lynn Canal with a 350-foot ACF.
- There was an improvement in service with deployment of two 350-foot ACF's in Lynn Canal, but at an increased cost that resulted in the highest annual AMHS subsidy of any alternative UAF analyzed.



Change in Direction

- In fall 2012 the conceptual design had reached a point where accurate cost estimates could be provided by both the naval architect and ASD. These estimates showed the total project cost at \$150-\$167 million – 25-39 percent higher than the \$120 million appropriated and more than five times the original 2006 estimate.
- DOT&PF consulted with the Governor and received direction to reevaluate the direction the project had taken. The vessel design and purpose were reviewed and the department determined that going back to the original concept was the best course of action for service to the public.
- Governor Parnell announced in December 2012 to revert the design back to a stern/bow roll on-roll off concept which will cost less to build and operate, and better serve Alaskans.



Service Changes

- Two shuttle ferries operating between Juneau and Haines would have about 70% more capacity.
- Capital costs will be lower.
- Operating costs will be lower.



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Police Dept. 907.766.2121 • (fax) 907.766.2128
Fire Dept. 907.766.2115 • (fax) 907.766.3373

Stephanie Scott,
Mayor

January 16, 2013

Jerry Lapp,
Deputy Mayor

Dear Senator Egan and Representative Wilson,

Dave Berry Jr.,
Assembly Member

Thank you for convening a joint meeting of your respective Transportation Committees to inquire into the Administration's proposal to alter the plan to build the Alaska Class Ferry. The attached questions were collectively prepared by the Haines Borough Assembly during a special meeting convened on January 15, 2013, and we respectfully request the Committee's assistance in getting answers to them.

Steve Vick,
Assembly Member

Debra Schnabel,
Assembly Member

Joanne Waterman,
Assembly Member

Norman Smith,
Assembly Member

The plan to build and deploy the Alaska Class Ferry was developed with all stakeholders involved. In fact, a little over a year ago, the process was lauded by Commissioner Luiken at the December 15, 2011 Anchorage meeting of the Marine Transportation Advisory Board (MTAB).¹ We hope that you can bring to light the information that we need in order to fully evaluate this change of plan. We also hope that you can restore the public to its role through MTAB in the planning process.

Mark Earnest,
Borough Manager

Thank you for your work.

Julie Cozzi,
Borough Clerk

Sincerely,

Michelle Webb,
Deputy Clerk

Stephanie Scott
Mayor, Haines Borough

¹ According to meeting minutes, Commissioner Luiken "thanked the board and DOT & PF officials for their stable and persistent work on the Alaska Class Ferry project. He said it demonstrated the excellent team effort made working with the design company and Alaska Ship and Drydock. He said that the partnership with the design company (Elliott Bay Design Group in Seattle) and Alaska Ship and Drydock are a good example of how the process can work by bringing together two groups for the purpose of building a superior ferry."

Questions to assist the Alaska State House and Senate Transportation Committees inquiry into Governor Parnell's proposed replacement of the Alaska Class Ferry with two smaller vessels, prepared by the Haines Borough Assembly

I. Service Standard in Southeast Alaska: frequency, versatility, capacity, and backup

- a. Demonstrate how the plan to use the smaller ferries meets the need for versatility (i.e. the Prince Rupert run).
- b. The Alaska Class Ferry was suited to serve the entire southeast region. Explain how the plan for two smaller ferries supports the need for backup service should other vessels experience mechanical problems, and can add flexibility to the system when special community events require greater access. What is the envisioned operating region of the new plan?
- c. How will the new plan address the needs for service in the Lynn Canal during periods of inclement weather, especially high seas?

II. The Role of the Marine Transportation Advisory Board (MTAB)

- a. Wasn't the purpose behind the MTAB public process to get the design "right"? Wasn't the goal of the process to match the vessel to the need? MTAB identified the actual need and the proper solutions were articulated in the form of a concept design. Shouldn't the focus be on funding the right tool for the job, as opposed to changing the tool?
- b. What is the role of the MTAB in the Administration's new plan?
- c. How does the State's plan for the role of MTAB align with AS 19.65.180 (C) with respect to developing a strategic plan for the Alaska Marine Highway?

III. The Proposed Design for the Two Smaller Ferries to Replace the Alaska Class Ferry

- a. There is a renewed focus on bow doors. While bow doors are said to offer great efficiency of roll-on/roll-off operation, the need to seal things properly to provide sufficient watertight integrity may result in significant construction and operation costs. There have been a couple of serious life-taking ferry accidents in the Baltic - all related to bow door failures.
- b. Does not the proposed design, stern/bow roll-on/roll-off (RORO) require a specialized loading dock? Is this factor considered in the cost/benefit analysis of the new plan?

- c. A partially opened car deck configuration has been referenced in earlier discussions. Will this be safe for the proposed routes? If it is deemed unsafe, how will the change affect the cost of construction for the two smaller ferries?

IV. Funds/Cost: The purpose of the new plan is to control costs. How will it achieve this? "With declining oil production and declining state revenue, we have to be smarter with the people's money while meeting Alaskans' marine transportation needs." (December 4, Press Release from Governor Parnell announcing new direction.)

- a. We understand that the Alaska Class Ferry design was 35% complete, and that thus the cost estimates were in the same preliminary state. Provide us with the information that led to your conclusion that the AK Class Ferry would run over budget.
- b. To what level have the smaller ferries suggested as an alternative to the Alaska Class Ferry been designed? To what level has the cost of construction been estimated? Will you please provide us with the design and cost estimate documents?
- c. Provide the estimates of the operation and maintenance for one large Alaska Class Ferry, that makes one round trip but that can handle expected loads compared to the cost of operating 3 small shuttle ferries with crews several times a day.
- d. According to Commissioner Kemp's December 20 report, the decision to build two smaller ferries instead of the Alaska Class Ferry is based in part on a prediction in a report of "a substantial increased cost that resulted in the highest annual AMHS subsidy of any alternative UAF analyzed" (page 2, Commissioner Kemp, 12/20/12). The report is based on AMHS data from 2006. Why do you have such confidence in a report based on 6-year-old data, knowing that utilization (both commercial and non-commercial) has increased in the interim?
- e. How do you read the following sections of the UAF report that connect the highest increase in subsidy to a ferry-road combination as opposed to the replacement of the Malaspina by an Alaska Class Ferry?

Under Option 1B (Malaspina is replaced by an Alaska-Class shuttle ferry):

AMHS' financial performance is only slightly worse than the status quo (Option 1A) (p.189)

Profitability index "is statistically identical to that of the Status Quo and is to be expected." (p.191)

The Option 4 (Multiple Alaska-Class Ferry plus Juneau Access Highway) would (p.190):

Result in a greater operating subsidy than all options except for the “full” Service Expansion Option 3.

The revenues generated by the expanded Lynn Canal service fall well short of the level expected to accrue from the proposed capital expense.

In this option, revenue yield actually decreases while Marine Vessel Operating costs remain unchanged.

The solution – change the current labor contract:

Option 4 “appears the least ‘unprofitable’ of the six options.” (p. 191)

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The report concludes: “Options 1B and, 4 illustrate that ship replacement of one or more existing vessels with Alaska-Class ships will increase the subsidy requirement, particularly in Option 4 where the fleet size increases.” (p. 193)

- f. The per/mile ticket prices between Haines and Skagway are the highest in the system per/mile than on other legs of the Marine Highway. How will the construction of lower cost ferries affect user costs, specifically in Lynn Canal?

V. Reorganization of the State Department of Transportation and Public Facilities

- a. Why is it necessary to eliminate the position of Deputy Commissioner of Marine Operations? Where will the functions of the Deputy Commissioner be handled? Help us understand the proposed reorganization and administrative structure.
- b. The Alaska Marine Highway is a statewide function similar to airports and road systems. It serves communities and commerce from Bellingham, Washington to the Aleutians. Where does it fit in the structure?

VI. Process Oriented Questions:

- a. Why did the current Administration wait so long, at such a cost (+/- \$3 million), to weigh into a public process that was producing something “unwanted”? Is there some element in the procurement regulations that needs to be addressed to avoid wasting funds in a similar manner in the future?

LEGAL SERVICES

DIVISION OF LEGAL AND RESEARCH SERVICES
LEGISLATIVE AFFAIRS AGENCY
STATE OF ALASKA

(907) 465-3867 or 465-2450
FAX (907) 465-2029
Mail Stop 3101

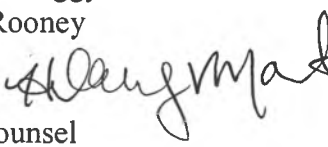
State Capitol
Juneau, Alaska 99801-1182
Deliveries to: 129 6th St., Rm. 329

MEMORANDUM

January 16, 2013

SUBJECT: Marine Transportation Advisory Board
(Work Order No. 28-LS0316)

TO: Representative Peggy Wilson
Attn: Becky Rooney

FROM: Hilary Martin 
Legislative Counsel

Under AS 19.65.180(a), the commissioner of transportation and public facilities must consult with the Alaska Marine Transportation Advisory Board regarding candidates for the position of "director or deputy commissioner of the Alaska marine highway system."¹ You have asked if the Department of Transportation and Public Facilities reorganizes so that there is no longer a person with the title of "director" or "deputy commissioner" of the Alaska marine highway system, whether the department would still have to consult with the board.

Consultation Requirement

Under the statute, the commissioner must consult with the board regarding candidates for the position of "director or deputy commissioner of the Alaska marine highway system."² The language does not necessarily apply to a specific division or section within the department, but to the Alaska marine highway system (AMHS) in general. The intent of the statute seems to be that the commissioner must consult the board regarding the candidates for the position, even if the position is not titled "director" or "deputy commissioner".³ One could also argue that the section refers generally to the "director"

¹ The full language of AS 19.65.180(a) is:

After the commissioner of transportation and public facilities has considered one or more candidates for the position of director or deputy commissioner of the Alaska marine highway system, the commissioner shall confer with the board regarding that candidate or those candidates before making an appointment to that position. The selection of those candidates shall be without regard to political affiliation.

² AS 19.65.180(a).

³ The committee minutes support this interpretation. When discussing the language of this statute, staff to the sponsor stated that both director and deputy commissioner were

Representative Peggy Wilson

January 16, 2013

Page 2

or "deputy commissioner" of the division or other segment of the department with responsibility for management of the AMHS.

Constitutionality of Consultation Requirement

Even if the department is still required to consult with the board, the consultation requirement itself might be unenforceable as it infringes on the power of the executive. The Alaska Supreme Court has held "that the appointment of executive officers is an executive function."⁴ The governor's power cannot be shared with another entity, including the legislature or a board, unless specifically provided for in the constitution.

The consultation requirement could therefore be invalid on separation of powers grounds. However, because it is only a consultation requirement, and does not require the board to create a list of candidates, or require confirmation of the selection, the statute could be valid. The attorney general seems to support this reasoning, recommending in one case that a requirement that the governor choose from a list of candidates supplied by a council be changed to a consultation requirement.⁵ The statute does not require that the commissioner follow the recommendation of the board, only that the commissioner "confer with the board regarding the candidate." Therefore, it is possible that as the statute is only a consultation requirement, it does not infringe on the power of the executive.

If I may be of further assistance, please advise.

HVM:lnd
13-018.lnd

included in the bill because the administration had changed the title of the position before, and so both titles were included "to allow for whichever of these positions an administration decides to appoint." House Transportation Committee meeting, pg. 15, March 27, 2008.

⁴ *Bradner v. Hammond*, 553 P.2d 1, 6 (1976).

⁵ 1981 Inf. Op. Alaska Att'y Gen. (April 23; J-66-698-81).

Alaska Department of Transportation & Public Facilities

Alaska Class Ferry: Project Overview and Change in Direction

System Overview

The Alaska Marine Highway System (AMHS) is currently comprised of 11 ferries, each of which performs a distinct mission for the department. The fleet can be divided into three classes of service.

The larger vessels are described as “mainline” ferries and are able to do the heavy haul for public travel with a greater capacity for semi-trailers, large trucks, heavy equipment, etc. These vessels are a 24/7 operation when in service and are manned with multiple crews and crew quarters. The vessels provide all the amenities the system can offer: staterooms, dining rooms, movie rooms, a large car deck, etc. These vessels are the: M/V Columbia, M/V Malaspina, M/V Matanuska, M/V Kennicott, M/V Tustumena and M/V Taku.

A second class of vessel is the Aurora Class, which includes the M/V LeConte and M/V Aurora, both of which are 235 feet long and can transport 34 standard vehicles and up to 300 passengers. These vessels are designed to provide public transportation to smaller communities and fill in for mainliners when required and where possible. The vessels have the capability to operate 24/7 with multiple crews and crew quarters onboard. There are no staterooms available for travelers and food service is similar to what is available on mainline vessels. Currently the M/V Aurora operates with multiple watch crews in Prince William Sound and the M/V LeConte serves Lynn Canal and Icy Straits within the work/rest requirements for one crew.

The last vessel class is the “shuttle ferries.” These are home ported in one community, make a trip to another community and return each night for overnight moorage. The M/V Lituya, FVF Fairweather, FVF Chenega and the non AMHS entity Inter-Island Ferry Authority M/V Prince of Wales are examples of this vessel class. These vessels do not have staterooms for crew or passengers, are minimally crewed and are operated on routes where the sailing can be completed within 12 hours. The 12-hour criterion is important because operations over this length of time require additional crews and the inclusion of crew quarters and eating facilities add operating and capital costs. These vessels offer the most efficient service but are limited to their use in that they can only serve communities distanced less than 12 hours apart from the time the crew begins in the morning and ends their shift each night.

Brief History

On June 28, 2006 the Alaska Department of Transportation and Public Facilities (DOT&PF) issued a statement of services for a shuttle ferry class of vessel described as “Southeast Shuttle Ferry” with the purpose to:

“Select Naval Architecture and Marine Engineering Firm to modify a concept ferry boat design to meet Alaska Marine Highway Systems operational and performance requirements for a new class of Southeast Alaska Shuttle Ferries.”

The statement of services specified that the vessel design must meet the following criteria:

- Vessel Type: Roll On-Roll Off Passenger Ferry
- Overall Length: 255ft to 305ft

- Passenger Capacity: 450 (interior seating for 300 passengers)
- Vehicle Capacity: 48-60 plus
- Loading Ability: Bow, Stern and Side
- Cruise Speed: 18 knots (20 knot sprint speed)
- Operation: Day Boat Operations (12 hours)

This began the process toward building the Alaska Class Ferry (ACF), which would be the first AMHS stern/bow roll on-roll off (RORO) vessel since the M/V Bartlett. The stern/bow RORO would enable the most efficient vehicle loading and unloading capabilities. It was estimated in 2006 that the cost to build a vessel that met the above criteria to be approximately \$25 to \$30 million.

The naval architect firm, Elliott Bay Design Group, was selected to begin working with DOT&PF to design the vessel that would fulfill the criteria in the statement of services.

As the concept developed, there were several changes made that differed from the criteria in the statement of services. One of the most important changes was the elimination of a bow door, which decreased the ability of vehicles to roll on and roll off in an efficient manner. Less time in port and more time underway was an important characteristic for a Southeast Alaska Shuttle Ferry, especially for routes that were on the edge of being completed in less than 12 hours. A second major change to the concept design was the inclusion of crew quarters, which conflicted with the "day boat operations" specification in the original statement of services. The vessel was also lengthened to 350 feet during this process. The ACF Design Study Report was completed in 2009 and included these changes to the original vision of the vessel; the cost estimate increased to \$120 million.

The 2010 Alaska State Legislature appropriated \$60 million of state general funds toward building the first Alaska Class Ferry. The appropriation matched \$68 million in Federal Highway Administration funds. Later that year, Governor Parnell "defederalized" the ACF project and the department transferred approximately \$1.5 million that had been expended for design to other state transportation projects. Defederalizing the ACF project allowed the state more flexibility to choose where and how the ACF would be designed and constructed. This aligned with the Governor's and legislature's intent that the vessel be built in Alaska for Alaskan jobs. The federal funds were later redistributed to other transportation projects in Alaska; the funds were not lost or permanently sent back to the federal government.

The 2011 Alaska State Legislature appropriated an additional \$60 million to the project. DOT&PF continued working with Elliott Bay Design Group to design an ACF that could be built within the \$120 million appropriated budget.

In parallel with the ACF development, DOT&PF in 2007 hired the University of Alaska Fairbanks (UAF) to independently analyze the Alaska Marine Highway System. The study was published in 2011 and found that there was no improvement in the overall efficiency of the AMHS by replacing the M/V Malaspina in Lynn Canal with a 350-foot ACF. When two additional ACF's of this size were deployed (with the retirement of the M/V Taku) the study found that the average annual AMHS operating subsidy increased by approximately \$6.7 million. There was an improvement in service with deployment of two 350-foot ACF's in Lynn Canal, but at a substantial increased cost that resulted in the highest annual AMHS subsidy of any alternative UAF analyzed.

In late 2011 DOT&PF management was concerned that the vessel design had diverged away from the original design concept described in the statement of services. Instead of a stern/bow RORO shuttle class ferry, the design had morphed into essentially a large Aurora Class vessel. Rumors within the ship building industry also indicated that the vessel would exceed the \$120 million budget. At this time the department began altering and removing design features in an effort to reduce construction costs.

In April 2012 DOT&PF contracted with Alaska Ship and Drydock (ASD) designating the shipyard as the Construction Manager/General Contractor for the design phase of the ACF project. ASD operates the Ketchikan Shipyard under a long-term private/public partnership agreement with the Alaska Industrial Development and Export Authority (AIDEA). As part of the contract with DOT&PF, ASD would have the ability to submit the first price proposal for the construction contract as the design neared completion. If the price ASD submits is acceptable to DOT&PF a contract would be initiated with ASD to construct the vessel. If a price could not be agreed upon the state would advertise the project for competitive bids which would allow firms located outside of Alaska an opportunity to construct the vessel.

In fall 2012 the conceptual design had reached a point where accurate cost estimates could be provided by both the naval architect and ASD. These estimates showed the total project cost at \$150-\$167 million – 25-39 percent higher than the \$120 million appropriated and more than five times the original 2006 estimate.

The department was now faced with a vessel design that did not meet the original intent of constructing a stern/bow RORO shuttle ferry, a study provided by the University of Alaska that cast doubt on the use of the vessel, and a cost estimate that exceeded the amount available for construction.

Armed with this information the department consulted with the Governor and received direction to reevaluate the direction the project had taken. The vessel design and purpose were reviewed and the department determined that going back to the original concept was the best course of action for service to the public. Governor Parnell announced in December 2012 to revert the design back to a stern/bow RORO concept which will cost less to build and operate, and better serve Alaskans.

Change in Direction

The former ACF concept was a 350-foot ferry (about the same length as the M/V Taku) with a capacity of 60 standard cars, no passenger staterooms, and a crew of 23-28 with requisite crew quarters/galley.

The stern/bow RORO ACF will be, in comparison, a smaller vessel approximately 260-300 feet in length (longer than the Aurora Class) with a standard vehicle capacity around 50 cars. It will not have passenger or crew staterooms and will operate with fewer crewmembers than the former ACF concept. This ferry will be designed with stern and bow loading capabilities and possibly port/starboard loading doors. The new ACF's will have the capability to replace the M/V Malaspina as shuttle ferries operating in Lynn Canal between Juneau, Haines and Skagway. It is expected that with its length and a modified hull form, the vessel will have similar seakeeping characteristics as the M/V Taku. DOT&PF intends to initially build two Alaska Class Ferries within the appropriated budget.

Next Steps

DOT&PF is currently amending its contract with Elliott Bay Design Group to refocus the ACF design toward the stern/bow RORO vessel concept. Because of a less complex design, the department anticipates that both the design and construction times will be faster in comparison to the previous design concept.

Serving Southeast Alaska

Beside significant construction and operating cost savings, the stern/bow RORO Alaska Class Ferry provides the greatest frequency, versatility and capacity while also serving as a backup for other vessels.

Frequency

One possibility is to operate two shuttle ferries to supplement mainline service in Lynn Canal. One vessel could be home ported in Haines or Skagway and the other in Juneau. The northern ACF would shuttle between Haines and Skagway. The Juneau ACF would make one or two round trips per day between Juneau and Haines. Because these vessels would have stern and bow loading capabilities and operate to a single port, the load times at each port will be significantly less in comparison to other AMHS ferries and, in turn, provide for a more efficient use of operating hours per day.

Versatility

Another possibility is deploying the ferries to other Southeast communities when the traffic demand requires an additional vessel (i.e. community events, Celebration). The communities of Hoonah, Tenakee and Gustavus can all be served within a 12-hour timeframe to/from Juneau.

Capacity

A minimum of 200 standard vehicles could be transported each day between Juneau and Haines as needed during peak times - a total capacity capable of transporting 67 percent more vehicles than the former ACF concept transporting 120 standard vehicles per day.

Backup

With several smaller ferries, one ferry can be deployed to other Southeast communities when the M/V LeConte is not running due to required annual maintenance or breakdowns. The ACF's will also serve as a backup vessel to one another during scheduled and unscheduled maintenance days. This will allow AMHS to continue to provide a consistent level of service with the least amount of impact to the system as a whole.

Cost

Preliminary analysis of total costs – capital and operating – over the life of the new vessels indicates that two smaller ACF's can yield significant cost savings over the former ACF concept. This is due to differences in operating costs of different vessels, and the opportunity to scale the use of the vessels to the very big swing in capacity required between peak and off-season.

STATEMENT OF SERVICES

Southeast Shuttle Ferry Design Services

PURPOSE

Select Naval Architecture and Marine Engineering Firm to modify a concept ferry boat design to meet Alaska Marine Highways Systems operational and performance requirements for a new class of Southeast Alaska Shuttle Ferries.

VESSEL INFORMATION

Vessel Type.....Roll On-Roll Off (Ro-Ro) Passenger Ferry
USCG Classification.....Subchapter H
Classed.....American Bureau of Shipping (ABS) ✽A1, Ⓢ, ✽AMS, Ferry Service Passenger
Service.....Domestic Voyage
Service Route Type.....Lakes, Bays and Sounds
Southeast Alaska Stability Policy.....Exposed Routes
Length Overall.....255' to 305'
Beam.....56' to 74'
Draft.....10' to <16'
Passenger Capacity.....450 – Interior Seating for 300 Passengers
Vehicle Capacity.....48 to 60+
Vehicle Loading Ability.....Bow, Stern, and Side (Port & Starboard)
Cruise Speed.....18 Knots
Sprint Speed.....20 + Knots
Operation.....Day Boat Operations – 12 Hours
Manning Level.....Minimum Manning IAW Regulatory Requirements – Unmanned Engine Room
ADA Compliant.....Americans with Disabilities Act (ADA) with Passenger Elevator

DESCRIPTION OF THE PROJECT

The Naval Architecture and Marine Engineering Firm shall (1) provide concept design for new Southeast Shuttle Ferry, (2) develop and assemble, plans, specifications and estimate (PS&E) for competitive bidding, and provide design support during advertising and construction. The selected consultant may also serve as the Owner's Representative in meetings with the public, other agencies and other design firms.

MINIMUM QUALIFICATIONS

Firms must demonstrate experience in the design of projects of the same or greater magnitude and complexity as the Southeast Shuttle Ferry. The firm must have designed and provide plans, specification and estimate for at least one such vessel in the last five years.

SCOPE OF THE REQUIRED DESIGN SERVICES

The design phase of the project development spans all activities of preliminary vessel design through advertisement for construction. The design product will be plans, specifications and estimate (PS&E) assembly adequate to obtain competitive bids and detailed enough to construct the project. During the construction phase, the Consultant shall be available to provide engineering interpretation of the PS&E assembly and review of contractor submittals.

STATE OF ALASKA PROJECT ESTIMATE

Department of Transportation and Public Facilities

April 14, 2006

Federal project no: SHAK-9500(112)

State project no: 73073

Project Name: SEA Region Shuttle Ferry

ITEM	IMPROVEMENT TYPE	TOTAL EST'D COST	MATCHING RATIO	FEDERAL FUNDS	MILES
Requested Program and Agreement		APPN: <u>QK2</u>			
1) 10	15	251,000	100.00%	250,000	N/A

TOTALS

251,000

250,000

1) Includes \$1,000 non-participating and \$126,360 for Consultant.

STATE OF ALASKA PROJECT ESTIMATE

Department of Transportation and Public Facilities

December 17, 2010

Federal project no: SHAK-9500(112)

State project no: 73073

Project Name: Southeast Shuttle Ferry

ITEM	IMPROVEMENT TYPE	TOTAL EST'D COST	MATCHING RATIO	FEDERAL FUNDS	MILES
As Programmed and Agreement		<u>APPN: QK2</u>			
1) 10	15	2,501,000.00	100.00%	2,500,000.00	N/A

TOTALS	<u>2,501,000.00</u>	<u>2,500,000.00</u>
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1) Includes \$1,000 non-participating and Consultant for design study report \$198,830 and PS&E \$1,962,500.

Revised Program and Agreement		<u>APPN: QK2</u>			
10	15	0.00	100.00%	0.00	N/A
1) 11	15	2,500,000.00	0.00%	0.00	

TOTALS	<u>2,500,000.00</u>	<u>0.00</u>
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1) Non-participating

Governor Parnell

Southeast Conference has some concerns about the change in direction your administration has taken concerning the Alaska Class Ferry. From the inception of the Alaska Marine Highway System SEC has been a tireless advocate for its continued operation and success. We were strong supporters of the Alaska Class Ferry concept and what it would mean for the entire AMHS System. It is exciting for us to believe that the State of Alaska is working on replacing our aging fleet of ferries which are extremely critical to the economies of coastal Alaska Communities.

Some of our concerns with the latest shuttle ferry concept being proposed are listed below.

-The sudden change in direction to smaller shuttle ferries without any input from strong ferry advocacy organizations (MTAB, SEC)

-Safety, reliability, and comfort of smaller shuttle ferries in proposed routes

-Cost of replacing one large Alaska Class Ferry with 3 smaller shuttle ferries (without cost estimates for smaller vessels)

-Cost of operating 3 small shuttle ferries with crews several times a day compared to a daily ACF ferry that makes one round trip but that can handle expected loads. Operating three shuttle ferries continuously in routes where passenger and vehicle numbers vary widely from day to day, and month to month.

-Long term effects of proposed changes to ferry replacement program. It is well understood that our aging fleet of smaller day boats and larger mainliners are in serious need of replacement. The cost the proposed changes to the system will most likely preclude moving forward with a sensible vessel replacement program for routes outside of the Lynn Canal. It is unknown what these new shuttle ferries will eventually cost but the road portion of the shuttle system (Juneau Access) is estimated to be over 400,000,000\$. The 2014 STIP reflects only a portion of that (260,000,000\$) How do you propose to pay for the replacing our aging fleet and also pay for a project for one part of the system which will cost over ½ a billion dollars?

-The majority of the people in the communities of the upper Lynn Canal do not support the road shuttle concept, The reasons are mainly safety, reliability, and ease of travel. The mainline routes to the upper Lynn Canal are extremely important to the State of Alaska providing freight corridors, moving military families to the interior, and providing a good portion of the revenue for AMHS. It has recently been stated that under the road, shuttle concept the mainliners will turn around in Juneau.

-It is our concern that this change in direction does not have strong support throughout the effected routes and that the long term effects will diminish the Alaska Marine Highway Systems ability to move people and goods throughout the coastal areas of our great State.

SEC Board



THE STATE
of **ALASKA**
GOVERNOR SEAN PARNELL

Department of Transportation and Public Facilities

OFFICE OF THE COMMISSIONER
Pat Kemp, P.E., Commissioner

3132 Channel Drive
P.O. Box 112500
Juneau, Alaska 99811-2500
Main: 907.465.3900
Fax: 907.586.8365
dot.state.ak.us

January 9, 2013

Marine Transportation Advisory Board
Mr. Robert Venables, Chair
P.O. Box 50
Haines, AK 99827
Venables@aptalaska.net

Dear Chairman Venables and Board Members:

I am writing to inform you of my intentions regarding the filling of Department of Transportation and Public Facilities Deputy Commissioner vacancies. As you know, Alaska Statute Title 19, at Section 19.65.110. Powers, duties, and functions states:

(a) After the Commissioner of Transportation and Public Facilities has considered one or more candidates for the position of director or deputy commissioner of the Alaska Marine Highway System (AMHS) the commissioner shall confer with the board regarding that candidate or those candidates before making an appointment to that position. The selection of those candidates shall be without regard to political affiliation.

Under the previous Commissioner's organization, there was a Deputy Commissioner for each of the following: Marine Operations, Highways & Facilities, and Aviation. Under that organization the Deputy Commissioner, Marine Operations had as his sole responsibility the Alaska Marine Highway System. The statute required the Commissioner confer with the Board before making that appointment. I am changing the organizational structure such that my deputies for aviation and AMHS will have greater areas of responsibility and I anticipate this will be somewhat fluid as needs and priorities change. As such there will be a Deputy who has as part of his/her responsibility oversight and coordination of the AMHS, but there will be no Deputy Commissioner of Marine Operations. There will continue to be a Director of the Marine Highways, with the title of AMHS General Manager. This is similar to the situation with our

"Get Alaska Moving through service and infrastructure."

Regional Directors, who manage a geographic region of the state's highways, airports, harbors and other facilities with assistance /guidance from the Deputy Commissioners.

Given the reorganization as described above, with no Deputy Commissioner of Marine Operations, I do not intend to confer with the Board regarding the appointment of Deputy Commissioners. I will consult with the Board before appointing a new AMHS director should a vacancy in that position arise. I realize the statute and past organizational structure may create an expectation that there will be a Deputy Commissioner of AMHS and therefore I want to make my intentions clear before I make Deputy Commissioner appointments.

I look forward to working with you and the board, especially with the exciting change in direction with the Alaska Class Ferry project.

Sincerely,

A handwritten signature in black ink, appearing to be 'Pat Kemp', with a horizontal line extending to the right.

PKC
Pat Kemp, P.E.
Commissioner



MUNICIPALITY OF SKAGWAY

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P.O. BOX 415, SKAGWAY, ALASKA 99840
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January 4, 2013

Alaska Department of Transportation and Public Facilities
Division of Program Development, ATTN: STIP
P.O. Box 112500
Juneau, AK 99811-2500

RE: STIP Amendment 4

To Whom It May Concern:

This letter contains the Municipality of Skagway's comments on Amendment 4 of the 2013-2015 Statewide Transportation Improvement Program (STIP).

The most significant change in Amendment 4 affecting Skagway and the upper Lynn Canal region is the addition of more than \$260 million in FFY14 and FFY15 for Juneau Access: Glacier Highway MP 40.5-91.1. The present STIP through Amendment 3 contains less than \$50 million for this project. Most of the Amendment 4 money is slated for actual construction. (There is an error in the FFY 15 column for this project: the total should be \$100 million, not \$20 million.)

This allocation of large amounts of construction funds to a project with an unknown future is a troubling development. This is of particular concern given the controversial history of the Juneau Access road alternative. The Environmental Impact Statement (EIS) for this project is presently undergoing a supplemental review required by a 2009 Alaska District Court ruling that was upheld by the 9th Circuit Court of Appeals. That decision found the EIS inadequate because it did not thoroughly analyze enhanced ferry service. According to Alaska DOT & PF, the supplemental draft EIS will not be available for public comment until after July 2013.

The Municipality believes it is premature to include significant amounts of money in the STIP to construct the Juneau Access road alternative when the final EIS's preferred alternative is yet to be determined.

The Municipality asks that the allocation for the Juneau Access project in STIP Amendment 4 be reduced to the STIP Amendment 3 amounts.

The Municipality also asks that a portion of the state funds and the other state funds proposed for the Juneau Access project in Amendment 4 be allocated instead to the Alaska Class Ferry project (STIP Need ID: 24433) to cover the apparent budget shortfall in that ferry construction project.

The Municipality also asks that the comment period for STIP Amendment 4 be extended thirty days to February 7, 2013. To have a good portion of the comment period occur over the holiday season when many Alaskans are travelling out of state and occupied with the holidays diminishes the public's ability to comment on this important planning document.

An extended public comment period will also allow the opportunity to review and comment on the Governor's December 4, 2012, decision to change the Alaska Class Ferry project from construction of one vessel to two smaller vessels. This action raises major questions about the direction of the Alaska Class Ferry project, but gives the public no opportunity to review what factors went into the Governor's decision. The public must have an opportunity to review and comment on the issues raised by the Governor's unilateral decision to change the Alaska Class Ferry project.

Thank you for your consideration of these comments.

Sincerely,

A handwritten signature in cursive script that reads "Stan Selmer".

Mayor Stan Selmer
Municipality of Skagway

Rebecca Rooney

From: Brooke Elgie <brookeelgie@gmail.com>
Sent: Tuesday, January 15, 2013 8:37 PM
To: Rep. Peggy Wilson
Cc: Representative_Eric_Feige@legis.state.ak.us
Subject: ferries

The Governor's new ferry plan is **WRONG** on two counts:

It is **WRONG** to unilaterally discard years of ferry planning done by professionals and supported by public input.

It is also **WRONG** to suppose that open deck ferries will serve the needs of isolated bush communities like ours where a great deal of our basic supplies are brought in by ferry and where trips can be as long as 12 hours in horizontal rain and snow. Open deck baggage storage and no food service – in winter???

Please tell the Governor "NO"

Brooke Elgie

Wendy Stern

Tenakee Springs

Rebecca Rooney

From: Linda Kadrlik <adventuresafloat@gci.net>
ent: Wednesday, January 16, 2013 7:32 AM
To: Senator_Dennis_Egan@legis.state.ak.us; Rep. Peggy Wilson
Subject: Alaska Class Ferry

I am opposed to Gov. Parnell's decision to build two smaller ferries instead of the already approved Alaska Class ferry. Surely funding can be found to build this more appropriate ferry for southeast Alaska. Perhaps Gov. Parnell should try to get to Skagway by ferry during a winter storm, and then he would know why we need the Alaska Class ferry. Let me know how I can help. Linda

Adventures Afloat
Francis and Linda Kadrlik
4950 Steelhead, Juneau, AK 99801
(800) 3AFLOAT, (907) 789-0111
FAX: (907) 789-3312
<http://home.gci.net/~valkyrie>

Rebecca Rooney

From: Linda Kadrlik <adventuresafloat@gci.net>
ent: Wednesday, January 16, 2013 7:29 AM
To: Senator_Dennis_Egan@legis.state.ak.us; Rep. Peggy Wilson
Subject: Alaska Class Ferries

I am opposed to Gov. Parnell's decision to build two smaller ferries instead of the Alaska Class Ferry already approved. Surely we can find the money to fully fund that ferry instead of the smaller ferries that won't provide as much secure transportation. Maybe Gov. Parnell should ride the ferry up L

Adventures Afloat
Francis and Linda Kadrlik
4950 Steelhead, Juneau, AK 99801
(800) 3AFLOAT, (907) 789-0111
FAX: (907) 789-3312
<http://home.gci.net/~valkyrie>

Rebecca Rooney

From: Rob Goldberg <artstudioalaska@yahoo.com>
ent: Tuesday, January 15, 2013 9:43 PM
To: Rep. Peggy Wilson
Cc: Representative_Eric_Feige@legis.state.ak.us
Subject: Alaska Class Ferry

To: Alaska State House and Senate Transportation Committee Members

Re: Alaska Class Ferry

Last month, just before Christmas, my wife and I embarked on a trip to take our kids to Florida to see their grandparents. We were scheduled to leave Haines on a Friday, on the LeConte, in order to catch the jet out of Juneau on Saturday. The weather was bad all that week, with temperatures in the single digits, north wind gusting to 75mph, 15 foot seas in Lynn Canal and heavy freezing spray. Wednesday's run of the LeConte from Juneau to Haines was cancelled because of the dangerous conditions. Thursday morning we made a decision to go to Juneau a day early, on the Taku. It cost us an extra night in a hotel in Juneau, but we made our flight. Friday's run of the LeConte was cancelled.

The LeConte is 235 feet long. The Taku is 352 feet long. The difference in length and volume between the two ferries makes the difference between being able to operate safely in Lynn Canal in the winter, or canceling trips. It also makes the difference between everyone on board being sick and everyone being comfortable.

After much public input from people who actually depend on the ferry system, the new Alaska Class ferry was designed to be about the size of the Taku. Anything smaller will not be safe and reliable in Lynn Canal in the winter.

The governor's abrupt decision to cancel the Alaska Class ferry and replace it with two, smaller, open decked ferries is an insult to Alaskans who need the ferries to travel in the winter. They simply will not work. The state will be repeating the mistake made when the Fairweather and the Chenega were tried in the Lynn Canal.

I don't like to attribute motive, but the governor's two shuttle ferries are suspiciously similar to the ones proposed by DOT for the East Lynn Canal road. If the governor wants to kill the ferry system and build a road, he should just say so.

I know a bit about the proposed East Lynn Canal road. I wrote the Haines Borough's comments on the draft EIS years ago. I'm also one of the few people who has actually had boots on the ground over there, on mountaineering trips. I can tell you with certainty that the road would be vastly more expensive than advertised. The terrain challenges are enormous. A road would be dangerous and closed by avalanches much of the winter. Any shuttle ferry would have to be the size of the Taku, or the Alaska Class ferry, to be able to operate safely in the winter between the Katzehin terminal and Haines and Skagway.

If we need an Alaska Class ferry to be the shuttle, why build a road? It would be a huge waste of money. Access to Juneau would be made worse, not improved.

I urge you to tell the governor to restore the Alaska Class ferry. His two smaller ferries just won't work.

Sincerely,
Rob Goldberg
Haines

Rob Goldberg and Donna Catotti
Catotti and Goldberg Art Studio
PO Box 1154 Haines, AK 99827 USA
907-766-2707
artstudioalaska.com

Rebecca Rooney

From: H Unruh <unruh5832@yahoo.com>
Sent: Tuesday, January 15, 2013 4:58 PM
To: me
Subject: Re: Proposed ferries for Lynn Canal

Dear Alaska Representatives & Senators,

It is my understanding that Governor Parnell recently made a unilateral decision to change the direction of the new Alaska class Ferry----against the wishes of the SE Legislators. His preference for smaller, partially open-deck ferries to operate in Lynn Canal seems to be based on ?????? There are two years of legislative review and public process (people who actually depend on ferry travel) which concluded that large, safe and reliable ferries are what is needed.

Anyone who has been on a ferry in inclement weather would probably not want to depend on a smaller partially open-deck ferry!

I hope you will fight for reasonable, safe, reliable ferries. Perhaps send the governor & his friends on one of the smaller ferries up Lynn Canal during a storm.

I hope you will resist the Governor and Department of Transportation Commissioner Pat Kemp as they attempt to circumvent the public process. In all cases, but especially when it may mean life or death to those who depend on the ferry system for transportation.

Thank you for taking a stand.

Helen Unruh

Rebecca Rooney

From: Robert Andrews <andrews@aptalaska.net>
ent: Tuesday, January 15, 2013 5:06 PM
To: Rep. Peggy Wilson
Subject: Ferries

Governor Parnell's unilateral decision to scrap the Alaska Class ferries and, instead, pursue some scaled-down open deck vessel is completely at odds with the public process and does not consider the needs of winter-traveling Alaskans. Our ferries need to be safe and reliable in all weather conditions, both summer and winter. Even if these Alaska Class vessels exceed the original cost estimate, they will be worth it in the long run with added dependability.

It is not the governor's place to take it upon himself to replace a program that has been well thought out and arrived at with due public process.

Sincerely,
Robert A. Andrews



Municipality of Skagway

GATEWAY TO THE KLONDIKE

P.O. BOX 415 SKAGWAY, ALASKA 99840

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WWW.SKAGWAY.ORG

January 18, 2013

The Honorable Dennis Egan
State Capitol, Room 9
Juneau, AK 99801-1182

The Honorable Beth Kerttula
State Capitol, Room 404
Juneau, AK 99801-1182

Dear Senator Egan and Representative Kerttula:

The Municipality of Skagway is concerned about Governor Sean Parnell's recent decision to change the scope of the Alaska Class Ferry (ACF) project from construction of one vessel to the construction of two, smaller vessels. The decision raises questions but provides few answers about the direction the Alaska Marine Highway System (AMHS) will follow as the state upgrades the AMHS fleet to best serve our communities.

The Municipality's most immediate concern is that the decision was made without any apparent public involvement. The Alaska Class Ferry enjoyed widespread support among communities and local decision makers. Yet, the Marine Transportation Advisory Board, elected officials from communities that rely on the AMHS, regional organizations such as Southeast Conference and the public at large were denied the opportunity to review and comment on the proposed change.

The Municipality applauds the decision of the Senate and House Transportation Committees to conduct public hearings on this important development. We respectfully request that you investigate the following issues:

1. **SAFETY:** Lynn Canal is well known for its high winds and rough seas. Will safety considerations be compromised by the proposed features of smaller hull size, partial open deck and bow doors?
2. **DEMAND:** Will the new smaller ships be able to meet the capacity demand in the Lynn Canal corridor? What will be the impact on our local economies of reduced capacity, in particular when the 12-hour rule is implemented?

3. CONSTRUCTION COST: Without a design, how does the Department of Transportation and Public Facilities know that the construction cost of two boats will not exceed the cost of one ACF taking into consideration costs of redundancy such as twice the number of engines, radars, generators, etc.?
4. OPERATING COST: How do operating costs pencil out when expenses such as fuel and crews are doubled and two boats must be maintained, rather than one?
5. VIABILITY: A recent memo from Commissioner Pat Kemp stated that the position of Deputy Commissioner of Marine Operations will be eliminated from the Department of Transportation and Public Facilities. The Municipality of Skagway is concerned about the perception that the department is planning to dismantle the Marine Highway.

Thank you for your consideration.



Stan Selmer
Borough Mayor

cc: Representative Peggy Wilson, Chair, House Transportation Committee
Representative Jonathan Kreiss-Tomkins, House Transportation Committee
Representative Cathy Muñoz, House Finance Committee
Governor Sean Parnell
U.S. Senator Mark Begich
U.S. Senator Lisa Murkowski
U.S. Representative Don Young
Marine Transportation Advisory Board
Southeast Conference



435 Airport Rd, PO Box 320, Hoonah, AK 99829
(907) 209-4806 phone (907) 523-2966 fax

January 25, 2013

Senator Dennis Egan, Chair, Senate Transportation Committee, via Fax #907 465-2108

Representative Peggy Wilson, Chair, House Transportation Committee, via Fax # 907 465-3175

Subject: Public testimony on the Alaska Marine Highway System

Dear Senator Egan & Representative Wilson & Committee members,

Thanks for the opportunity to testify earlier today. I am unsure as to whether I was articulate enough to appropriately convey my thoughts on the current Ferry situation. I could not go to bed with clear conscience without at least trying to clarify my thoughts.

When I started the teleconference I went in upset at the heavy handedness of the Administration and convinced that the result was undoubtedly going to be inadequate replacement Ferry's. While listening it seemed clear to me that, by design the checks & balances in our political system were working. Proper decision making always needs to include critical thinking. It sounded like there was some sort of coup at the DOT and I for one am always praying for Government service delivery to become more efficient and effective. However, we are talking about public safety. The Fast Ferry's may seem like a wonderful State asset but I am confident that with a close examination they would quickly prove to be one of the worst decisions the AMHS has ever made. Please, do not let them do anything like that again...

I use the Ferry system probably 20 times a year. It would probably be 100 times a year if the scheduler had an adequate number of vessels to schedule. Smaller more agile ferry's make good sense to me... But they absolutely have to be made with good steel and not have open decks. I have seen grown AMHS passengers crying, as they feared for their life, because of winter storms. Please, make sure that DOT starts producing some documentation on what this change of course really means. It seems to me that the Alaska class Ferry design is quite similar to the LeConte and I would take that ferry anywhere.

Helping to establish a shipbuilding capability is also just plain smart. We could certainly use the employment and those skills. Whatever the final design ends up being please make sure that it is safe and built in our State. I do not particularly care where in Alaska it's built but Ketchikan seems most logical. Thank you very much for re-establishing public input and whatever happens please recognize that the need for more Ferry's is extremely urgent and the Alaska class Ferry design was quite well thought out...

Sincerely,

Ed Phillips

Owner

1/24/13

Madame Chair Rep. Wilson, Senators and Representatives, thank you for this opportunity to speak today. My name is Mim McConnell and I humbly serve as mayor of the City and Borough of Sitka.

First, I would like to thank Captain Mike Neussl for his time of service at the helm of the Alaska Marine Highway System while Deputy Commissioner of marine operations. I never had the opportunity to meet this gentleman but have heard nothing but good reports concerning his time of service.

Now for the matter at hand, the Alaska Class Ferry Project.

The City and Borough of Sitka and citizens of Sitka have many questions concerning the Governor's decision to abandon the Alaska Class Ferry Project. As one of the many communities whose economy is heavily reliant on Alaska Marine Highway service, Sitka has strongly supported the efforts of the Marine Transportation Advisory Board and many Alaskans who have advocated for getting the Alaska Class ferry on line as a start to replacing Alaska's aging fleet of mainline vessels.

The sudden change in the State's direction to proactively develop a new generation mainline vessel and replace it with three smaller shuttle ferries does not appear to achieve this mission. Shuttle ferries will not be able to serve Sitka, do not achieve the intent of the Alaska Class ferry project, and were committed to without any Marine Transportation Advisory Board or public input. We've received some information about how the proposed shuttle ferries will be used and why the Alaska Class ferry project was canceled. What all communities using the Alaska Marine Highway System would appreciate is learning is how the mission for the Alaska Class ferry will be met, and having the opportunity to comment before such important decisions are finalized.

I hope that the Marine Transportation Advisory Board will be allowed to thoughtfully consider this issue and comment, and that this proposal will be opened up to public involvement before the decision to abandon the Alaska Class ferry project is finalized. Alaska Marine Highway's future could be enormously disadvantaged by not developing an Alaska Class mainline vessel to transition to a more reliable future mainline system. Thank you for listening.

Mim McConnell, Mayor
City and Borough of Sitka
100 Lincoln St, Sitka, AK 99835



THE STATE
of **ALASKA**
GOVERNOR SEAN PARNELL

Department of Transportation and
Public Facilities

OFFICE OF THE COMMISSIONER
Reuben Yost, Deputy Commissioner

3132 Channel Drive
P.O. Box 112500
Juneau, Alaska 99811-2500
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Fax: 907.586.8365
dot.state.ak.us

October 24, 2013

Senator Dennis Egan
State Capitol, Room 9
Juneau, Alaska 99801

Dear Senator Egan,

Thank you for your letter of October 11, 2013 requesting information on the possible use of Liquefied Natural Gas (LNG) as a fuel source for future Alaska Marine Highway System (AMHS) vessels. As you know, AMHS is not only in the functional design phase of the new Day Boat Alaska Class Ferry (ACF) but has also begun the preliminary design process for an M/V Tustumena replacement vessel. You are correct in surmising that we are thinking about the potential use of LNG for these vessels.

AMHS has specified that the design study report evaluate the possibility of using LNG to power the Tustumena replacement vessel. The Tustumena is homeported in Homer, which has LNG availability. The current design for the ACF ferries does not include initial use of LNG, as LNG for vessel fueling is not currently available in Juneau, Haines or Skagway. The ACF design will not preclude the vessels being retrofitted for LNG in the future. The ACF design calls for the main engines to be the Electro-Motive Diesel (EMD) 12-710 Tier III, which can be easily converted to LNG. EMD engine technology currently includes Dynamic Gas Blending (DGB), which allows LNG use and reduces air emissions. DGB provides for either 100% diesel use or up to 80% substitution with LNG. EMD's current estimate for converting this engine model to LNG is \$400,000; conversion could reduce fuel costs as much as 43% annually.

Fuel handling and storage is different for LNG. AMHS has pursued this issue with the ACF contractors, Elliott Bay Design Group (EBDG). Another EBDG design team is currently working with Staten Island Ferries on the conversion of some of their vessels to LNG, so they are familiar with the process. EBDG has considered the additional adaptations of the ACF vessel design that would be necessary for future LNG use including storage tanks, vaporizer equipment, double wall piping, gas detection alarms and ventilation requirements. For instance, the United States Coast Guard requires LNG fuel storage tanks on a passenger vessel to be placed on a weather deck (open to the air), with no passenger deck above. The best location for the necessary double wall LNG storage tanks would be where the solarium is currently located, as this would provide the lowest center of gravity and avoid creating stability issues. If the vessel was to be converted it would be relatively easy to move the solarium space to the open space aft of the crew day room.

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In summary, the design process for the M/V Tustumena replacement vessel will include consideration of immediate LNG use as well as the potential for future conversion if not initially selected. The ACF design does not include immediate use of LNG, but the potential for future conversion is being considered to avoid design features that could preclude it.

Thank you for your kind words regarding the ongoing design process for the ACFs. I look forward to sharing our progress with you as we near final design and construction.

Sincerely,

A handwritten signature in black ink, appearing to read "Reuben Yost", with a stylized flourish extending to the right.

Reuben Yost
Deputy Commissioner



LAWS OF ALASKA

2010

Source

HCS CSSB 230(FIN) am H

Chapter No.

AN ACT

Making and amending appropriations, including capital appropriations, supplemental appropriations, and other appropriations; making appropriations to capitalize funds; and providing for an effective date.

BE IT ENACTED BY THE LEGISLATURE OF THE STATE OF ALASKA:

THE ACT FOLLOWS ON PAGE 1

1 obligation bonds for library, education, and educational research facilities for the fiscal year
2 ending June 30, 2011.

3 * **Sec. 37. DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES.** (a) The
4 sum of \$60,000,000 is appropriated from the Alaska marine highway vessel replacement fund
5 (AS 37.05.550) to the Department of Transportation and Public Facilities for construction of
6 an Alaska marine highway Alaska class ferry.

7 (b) It is the intent of the legislature that the new Alaska class ferry be constructed at a
8 shipyard facility located in the state unless the commissioner of transportation and public
9 facilities

10 (1) determines in writing that there is no shipyard facility located in the state
11 that is equipped or qualified to perform the work required; or

12 (2) after evaluating the costs and benefits of constructing the vessel in the
13 state, taking into consideration the Alaska bidder preference set out in AS 36.30.170, the
14 interport differential, and the cost implications of foregoing federal aid, determines that
15 construction at a shipyard facility located in the state is not in the best interest of the state.

16 (c) Section 58(c), ch. 3, FSSLA 2005, as amended by sec. 20(p), ch. 30, SLA 2007, is
17 amended to read:

18 (c) Subject to sec. 62, ch. 3, FSSLA 2005, the sum of \$40,300,000 is
19 appropriated from the Alaska capital income fund to the Department of Transportation
20 and Public Facilities for the following projects in the amounts stated, subject to
21 reallocation among the projects under AS 37.07.080(e):

22	PROJECT	AMOUNT
23	Alaska marine highway <u>facility in</u>	\$5,000,000
24	<u>Ward Cove</u> [ALL-TIDE LAY-UP	
25	FACILITY]	
26	DeLong Mountain, port expansion EIS	500,000
27	completion	
28	Denali Borough, Stampede Road Improvements	200,000
29	Industrial roads	3,300,000
30	King Cove Road	2,000,000
31	McCarthy Road, major maintenance	2,000,000

1	Nome warm storage building construction	1,000,000
2	North Slope, Foothills West Road EIS	3,000,000
3	North Slope, Bullen Point EIS	4,500,000
4	Dalton Highway, surfacing upgrades	6,000,000
5	Dalton Highway, Washington Creek Bridge	3,500,000
6	Parks Highway reconstruction	1,800,000
7	Richardson Highway, highway passing lanes,	5,000,000
8	Milepost 265 - 341	
9	Richardson Highway, Shaw Creek Bridge	2,500,000

10 (d) Section 1, ch. 159, SLA 2004, page 40, lines 30 - 32, is amended to read:

11 ALLOCATIONS

12	Alaska Marine	25,000,000
13	Highway: <u>Alaska</u>	
14	<u>Class Ferry</u> [NEW	
15	FAST VEHICLE	
16	FERRY #3] (ED 1-5)	

17 (e) Section 1, ch. 159, SLA 2004, page 40, line 33, through page 41, line 4, is
18 amended to read:

19 ALLOCATIONS

20	Alaska Marine	43,000,000
21	Highway: <u>Alaska</u>	
22	<u>Class Ferry</u> [NEW	
23	FAST VEHICLE	
24	FERRY #4] (ED 1-5)	

25 * **Sec. 38.** OFFICE OF THE GOVERNOR. (a) The sum of \$1,700,000 is appropriated from
26 the general fund to the Office of the Governor for the development of an in-state natural gas
27 pipeline for the fiscal year ending June 30, 2010.

28 (b) The sum of \$6,500,000 is appropriated from the general fund to the Office of the
29 Governor for the development of an in-state natural gas pipeline for the fiscal year ending
30 June 30, 2011.

31 * **Sec. 39.** HOUSE DISTRICT 2. Section 16, ch. 29, SLA 2008, page 181, lines 22 - 24, is



LAWS OF ALASKA

2012

Source

HCS CSSB 160(FIN) am H

Chapter No.

AN ACT

Making and amending appropriations, including capital appropriations, supplemental appropriations, and other appropriations; making appropriations to capitalize funds; and providing for an effective date.

BE IT ENACTED BY THE LEGISLATURE OF THE STATE OF ALASKA:

THE ACT FOLLOWS ON PAGE 1

	Appropriation	General	Other
	Allocations	Funds	Funds
1			
2			
3	*****	*****	
4	***** Department of Transportation and Public Facilities *****		
5	*****	*****	
6	88th Avenue Upgrade -	800,000	800,000
7	Spruce Brook Street to		
8	Elmore Road (HD 16-32)		
9	Alaska Marine Highway	60,000,000	60,000,000
10	System - Alaska Class		
11	Ferry (HD 1-40)		
12	Anchorage - Johns Road and	4,000,000	4,000,000
13	Klatt Road Intersection		
14	Design and Build (HD		
15	16-32)		
16	Anchorage - Midtown Traffic	10,000,000	10,000,000
17	Congestion Relief (HD		
18	16-32)		
19	Chena Small Tracts Road	298,000	298,000
20	Bicycle & Pedestrian		
21	Facility (HD 7-11)		
22	Cooper Landing - Walkable	550,000	550,000
23	Community Project (HD		
24	33-35)		
25	Fairbanks Metropolitan Area	7,500,000	7,500,000
26	Transportation System		
27	(FMATS) - Transportation		
28	Improvement Program (HD		
29	7-11)		
30	Greer Road Surfacing (HD	150,000	150,000
31	33-35)		
32	Intersection Improvements	100,000	100,000
33	Study - Caribou and		



THE STATE
of **ALASKA**
GOVERNOR SEAN PARNELL

Department of Transportation and Public Facilities

OFFICE OF THE COMMISSIONER
Patrick J. Kemp, P.E., Commissioner

3132 Channel Drive
P.O. Box 112500
Juneau, Alaska 99811-2500
Main: 907.465.3900
Fax: 907.586.8365
dot.state.ak.us

January 22, 2013

The Honorable Dennis Egan
Chair, Transportation Committee
State Senate
Alaska State Capitol
Juneau, Alaska 99801-1182

The Honorable Peggy Wilson
Chair, Transportation Committee
House of Representatives
Alaska State Capitol
Juneau, Alaska 99801-1182

Dear Senator Egan, Representative Wilson, and members of the House and Senate Transportation Committees:

Recently you received a communication from a Haines resident that makes several allegations about the recent change to the Alaska Class Ferry (ACF). In order to facilitate a more thorough response to your constituent, I would like to share some additional factors for consideration.

The main three points that I would ask you to consider while evaluating her concerns are issues of public process, the need for a 350-foot ferry for Lynn Canal, and the associated costs.

The email from Senator Robin Taylor on January 11 was the first contact I have had with him since June of 2006. Senator Taylor has not offered advice to the change of the scope of the ferry nor was he consulted for advice.

Deputy Commissioner Reuben Yost has oversight of the Alaska Marine Highway System (AMHS) for the Department of Transportation and Public Facilities but, as described in my testimony at the January 17 Joint Transportation Committee meeting, I have eliminated the assignment of a transportation mode to describe the department's deputies. The department's three deputies and the commissioner will provide executive oversight for the department but will not be involved in the day-to-day management of a division or a system; that responsibility now rests at the director level. In the case of AMHS, that individual is Captain John Falvey and he will manage and have complete authority over the system from his Ketchikan office.

The ACF shuttles will be designed to handle the conditions in Lynn Canal, as well as other routes in Southeast Alaska. The vessel size will be based on the capacity needs of various routes. As I mentioned at the meeting last week, we are preparing a Design Concept Report that will address these and all other major aspects of the two-shuttle concept. While public involvement is important information for the

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design process, the department needs to develop the vessel criteria using marine architects, based on marine engineering science, weather conditions, United States Coast Guard (USCG) regulations, masters input, etc. These parties are very well versed in regard to vessel length. These vessels will have the same or better sea keeping characteristics than the Motor Vessel (M/V) Taku, which rarely misses a scheduled sailing anywhere in the system.

Capital and operating costs concerns are never a "strawman" and particularly not given the state's current financial projections. The estimated cost of the previous Alaska Class Ferry far exceeded the funding available and that cost estimate was anticipated to grow even higher. Governor Parnell was very concerned with escalating costs and about a year ago warned the department to modify the vessel's design to bring it within budget or provide an alternative concept that would fulfill the vessel's intended mission. The department subsequently attempted to reduce costs by eliminating or modifying certain features but it was clear that even with significant modifications, the cost of this particular design could not be built for \$120 million.

As I testified, the original vessel concept in 2006 was to be a roll-on, roll-off with a bow door and would be operated as a day boat. Over a several year period the design morphed from that concept to a 350' vessel that can best be described as a class between an Aurora size and a mainliner (Malaspina, et.al.). The public involvement process was a factor in this morphing and it appears that budgetary constraints were not a significant factor as the scope of the vessel changed.

After determining that the 350' design could not be delivered within budget, the department next reviewed the needs for the system and concluded that the best course of action was to return to the original concept of constructing a roll-on, roll-off vessel. In 2006, the estimated cost to construct such a vessel was \$30 million. With inflation and other factors, we believe two of these vessels could be constructed within the \$120 million appropriated.

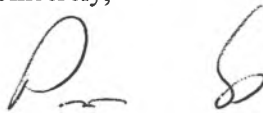
The Governor reviewed the information and made what I consider to be a very forthright decision by directing the Department to return to the concept envisioned in 2006. Fiscal considerations were, indeed, a significant factor for the Governor as well as his desire to have an Alaska ferry built in Alaska at a reasonable cost.

Returning the scope of this project to that originally envisioned, a day boat shuttle, will reduce capital costs and annual operating costs. A soon-to-be published Design Concept Report will provide a cost estimate for the ACF shuttles and the terminal modifications required. The ACFs will have an estimated useful life of approximately 60 years. Also, as explained, no funds were returned to the federal government in order to build the ACFs in Alaska. The federal funding originally programmed for the ACF were used for other Alaska transportation projects.

I have no idea what Ms. Menke is describing as fungible costs and the allegation that the department manipulates those estimates. The department always includes development and overhead costs in project estimates.

I trust this response will address many of the concerns that surround the change to the ACF concept. I look forward to providing additional information as it becomes available.

Sincerely,



Patrick J. Kemp, P.E.
Commissioner

Cc: Senator Dennis Egan
Vice-Chair Senator Fred Dyson
Senator Click Bishop
Senator Anna Fairclough
Senator Hollis French
Representative Peggy Wilson
Representative Doug Isaacson
Representative Craig Johnson
Representative Eric Feige
Representative Bob Lynn
Representative Lynn Gattis
Representative Jonathan Kreiss-Tomkins



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Fire Dept. 907.766.2155 • (fax) 907.766.3373

December 21, 2012

The Honorable Sean Parnell
Governor of Alaska
P.O. Box 11001
Juneau, Alaska 99811-0001

Dear Governor Parnell:

Governor Parnell, your decision, announced on December 4, to abandon the ongoing Alaska Class Ferry project in favor of smaller shuttle ferries raises many questions. As Mayor of Haines, I have been inundated with comments, questions, and requests for information from Haines residents. As to the questions and requests, I am as bereft of information as any of my constituents. I cannot overemphasize the importance of this issue to my constituents.

The Haines Borough Assembly will convene on Tuesday, January 8, 2013, and again on January 22nd. Sometime during our January schedule, I expect that the assembly will wish to express a more formal opinion on this issue. For that reason, I respectfully ask that you and your staff provide me with whatever of the following information that it is possible to assemble and transmit by early January, so that assembly members will be able to incorporate the information into their thinking. I have tried to formulate questions that I think are key to our perception of the course change, but please do not hesitate to confine your response to my questions. Please feel free to tell us what you believe we need to know in order to think clearly and carefully about your proposal.

First, we are all very curious to know how the decision was made. Your press release states that the decision was made "after it became apparent ... that building a 350-foot ferry, on budget, in Alaska, could not be accomplished." Where did these cost figures come from? Who participated in this decision?

Would, or should, the new plan be subject to legislative approval?

Why did you not follow established procedures for decisions about Alaska's ferries? The Marine Transportation Advisory Board was established in 2003 and adopted in statute in 2009.

It is a part of your Department of Transportation. The MTAB was taken completely by surprise by this decision. As far as I can discover, the Ketchikan shipyard was taken by surprise by this decision, as were Southeast Alaska legislators. It looks to me as though your Deputy Commissioner for Marine Operations, Captain Michael Neussl was also surprised. Many Haines residents feel that circumvention of the public process is one of the most important issues implicated by your decision. How can I reassure them that participation in the public process is not in vain?

If there is no design established yet for the two shuttle ferries your new plan contemplates, how can you make the statement:

By setting a new course, Alaskans can build two smaller Alaska Class Ferries and stay on budget, and at the same time provide the same or better level of service Alaskans expect from our marine highways.

[Press release, Governor's Office, December 4, 2012]

What level of service do you believe is appropriate for Upper Lynn Canal? By that I mean, how many cancellations or seasickness-inducing sailings are you planning for? The *Southeast Alaska Transportation Plan Shuttle Ferry Study*, prepared in January of 2010, notes that

If the vessel is sized to the traffic demand, AMHS must ask of their customers whether a reduced reliability of service is acceptable, especially during the winter months. If a vessel can make 99 out of 100 scheduled trips in the summer, but only 80 out of 100 trips in the winter, does that reach an acceptable level of service? How should the ferry compare with other publically funded transportation such as the highways? These are policy questions, not design questions.

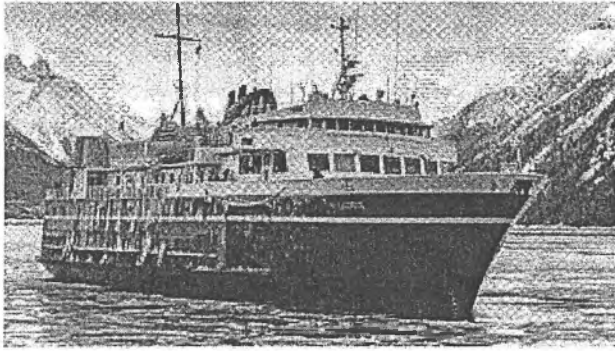
[Study, page 21]

That study discusses wave conditions and wind speeds in various areas in Southeast. While I am not familiar with the exact terminology used, I question whether the Elliot Bay group properly assessed conditions in Upper Lynn Canal. The wave heights throughout the study appear to be underestimated.¹ The LeConte was cancelled twice this week due to 11-foot seas. The past several weeks have seen sustained winds in the 55-knot range, with higher gusts. While the LeConte (235') was cancelled Wednesday and Friday, the Taku (352') sailed Thursday, in essentially the same weather.

What connection, if any, does this change bear to the proposed Juneau Access Road?

What is the conceptual design for the shuttle ferries? Is it true that you are considering an at least partially open car deck? [Did you see this recent picture of the LeConte in Upper Lynn Canal?]

¹ It is also possible that sea conditions need to be reexamined in light of research that indicates that wind speeds and wave height will increase with global warming. See <http://www.researchgate.net/publication/50850792> Global trends in wind speed and wave height



Your DOT Commissioner (Acting) Pat Kemp told me that his department is "going to assemble a paper that discusses the information and key points that led to the decision." [Email to me, Dec. 11, 2012] Can you tell me when this study will be available?

Finally, may I meet with you or your representative soon, in person or via teleconference, about our concerns? Thank you.

Sincerely,

A handwritten signature in cursive script that reads "Stephanie K. Scott". The signature is written in dark ink and is positioned above the printed name.

Stephanie K. Scott
Mayor, Haines Borough

Cc: Senator Bert Stedman
Senator Dennis Egan
Representative Bill Thomas
Representative-Elect Jonathan Kreiss-Tomkins
Representative Cathy Munoz
Representative Peggy Wilson
Commissioner Pat Kemp
Deputy Commissioner Captain Michael Neussl
Robert Venables, Chair, Marine Transportation Advisory Board
Shelly Wright, Executive Director, Southeast Conference
Stan Selmer, Mayor, Municipality of Skagway



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Police Dept. 907.766.2121 • (fax) 907.766.2128
Fire Dept. 907.766.2115 • (fax) 907.766.3373

Stephanie Scott,
Mayor

January 16, 2013

Jerry Lapp,
Deputy Mayor

Dear Senator Egan and Representative Wilson,

Dave Berry Jr.,
Assembly Member

Thank you for convening a joint meeting of your respective Transportation Committees to inquire into the Administration's proposal to alter the plan to build the Alaska Class Ferry. The attached questions were collectively prepared by the Haines Borough Assembly during a special meeting convened on January 15, 2013, and we respectfully request the Committee's assistance in getting answers to them.

Steve Vick,
Assembly Member

Debra Schnabel,
Assembly Member

Joanne Waterman,
Assembly Member

Norman Smith,
Assembly Member

The plan to build and deploy the Alaska Class Ferry was developed with all stakeholders involved. In fact, a little over a year ago, the process was lauded by Commissioner Luiken at the December 15, 2011 Anchorage meeting of the Marine Transportation Advisory Board (MTAB).¹ We hope that you can bring to light the information that we need in order to fully evaluate this change of plan. We also hope that you can restore the public to its role through MTAB in the planning process.

Mark Earnest,
Borough Manager

Thank you for your work.

Julle Cozzi,
Borough Clerk

Sincerely,

Michelle Webb,
Deputy Clerk

Stephanie Scott
Mayor, Haines Borough

¹ According to meeting minutes, Commissioner Luiken "thanked the board and DOT & PF officials for their stable and persistent work on the Alaska Class Ferry project. He said it demonstrated the excellent team effort made working with the design company and Alaska Ship and Drydock. He said that the partnership with the design company (Elliott Bay Design Group in Seattle) and Alaska Ship and Drydock are a good example of how the process can work by bringing together two groups for the purpose of building a superior ferry."

Questions to assist the Alaska State House and Senate Transportation Committees inquiry into Governor Parnell's proposed replacement of the Alaska Class Ferry with two smaller vessels, prepared by the Haines Borough Assembly

I. Service Standard in Southeast Alaska: frequency, versatility, capacity, and backup

- a. Demonstrate how the plan to use the smaller ferries meets the need for versatility (i.e. the Prince Rupert run).
- b. The Alaska Class Ferry was suited to serve the entire southeast region. Explain how the plan for two smaller ferries supports the need for backup service should other vessels experience mechanical problems, and can add flexibility to the system when special community events require greater access. What is the envisioned operating region of the new plan?
- c. How will the new plan address the needs for service in the Lynn Canal during periods of inclement weather, especially high seas?

II. The Role of the Marine Transportation Advisory Board (MTAB)

- a. Wasn't the purpose behind the MTAB public process to get the design "right"? Wasn't the goal of the process to match the vessel to the need? MTAB identified the actual need and the proper solutions were articulated in the form of a concept design. Shouldn't the focus be on funding the right tool for the job, as opposed to changing the tool?
- b. What is the role of the MTAB in the Administration's new plan?
- c. How does the State's plan for the role of MTAB align with AS 19.65.180 (C) with respect to developing a strategic plan for the Alaska Marine Highway?

III. The Proposed Design for the Two Smaller Ferries to Replace the Alaska Class Ferry

- a. There is a renewed focus on bow doors. While bow doors are said to offer great efficiency of roll-on/roll-off operation, the need to seal things properly to provide sufficient watertight integrity may result in significant construction and operation costs. There have been a couple of serious life-taking ferry accidents in the Baltic - all related to bow door failures.
- b. Does not the proposed design, stern/bow roll-on/roll-off (RORO) require a specialized loading dock? Is this factor considered in the cost/benefit analysis of the new plan?

- c. A partially opened car deck configuration has been referenced in earlier discussions. Will this be safe for the proposed routes? If it is deemed unsafe, how will the change affect the cost of construction for the two smaller ferries?

IV. Funds/Cost: The purpose of the new plan is to control costs. How will it achieve this? "With declining oil production and declining state revenue, we have to be smarter with the people's money while meeting Alaskans' marine transportation needs." (December 4, Press Release from Governor Parnell announcing new direction.)

- a. We understand that the Alaska Class Ferry design was 35% complete, and that thus the cost estimates were in the same preliminary state. Provide us with the information that led to your conclusion that the AK Class Ferry would run over budget.
- b. To what level have the smaller ferries suggested as an alternative to the Alaska Class Ferry been designed? To what level has the cost of construction been estimated? Will you please provide us with the design and cost estimate documents?
- c. Provide the estimates of the operation and maintenance for one large Alaska Class Ferry, that makes one round trip but that can handle expected loads compared to the cost of operating 3 small shuttle ferries with crews several times a day.
- d. According to Commissioner Kemp's December 20 report, the decision to build two smaller ferries instead of the Alaska Class Ferry is based in part on a prediction in a report of "a substantial increased cost that resulted in the highest annual AMHS subsidy of any alternative UAF analyzed" (page 2, Commissioner Kemp, 12/20/12). The report is based on AMHS data from 2006. Why do you have such confidence in a report based on 6-year-old data, knowing that utilization (both commercial and non-commercial) has increased in the interim?
- e. How do you read the following sections of the UAF report that connect the highest increase in subsidy to a ferry-road combination as opposed to the replacement of the Malaspina by an Alaska Class Ferry?

Under Option 1B (Malaspina is replaced by an Alaska-Class shuttle ferry):

AMHS' financial performance is only slightly worse than the status quo (Option 1A) (p.189)

Profitability index "is statistically identical to that of the Status Quo and is to be expected." (p.191)

The Option 4 (Multiple Alaska-Class Ferry plus Juneau Access Highway) would (p.190):

Result in a greater operating subsidy than all options except for the "full" Service Expansion Option 3.

The revenues generated by the expanded Lynn Canal service fall well short of the level expected to accrue from the proposed capital expense.

In this option, revenue yield actually decreases while Marine Vessel Operating costs remain unchanged.

The solution – change the current labor contract:

Option 4 "appears the least 'unprofitable' of the six options." (p. 191)

--

The report concludes: "Options 1B and, 4 illustrate that ship replacement of one or more existing vessels with Alaska-Class ships will increase the subsidy requirement, particularly in Option 4 where the fleet size increases." (p. 193)

- f. The per/mile ticket prices between Haines and Skagway are the highest in the system per/mile than on other legs of the Marine Highway. How will the construction of lower cost ferries affect user costs, specifically in Lynn Canal?

V. Reorganization of the State Department of Transportation and Public Facilities

- a. Why is it necessary to eliminate the position of Deputy Commissioner of Marine Operations? Where will the functions of the Deputy Commissioner be handled? Help us understand the proposed reorganization and administrative structure.
- b. The Alaska Marine Highway is a statewide function similar to airports and road systems. It serves communities and commerce from Bellingham, Washington to the Aleutians. Where does it fit in the structure?

VI. Process Oriented Questions:

- a. Why did the current Administration wait so long, at such a cost (+/- \$3 million), to weigh into a public process that was producing something "unwanted"? Is there some element in the procurement regulations that needs to be addressed to avoid wasting funds in a similar manner in the future?



Municipality of Skagway

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January 18, 2013

The Honorable Dennis Egan
State Capitol, Room 9
Juneau, AK 99801-1182

The Honorable Beth Kerttula
State Capitol, Room 404
Juneau, AK 99801-1182

Dear Senator Egan and Representative Kerttula:

The Municipality of Skagway is concerned about Governor Sean Parnell's recent decision to change the scope of the Alaska Class Ferry (ACF) project from construction of one vessel to the construction of two, smaller vessels. The decision raises questions but provides few answers about the direction the Alaska Marine Highway System (AMHS) will follow as the state upgrades the AMHS fleet to best serve our communities.

The Municipality's most immediate concern is that the decision was made without any apparent public involvement. The Alaska Class Ferry enjoyed widespread support among communities and local decision makers. Yet, the Marine Transportation Advisory Board, elected officials from communities that rely on the AMHS, regional organizations such as Southeast Conference and the public at large were denied the opportunity to review and comment on the proposed change.

The Municipality applauds the decision of the Senate and House Transportation Committees to conduct public hearings on this important development. We respectfully request that you investigate the following issues:

1. **SAFETY:** Lynn Canal is well known for its high winds and rough seas. Will safety considerations be compromised by the proposed features of smaller hull size, partial open deck and bow doors?
2. **DEMAND:** Will the new smaller ships be able to meet the capacity demand in the Lynn Canal corridor? What will be the impact on our local economies of reduced capacity, in particular when the 12-hour rule is implemented?

3. CONSTRUCTION COST: Without a design, how does the Department of Transportation and Public Facilities know that the construction cost of two boats will not exceed the cost of one ACF taking into consideration costs of redundancy such as twice the number of engines, radars, generators, etc.?
4. OPERATING COST: How do operating costs pencil out when expenses such as fuel and crews are doubled and two boats must be maintained, rather than one?
5. VIABILITY: A recent memo from Commissioner Pat Kemp stated that the position of Deputy Commissioner of Marine Operations will be eliminated from the Department of Transportation and Public Facilities. The Municipality of Skagway is concerned about the perception that the department is planning to dismantle the Marine Highway.

Thank you for your consideration.



Stan Selmer
Borough Mayor

cc: Representative Peggy Wilson, Chair, House Transportation Committee
Representative Jonathan Kreiss-Tomkins, House Transportation Committee
Representative Cathy Muñoz, House Finance Committee
Governor Sean Parnell
U.S. Senator Mark Begich
U.S. Senator Lisa Murkowski
U.S. Representative Don Young
Marine Transportation Advisory Board
Southeast Conference

Dana Owen

From: Richard Stokes <rastokes@gci.net>
Sent: Tuesday, January 22, 2013 9:03 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: DOT and plans for a new ferry--or two

Categories: Green Category

Senator Egan:

I was in the audience at the joint hearing last Thursday when Commissioner Kemp was being quizzed about DOT's plans.

I don't have the expertise to know what kind of ferry DOT should build for Southeast Alaska. Certainly the construction of another in the Taku class seemed to make sense. The Taku brought me to Juneau from Prince Rupert in May 1971, and I realize she's almost ten years older than that. Good as she has been she can't last forever.

The roll-on roll-off seemed to make some sense. I don't know about the question of open decks that Commissioner Kemp so adroitly evaded when you and others asked him. I realize a hearing was scheduled for January 24, but it sounded unlikely that you or the public was going to see any conceptual designs prior to the next hearing.

What did seem clear to me at the hearing was that DOT had dodged their obligations to consult with MTAB and the coastal communities served by the ferry system. Just because they worked themselves into a hole and now don't have the funds to build the product of a public process, doesn't mean they should make a unilateral decision to revert to an older plan.

I hope you can hold DOT to a correct public process. I appreciated the questions you asked last Thursday. Richard Stokes, Juneau resident since 1971.

From: Kathleen Menke ci@akmk.com
Haines, AK
Original via email 1/20/2013

Dear Senator Egan and Rep. Wilson: (and copied to others on the Transportation Committees in the Senate and House):

Please include this written testimony as my official record of testimony before the Transportation Committee, as next Thursday afternoon I will be preparing to board the ferry to Juneau and will be unable to testify in person.

We, in Haines, appreciate the questions and oversight of the House Transportation Committee. I was one of the people watching from live videoconference your last meeting.

Items necessary for the Committee to address are public process, substantive issues such as carry capacity and safety, reliability, and flexibility, and costs.

I address each here below as a twenty year resident of Haines and a thirty-year traveler of the Alaska Marine Highway.

#1 Public process

Public process is not being followed on appointments to oversight of AMH. The public being served requires that intent of state statute be applied with regard to appointment of Director, or Deputy Commissioner, or one accountable person in charge of AMH. The current change of organization actually makes no one clearly accountable or answerable to the public. That was clear at your last meeting when Reuben Yost said little, Pat Kemp alluded to the Governor (but not to another actually pulling many of the strings--Robin Taylor), and John Falvey was asked to speak on operations but was given neither title nor authority over decision making.

Following is an e-mail exchange between Robin Taylor and Pat Kemp that clearly shows decisions are being made without regard to public process. If our legislators won't ensure public process is followed and that we have one person clearly accountable for decisions made regarding the Alaska Marine Highway, who will? Clearly not our Governor.

Having Robin Taylor calling the shots out of the eye of any public accountability is disturbing. Questions should be asked as to why Mike Nuessl was asked to resign after Pat Kemp was appointed? Why someone with no ferry experience (Reuben Yost) is now the Deputy Director over the AMH? And about the Parnell, Taylor, Kemp, Yost avenue of communications with each other but not with the public? At the very least our legislators should be ensuring adequate public process.

----- Original Message -----

From: taylorrk13@comcast.net

To: Pat J Kemp (DOT)

Cc: al johnson ; John Sandor ; Ernie Christian

Sent: Tuesday, January 15, 2013 3:28 PM

Subject: Re:

Dear Pat:

The shuttle ferries is the only way to go and if we only built 30 miles of connecting road we could make the whole system ten times as efficient and generate a lot more revenue. ie: Haines to Cascade, point base the ferry in Haines and it can make two round trips per day morning and evening working only a ten hour day. Please revisit the Taylor plan for connecting roads and shuttle ferries it will work. I'd love to help you on this one.

I don't know if you are aware of it but last January 16-17, 2012, the entire electrical grid for southern southeastern Alaska was maxed out for two days! The only thing that kept it from crashing was that each town Petersburg, Wrangell and Ketchikan all turned off those municipal facilities which they had converted from diesel to electricity and put them back on diesel. Thus all the swimming pools, municipal and school buildings went back on diesel heat. The system was maxed out at 42 megawatts and thank God all systems worked and there was no failure. We have added significant additional electrical load to the system in the last year and already Ketchikan is paying huge diesel surcharges as they are operating the diesel --Bailey power plant already -- they ran out of hydro power a lot earlier than expected. I'm told that Petersburg has already put one new "Cat in a Can" diesel generator on line and has ordered another. Pat, Southern southeastern is out of power and has no new generation even on the drawing board, as they continue to hold meetings and debate who will own and who will operate facilities that they only dream of building. All of which they believe will be funded by large state or federal grants. They don't understand that NO Money is available or likely to come their way.

The key to our economic future is cheap reliable energy and without it----- tell the last guy leaving town to turn off the lights. The only way that new generation will be developed is if there is a market to sell the excess power into and to buy needed power from when we run low. The only corridor for access into southern Southeastern Alaska is the Bradfield! The federal law (T-lue) requires the Feds to give Alaska the corridor as soon as we survey it and delineate it's boundaries. This corridor should accommodate a road, pipeline and electrical intertie. Once the right of way has been conveyed from the feds to the state, the State of Alaska, need only grant permits and the intertie will be built by the private sector! Just like the Alaska Pipeline was built without state or federal funds and without the huge cost overruns and delays that would result if we waited for the government to fund, design and build it for us. The Canadians are building the new powerline up Hy.37 and down to the Forest-Kerr (run of river hydro project) on the Iskut. That puts the North American Electrical grid within 50 miles of Tyee Lake via the Bradfield Corridor.

I have met with Valard Const. (The contractor building the Hy 37 powerline) They are ready and willing to not only build the intertie to Alaska they are willing to help finance the project. I have also met with Puget Sound energy and they are also interested as is AP&T, and a whole series of other hydro developers. Pat we need to have the Bradfield corridor surveyed as soon as possible. It is a prerequisite that must occur before any new development of electrical energy can take place. Please let me know how I might assist you in getting this project (surveying the corridor) off the ground. Robin

From: "Pat J Kemp (DOT)" <pat.kemp@alaska.gov>
To: taylorrk13@comcast.net
Sent: Friday, January 11, 2013 9:11:21 PM
Subject: Re:

Hey Robin. It is really good to hear from you. Yeah, I'm back and it's a great opportunity to do some things. One of the first things we did was change this Alaska Class Ferry back to the shuttle ferries we envisioned. It started out as a shuttle then changed to an oversized Aurora that does nothing for us. So, right off the bat there's controversy, oh well. Hope you and Kay are doing well, stay out of trouble, haha. Pat

On Jan 11, 2013, at 7:19 PM, "taylorrk13@comcast.net" <taylorrk13@comcast.net> wrote:

Dear Pat:

Great to hear you got the DOT job. Now lets build the Bradfield Road, and Intertie. Best of luck Robin

#2 Substantive issues on the real need for the 350-foot, Alaska-class ferry or two or three more clones existing mainliner ferries.

Safety, reliability, and carrying capacity are essential to the health, safety, and economic stability of our SE Alaska communities. Flexibility for the entire AMH system can best be served by additional Alaska-class/mainliner ferries.

The health and safety of our children, elders, pets, and citizens of SE Alaska require no less than the 350-foot, Alaska-class ferry be built. Without clear safety and reliability and capacity, the economies of our SE Alaska communities is going to suffer. Last January I had a critically ill dog. The planes were not flying. The LeConte was cancelled. It was three days before I could get to Juneau on a ferry with the dog who subsequently died. It could just as easily been a person.

I recently was aboard the Taku with several regional wrestling teams from SE, heading from Juneau to Haines. Teams were from Metlakatla, Wrangell, Yakutat and elsewhere. The Taku is 352 feet. As we rounded the bend from Juneau heading north after a pleasant breakfast, we started rocking and rolling. Before long, the Captain announced, "Absolutely do not go out on the outer decks. We are locking down the elevator. Find something that is bolted down and hang on. We are in for some weather." Indeed we were in for five long hours of 70 knot winds and 14 foot waves. Most wrestlers and others, including myself, remained curled up on the floor somewhere, hanging on tightly to the nearest bolted down seat for the entire hard, hard ride that ensued.

Anyone who thinks anything less than a 350-foot, Alaska class ferry, will meet the safety and reliability needs of weather in the Upper Lynn Canal, or our capacity needs in the summer and frequently other times, really doesn't know what they are talking about.

As Senator Egan stated, any open-deck design is a recipe for failure. We already also know that the Fairweather and LeConte are not reliable for weather conditions in the Upper Lynn Canal.

#3 The cost factor seems to be a strawman, as Governor Parnell sent back to the feds money our Congressional delegation secured for the funding of this 350-foot, Alaska-class ferry.

In addition, if we are to be committed to picking one ship builder in Alaska for a project, then we must be willing to accept the cost overruns that will inevitably occur.

Third there are no actual costs tied to the change to smaller shuttle ferries. And budget and costs, as we learned from Comm. Kemp, are "fungible". Sometimes they include extra design costs, sometimes not. Sometimes they include costs for additional terminals or terminal modifications needed for the shuttle ferries, sometimes not.

We need to invest in the ferries that will meet our needs.

Thank you for your open-public processes and for taking testimony from Alaska communities affected by the recent decision by the Parnell Administration to abruptly change without public process. Please urge the Governor to restore both public process and the 350-foot, Alaska-class ferry that was developed through public process.

Regards, Kathleen Menke, Haines

Kathleen M.K. Menke
Crystal Images Photography and Publishing
Haines, Alaska
<http://www.akmk.com>

Dana Owen

From: george figdor <figdor@aptalaska.net>
Sent: Friday, January 18, 2013 12:59 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis; Representative_Craig_Johnson@legis.state.ak.us; Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop; Senator_Hollis_French@legis.state.ak.us
Subject: Support Alaska-class Ferries
Follow Up Flag: Flag for follow up
Flag Status: Flagged
Categories: Green Category

Dear Members of the Senate and the House Transportation Committees:

I urge you to fight for the Alaska-class ferry and reject the governor's slight of hand in trying to undermine our ferry system and incrementally impose a road on communities that have opposed it for decades.

I am a 40 year resident of Haines. While people in Haines do not often agree on issues, improved ferry service is one of those things that virtually everyone in this town sees eye to eye on. Like most residents of Southeast communities, we love our ferries and over the years have found the service pretty darn reliable, despite often harsh weather. Much community input went into the concept of the Alaska-class ferry.

Marine transportation is basic to the culture of Southeast. And we are proud to have this well run mass transit system in place, when so many other places in our nation are struggling to retrofit road-based transportation with mass transit.

George Figdor
Haines, Alaska

Dana Owen

From: jennifer talley <mellingbar@gmail.com>
Sent: Friday, January 18, 2013 7:38 AM
To: Senator_Dennis_Egan@legis.state.ak.us
Cc: Rep. Jonathan Kreiss-Tomkins; Rep. Peggy Wilson
Subject: Alaska State Ferries

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

Hello,

My name is Jennifer Talley. I am a nurse who resides in Haines and works in Juneau. The ferries are an integral part of, not only my life, but of several others who also commute to work from smaller Southeast Alaska communities.

Winter weather in Upper Lynn Canal can be hideous. There were a few times this winter when the ferry had to cancel due to weather and that was the LeConte. Originally, the Fairweather was "designed" to do sailings in the Upper Lynn Canal and that has not worked.

Obviously, Governor Parnell has never ridden the ferry in the winter, or he would not have unilaterally made a decision to change the design of them.

This is a bad mistake and should not happen. There is no need to waste money on a ferry that will not be able to function in the winter, what is the point?

Our fleet of ferries is aging and they need work.

The ferry system is one of the best things about Southeast Alaska. It is part of the fabric of the culture and should be a priority to maintain. The ferries have also seen a steady rise in ridership.

As my representatives, I would like you to fight to maintain the original design of the Alaska Class Ferry. I want something that is reliable for me to get back and forth to work. I want funds going to the ferry system to be well spent and thought out. I want money to go into a transportation system that already exists.

Thank you for your time,

Jennifer Talley
PO Box 1086
Haines, Alaska
99827
907 314 0864

Dana Owen

From: Nick Olmsted <olmstedkemp@yahoo.com>
Sent: Thursday, January 17, 2013 3:48 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: Alaska Marine Highway

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

Dear Senator Egan,

As a 35-year resident of Tenakee Springs on Chichagof Island, I believe I can offer some insight regarding winter travel in Southeast Alaska. I've been aboard the Le Conte many times when screaming wind and heavy icing caused the wide-eyed passengers to say almost in unison "I wouldn't want to be on a boat even one inch smaller!" Typical winter storms in Lynn Canal and Chatham Strait would be an emergency nearly anywhere else in the world.

Nothing can replace a large, displacement hull vessel for safe, reliable transportation. The idea of a small, open deck ferry would be laughable if it were not so dangerous.

We have come to depend on ferry service for our island community. I don't care if the ferry schedule is frequent or convenient. I just want some fixed points in the calendar in order to schedule a dentist appointment, a wedding or an employment obligation. I have no objection to paying state employees to provide that service, any more than I object to my federal tax dollars supporting the US Coast Guard.

Government exists to provide services that the private sector cannot.

Please press the "delete" button on Governor Parnell's harebrained scheme of an open-deck ferry, and reinstate the product of years of planning and discussion.

Sincerely,
Molly Kemp

Dana Owen

From: Bill Shattenberg <william@anchoralaska.com>
Sent: Thursday, January 17, 2013 2:19 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: Lynn Canal ferries

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

Senator Dennis Egan

I would like to voice some concerns regarding the Lynn Canal ferry plan.

1, A new ferry terminal at Cascade Point would be about forty miles out of Juneau. An estimated cab fare for a walk on passenger to downtown Juneau would be about \$90.00 minimum. A walk on fare Haines to Juneau is \$37.00. A air flight Haines to Juneau is about \$125.00 This is not affordable transportation.

The State is already looking at vanishing oil dollars. Having to build, maintain, and manage two ferry terminals both on the North end of Juneau is not practical.

3, While the travel time may be less on board the ship the overall time with getting to and from the terminal will offset the shorter boat trip. What is the point of building something new that will not work better than what is already in place.

Basically it looks like DOT and the Governor want to build roads regardless of what local Alaskans may want or need.

Thank you for your time.

Bill Shattenberg

5362 Commercial Blvd.

Juneau, Alaska 99801

Ph.907-780-3690

Cell 907-321-3691

Fax 907-780-3692

E-mail bill@anchoralaska.com

"The ordaining of laws in favor of one part of the nation, to the prejudice and oppression of another, is certainly the most erroneous and mistaken policy. An equal dispensation of protection, rights, privileges, and advantages, is what every part is entitled to, and ought to enjoy." Benjamin Franklin, 1774.

Dana Owen

From: Nina Chordas <nchordas@fastmail.fm>
Sent: Thursday, January 17, 2013 11:00 AM
To: Sen. Dennis Egan
Subject: ferries

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

Dear Senator Egan,

I was unable to open the link in your message about hearings regarding the AK State ferries, so thought I'd drop you a line to express my concerns. I have lived and worked in Alaska for the past 11 years, and consider the ferries of the Alaska Marine Highway to be one of the state's best and possibly most under-rated and under-supported assets. The sudden proposal for smaller ferries that recently came from the Governor's office strikes me as problematic at the very least. I'm deeply bothered by the high-handed manner in which plans that have been carefully and thoughtfully laid over a period of time by groups charged with working on a specific issue can get swept away in the blink of an eye and replaced by something that seems much less thought through or even beneficial to users. Who benefits from these changes? I don't think they are being made with the average ferry passenger in mind. I strongly support you in the advocacy for ferries that actually meet the needs of SE Alaska and its people.

Thank you for your work!
Nina Chordas

--
Nina Chordas M.A., Ph.D.
Associate Professor of English
Humanities Department Chair
University of Alaska Southeast, Juneau
nchordas@fastmail.fm
907-796-6407

Dana Owen

From: John Hudson <jhudson@gci.net>
Sent: Wednesday, January 16, 2013 10:14 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: ferries

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

Dear Senator,

Safe, comfortable and reliable ferry service in Lynn Canal is essential for local and out of state travels. I support the legislatures work to build a new Alaska Class ferry for this route and others.

John Hudson
Juneau

Dana Owen

From: Allison Smith <allison_theresa@yahoo.com>
Sent: Wednesday, January 16, 2013 6:44 PM
To: Rep. Peggy Wilson; "Representative_Eric_Feige@legis.state.ak.us"; Rep. Lynn Gattis; "Representative_Craig_Johnson@legis.state.ak.us"; "Representative_Bob_Lynn@legis.state.ak.us"; Rep. Jonathan Kreiss-Tomkins; "Senator_Dennis_Egan@legis.state.ak.us"; "Senator_Fred_Dyson@legis.state.ak.us"; Sen. Click Bishop; "Senator_Hollis_French@legis.state.ak.us"
Subject: Partially open-deck ferries...in Lynn Canal?
Follow Up Flag: Flag for follow up
Flag Status: Flagged
Categories: Green Category

Dear Legislators,

I am writing to share my strong opposition to Governor Parnell's decision, in which he ignored public input, deciding that two smaller, partially open-deck ferries should operate in Lynn Canal rather than one large, safe, reliable ferry - already approved by the legislature. First, I take issue with the governor circumventing the public process. Second, smaller ferries will not address the needs of Alaskans, businesses, and tourists who rely on the ferry to travel Lynn Canal. Conditions that can occur in Lynn Canal, such as strong winds and big waves make smaller, partially open-decked ferries unsafe and unreliable. It is not a sound use of state funds to purchase ferries or a good public safety decision.

Thank you in advance for getting us back on track with one seaworthy Alaska-class ferry for Lynn Canal.

Sincerely,

Allison Smith
12175 Glacier Hwy. B3
Juneau, AK 99801
907-586-1101

Dana Owen

From: Maggie Osborne <magoz31@hotmail.com>
Sent: Wednesday, January 16, 2013 5:17 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: Changes to the Alaska Class Ferry

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

Dear Senator Egan;

I am a resident and regular patron of the Alaska Marine Highway. I have always enjoyed and prefer travel via ferry. Currently, I am very concerned about the recent decision to change the design of the Alaska Class ferry. Travel in Southeast Alaska during the winter is difficult enough with ferries currently in service. Weather conditions and mechanical issues already lead to delays and/or cancellations on occasion. The decision by Governor Parnell to change the design for the Alaska Class ferry to smaller, partially open deck ferries is alarming to say the least. Residents of Southeast Alaska deserve better than to be ignored. Safe, reliable transportation is all the residents of Southeast ask for. Thank you for your time and consideration.

Sincerely,
Margaret A. Osborne
Box 192
Skagway, Alaska

Dana Owen

From: sean rielly <srielly@gmail.com>
Sent: Wednesday, January 16, 2013 4:31 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis; Representative_Craig_Johnson@legis.state.ak.us; Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop; Senator_Hollis_French@legis.state.ak.us
Subject: Re: unilateral change in direction of the Alaska Class Ferry

Good Afternoon Ladies and Gentlemen,

I am writing in regards to Gov Parnell's unilateral change in direction of the Alaska Class Ferry.

My concerns are that smaller, partially open-deck ferries operating in Lynn Canal would be unsafe and would not meet Juneau's access needs unlike the large, safe, reliable ferries that have been approved by legislature. The current larger closed deck vessels have proved to be seaworthy and safe in meeting Juneau's Access Needs especially with our inclimate weather and seas it is wise to have this seaworthy vessel.

If the Juneau Access Project "The Road" is completed it would be closed half of the year due to snow, avalanche danger among other hazards. During this time of year (legislative session) it is our most inclimate weather when we would need the safe seaworthy larger closed deck ferry to meet our transportation needs.

I am thankful for the Joint Transportation Committee Hearing that is being held so that the people of SE Alaska and Legislatures can discuss this matter further.

I do not support unilateral decisions that circumvent the public process, especially when it is in regards to safe transportation within the Great State of Alaska.

Thank you for your time and work

Take care,
Sean Rielly

I believe in Safe Ferries

Dana Owen

From: Bern Savikko <bsavikko@gmail.com>
Sent: Wednesday, January 16, 2013 10:27 AM
To: Senator_Dennis_Egan@legis.state.ak.us; Rep. Beth Kerttula;
representative_cathy_munoz@legis.state.ak.us
Subject: Kemp fires AMHS director and appoints Reuben Yost
Attachments: Kemp fires AMHS director and hires Reuben Yost 1-16-13.pdf

Dear Senator Egan and Representatives Kerttula and Munoz,

I have attached a letter that I hope you can find time to review to add a perspective to the recent developments within Alaska DOT/PF. Once again, I worry about the Alaska Marine Highway System's ability to "stay afloat" in today's political climate.

Thank you for your time and service,

Sincerely,

Bern Savikko

Senator Egan, Representative Kertulla, and Representative Munoz,

I wrote a rather lengthy letter to you back in mid-December regarding the Alaska Class Ferry. The recent firing of Captain Neussl by Pat Kemp further supports my opinion that the Alaska Marine Highway System (AMHS) is constantly being (mis)directed by political decisions instead of sound planning. The Alaska class ferry project has become another political issue instead of a basic transportation decision with political force prevailing over a sound engineering process.

The recent change dictated by the Governor and supported by Pat Kemp is an attempt to change the Alaska class ferry project to a Juneau Access project. I worked with Pat Kemp for years and he is a much respected engineer and a great guy. However, now that he is in a political position, he is allowing his passion for the Juneau Access project to influence his objectivity on the Alaska class ferry debate. My assumption is that Captain Neussl supported the original Alaska class design and it conflicted with Pat's long range plan to have a road up Lynn Canal. Pat's attempt (supported by the Governor) to change the type of ferry from one that is designed to service Lynn Canal efficiently to two that will better work as shuttles between Katzehin and Haines is not what AMHS has requested. The recent appointment of Reuben Yost (who I also respect) is a step to add further support to the Juneau Access project at the expense of AMHS.

The Juneau Access project has been presented by DOT in the past as the preferred option over improved ferry service in upper Lynn Canal. It has been debated by pro-road and pro-ferry advocates and to date the "preferred alternative" is still in question. I don't believe that this is a highway vs. ferry issue. The real issue is; what is the proper balance between roads and ferry routes to sustainably serve Southeast Alaska? Part of the answer is an objective perspective of the Juneau Access project and a properly designed vessel for Lynn Canal service.

Another issue that I mentioned in my previous letter is the loss of Berth II in Ketchikan. Pat has given AMHS direction to design and build a replacement berth in Ward Cove so the shipyard can have Berth II. As I stated previously, this is not a simple or inexpensive task and is in direct conflict with AMHS operations and their ability to efficiently service the busy Ketchikan hub. I would imagine Captain Neussl also had a different opinion on this subject than that presented by Pat Kemp and the newly appointed Reuben Yost will follow Pat's lead more easily. I respect both Pat and Reuben as long term DOT employees, but their present political positions and affiliation with the Juneau Access project add a distorted perspective to AMHS needs.

I will keep this letter short, but please feel free to email me if I can be of assistance on this subject. I have a fairly busy retirement schedule, but I would be willing to do research or provide information as required to help AMHS push through these difficult times.

Best Regards,

Bern Savikko

bsavikko@gmail.com

Dana Owen

From: Rob Goldberg <artstudioalaska@yahoo.com>
Sent: Tuesday, January 15, 2013 9:50 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Cc: Senator_Fred_Dyson@legis.state.ak.us
Subject: Alaska Class Ferry

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

To: Alaska State House and Senate Transportation Committee Members

Re: Alaska Class Ferry

Last month, just before Christmas, my wife and I embarked on a trip to take our kids to Florida to see their grandparents. We were scheduled to leave Haines on a Friday, on the LeConte, in order to catch the jet out of Juneau on Saturday. The weather was bad all that week, with temperatures in the single digits, north wind gusting to 75mph, 15 foot seas in Lynn Canal and heavy freezing spray. Wednesday's run of the LeConte from Juneau to Haines was canceled because of the dangerous conditions. Thursday morning we made a decision to go to Juneau a day early, on the Taku. It cost us an extra night in a hotel in Juneau, but we made our flight. Friday's run of the LeConte was canceled.

The LeConte is 235 feet long. The Taku is 352 feet long. The difference in length and volume between the two ferries makes the difference between being able to operate safely in Lynn Canal in the winter, or canceling trips. It also makes the difference between everyone on board being sick and everyone being comfortable.

After much public input from people who actually depend on the ferry system, the new Alaska Class ferry was designed to be about the size of the Taku. Anything smaller will not be safe and reliable in Lynn Canal in the winter.

The governor's abrupt decision to cancel the Alaska Class ferry and replace it with two, smaller, open decked ferries is an insult to Alaskans who need the ferries to travel in the winter. They simply will not work. The state will be repeating the mistake made when the Fairweather and the Chenega were tried in the Lynn Canal.

I don't like to attribute motive, but the governor's two shuttle ferries are suspiciously similar to the ones proposed by DOT for the East Lynn Canal road. If the governor wants to kill the ferry system and build a road, he should just say so.

I know a bit about the proposed East Lynn Canal road. I wrote the Haines Borough's comments on the draft EIS years ago. I'm also one of the few people who has actually had boots on the ground over there, on mountaineering trips. I can tell you with certainty that the road would be vastly more expensive than advertised. The terrain challenges are enormous. A road would be dangerous and closed by avalanches much of the winter. Any shuttle ferry would have to be the size of the Taku, or the Alaska Class ferry, to be able to operate safely in the winter between the Katzeihin terminal and Haines and Skagway.

If we need an Alaska Class ferry to be the shuttle, why build a road? It would be a huge waste of money. Access to Juneau would be made worse, not improved.

I urge you to tell the governor to restore the Alaska Class ferry. His two smaller ferries just won't work.

Sincerely,
Rob Goldberg
Haines

Rob Goldberg and Donna Catotti
Catotti and Goldberg Art Studio
PO Box 1154 Haines, AK 99827 USA
907-766-2707
artstudioalaska.com

Dana Owen

From: Maureen Moore <momooore57@gmail.com>
Sent: Tuesday, January 15, 2013 10:23 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: Ferry design change

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

Dear Sen. Egan,

As a long time resident of SE AK I take issue with Gov. Parnell's recent decision to elect an partially open-decked ferry to operate in Lynn Canal.

I wonder when the last time Gov. Parnell rode the ferry to Haines or Skagway? His decision leads me to believe the answer is, possibly, never.

Please do whatever is in your power to ensure that we only have the safest ferries in operation in our waters & that the Governor listens to the voices of the people that live & depend on ferries to safely traverse the "highways" around SE.

Thank you for your years of dignified service.

Maureen E. Moore
Gustavus & Homer

Dana Owen

From: Brooke Elgie <brookeelgie@gmail.com>
Sent: Tuesday, January 15, 2013 8:54 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Cc: Senator_Fred_Dyson@legis.state.ak.us
Subject: ferries

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Categories: Green Category

The Governor's new ferry plan is **WRONG** on two counts:

It is **WRONG** to unilaterally discard years of ferry planning done by professionals and supported by public input.

It is also **WRONG** to suppose that open deck ferries will serve the needs of isolated bush communities like ours where a great deal of our basic supplies are brought in by ferry and where trips can be as long as 12 hours in horizontal rain and snow. Open deck baggage storage and no food service – in winter???

Please tell the Governor "NO"

Brooke Elgie

Wendy Stern

Tenakee Springs

Dana Owen

From: Kiel Renick <krenick@gmail.com>
Sent: Tuesday, January 15, 2013 5:22 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: opposition to unilateral ferry changes

Dear Senator Egan,

I just wanted to let you know that I support the safe, publically determined and vetted, Alaska Class ferry project currently underway to better serve Southeast Alaska. I do not support unilateral changes to the project, but only changes that are brought about and decided upon by legislators and or the public through a public process, and only changes that would actually be suitable for travel in Southeast Alaska meeting our transportation and safety needs.

Thank you for your time, and for your work.

Sincerely,
Charles Kiel Renick

Dana Owen

From: H Unruh <unruh5832@yahoo.com>
Sent: Tuesday, January 15, 2013 4:58 PM
To: me
Subject: Re: Proposed ferries for Lynn Canal

Dear Alaska Representatives & Senators,

It is my understanding that Governor Parnell recently made a unilateral decision to change the direction of the new Alaska class Ferry----against the wishes of the SE Legislators. His preference for smaller, partially open-deck ferries to operate in Lynn Canal seems to be based on ????? There are two years of legislative review and public process (people who actually depend on ferry travel) which concluded that large, safe and reliable ferries are what is needed.

Anyone who has been on a ferry in inclement weather would probably not want to depend on a smaller partially open-deck ferry!

I hope you will fight for reasonable, safe, reliable ferries. Perhaps send the governor & his friends on one of the smaller ferries up Lynn Canal during a storm.

I hope you will resist the Governor and Department of Transportation Commissioner Pat Kemp as they attempt to circumvent the public process. In all cases, but especially when it may mean life or death to those who depend on the ferry system for transportation.

Thank you for taking a stand.

Helen Unruh

Dana Owen

From: Robert Andrews <andrews@aptalaska.net>
Sent: Tuesday, January 15, 2013 5:07 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: ferries

Categories: Green Category

Governor Parnell's unilateral decision to scrap the Alaska Class ferries and, instead, pursue some scaled-down open deck vessel is completely at odds with the public process and does not consider the needs of winter-traveling Alaskans. Our ferries need to be safe and reliable in all weather conditions, both summer and winter. Even if these Alaska Class vessels exceed the original cost estimate, they will be worth it in the long run with added dependability.

It is not the governor's place to take it upon himself to replace a program that has been well thought out and arrived at with due public process.

Sincerely,
Robert A. Andrews

Dana Owen

From: Tristan Knutson-Lombardo <tkl1289@gmail.com>
Sent: Tuesday, January 15, 2013 8:35 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis; Representative_Craig_Johnson@legis.state.ak.us; Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us; Sen. Cllck Bishop; Senator_Hollis_French@legis.state.ak.us
Subject: We Need REAL Alaska Class Ferries that Work!

Hi Transportation Committee,

I am writing to voice my displeasure in Governor Sean Parnell's unilateral and undemocratic decision to dismantle the Alaska Class Ferry project. Coastal Alaska needs a ferry system that works for us. With our aging fleet, it is important that we have ferries that are reliable, efficient and provide enough car/passenger/cabin space to serve our unique communities. For the last decade, our system has been ignored and devastated by irresponsible management, ignorant of public input.

You all, as the Joint Transportation Committee, have the opportunity to provide a good and long lasting service to the people of Southeast Alaska. Thousands of Alaskans use and depend on our ferry system, so please don't let us down!

Thank you,

Tristan Knutson-Lombardo
300 Maple St
Juneau, Alaska
907.723.1772

Dana Owen

From: Thom Ely <akthome@yahoo.com>
Sent: Tuesday, January 15, 2013 8:41 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis; Representative_Craig_Johnson@legis.state.ak.us; Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop; Senator_Hollis_French@legis.state.ak.us
Cc: Haines Borough; Stephanie Scott; s.selmer@skagway.org
Subject: Lynn Canal Ferry

Dear Representatives and Senators of the Transportation Committee,

For those of us that live and work in the communities of Haines and Skagway the Governors plan to substitute the contracted ferry design for his preferred alternative is unacceptable. Much time, effort and money has been spent on designing the Alaska Class Ferry to be deployed in the Lynn Canal as scheduled.

We depend on year round reliable ferry service to meet our personal and business needs. This last minute change of plans casts doubt on the States commitment to provide the service we need. The smaller open deck ferries that the Governor is touting will not be operational in the Lynn Canal during severe weather, which occurs often, any time of the year.

In addition the public process has been circumvented in what appears to be a political tactic to grab a larger share of Federal funds for the Governors real preferred alternative, a road to Juneau. The communities of Haines and Skagway and half the population of Juneau prefer our ferry system to a road up the Lynn Canal.

I urge you to overturn his proposal and direct the AK DOT to move forward with the original plan, an Alaska Class Ferry that can operate year round in the Lynn Canal for the next fifty years.

Sincerely,

Thom Ely
POB 1014
Haines, AK 99827
907-314-0860

Dana Owen

From: Linda Kadrlik <adventuresafloat@gci.net>
Sent: Wednesday, January 16, 2013 7:32 AM
To: Senator_Dennis_Egan@legis.state.ak.us; Rep. Peggy Wilson
Subject: Alaska Class Ferry

Categories: Green Category

I am opposed to Gov. Parnell's decision to build two smaller ferries instead of the already approved Alaska Class ferry. Surely funding can be found to build this more appropriate ferry for southeast Alaska. Perhaps Gov. Parnell should try to get to Skagway by ferry during a winter storm, and then he would know why we need the Alaska Class ferry. Let me know how I can help. Linda

Adventures Afloat
Francis and Linda Kadrlik
4950 Steelhead, Juneau, AK 99801
(800) 3AFLOAT, (907) 789-0111
FAX: (907) 789-3312
<http://home.gci.net/~valkvrie>

Dana Owen

From: Karen Capp <k.capp@yahoo.com>
Sent: Wednesday, January 16, 2013 8:59 AM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: Ferry

Categories: Green Category

I am not at all sure I know what a Partially Open Deck ferry looks like but I am thinking something you ride in warmer, less rainy climates for short runs. Alaska needs ferries that are safe, reliable, large Alaska Class Ferries that can make any run. Our seas do not lend themselves to little shuttle ferries unless you would only use them on protected runs of less than a 10 miles or so. Doesn't make sense to me at all to build 2 ferries of a type that the people didn't want or need.

Where is the public process or even respect for the public process in the Governor's decision?

Karen Capp
A Voter!

Dana Owen

From: Chris Fredell <islands@ptialaska.net>
Sent: Wednesday, January 16, 2013 9:22 AM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: need for capable, safe new ferry

Senator Egan, I raise my voice with those Southeast Alaska residents who are speaking out for the Alaska Marine Highway System, and the replacement vessels that will be needed in the very near future. I am disappointed at the governor's recent decision to abandon the planned Alaska Class replacement vessel, exchanging it for two cheaper and less capable boats. As a Southeast resident whose family and friends use the ferries year-round for both business and pleasure, I strongly encourage you and your senate committee peers to correct this decision. Please help to restore the state's commitment to safe and effective public marine transportation in our region.

Thanks for your efforts, and your service to our community and our state. And--Best Wishes for the upcoming legislative session!

Chris Fredell
Juneau, AK.

Dana Owen

From: Marie Shipley <marieshipley.925@gmail.com>
Sent: Wednesday, January 16, 2013 10:13 AM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: open ferries - a bad idea and bad governing

Categories: Green Category

Dear Senator Egan:

As a fellow Alaskan, I am asking that you please speak out in support of safe, reliable ferries and that you not support unilateral decisions that circumvent the public process as demonstrated by Governor Parnell.

To our island communities in SE Alaska, the ferry system is not a luxury but the lifeline to services, business and travel within the state and to the lower 48.

Governor Parnell's unilateral decision to drastically change the direction of the new Alaska Class Ferry—against the wishes of the Southeast legislators and the people they represent.

Ignoring two years' worth of legislative review and public process, the Governor decided he would prefer two smaller, partially open-deck ferries to operate in Lynn Canal rather than a large, safe, reliable ferry that has been approved by the legislature.

Smaller, partially open-deck ferries in Lynn Canal? For those who travel Lynn Canal in the winter, know how unsafe and unrealistic this idea sounds.

The Governor and his recently appointed Department of Transportation Commissioner Pat Kemp are attempting to circumvent the public process and force their ideas onto the public.

Thank you for your representation.

Sincerely,

Marie Shipley

Dana Owen

From: Marina Lindsey <marina.lindsey@yahoo.com>
Sent: Wednesday, January 16, 2013 8:39 AM
To: Rep. Peggy Wilson; Representative_Eric_Felge@legis.state.ak.us; Rep. Lynn Gattis; Representative_Craig_Johnson@legis.state.ak.us; Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop; Senator_Hollis_French@legis.state.ak.us
Cc: marina.lindsey@yahoo.com
Subject: Lynn Canal Ferries - We need one big, safe vessel

Dear Legislators,

I am writing to let you know that I'm opposed to Governor Parnell's decision, ignoring public input, in which he decided that two smaller, partially open-deck ferries should operate in Lynn Canal rather than one large, safe, reliable ferry - already approved by the legislature. One, it is inappropriate for a Governor to circumvent public process, and two, smaller ferries will not address the needs of Alaskans, businesses, and tourists who rely on the ferry to travel Lynn Canal. As we all know, there can be very strong winds and big waves in Lynn Canal, and spending public money on more ferries that will not be safe or reliable is not an effective use of State money. It's also not a good public safety decision.

Thank you in advance for getting us back on track with one, seaworthy Alaska-class ferry for Lynn Canal.

Sincerely,

Marina Lindsey
3431 Greenwood Ave.
Juneau, AK 99801
908.780.7818

Dana Owen

From: Willeke Burnham <w.burnham@skagway.org>
Sent: Wednesday, January 16, 2013 11:48 AM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: Alaska Class Ferry

Senator Egan,

I wish to express my concern regarding the Alaska Class Ferry Redirection. I am a long term resident of Skagway and often rely on the ferry for transportation to Juneau and beyond. The majority of my trips to Juneau are for doctors appointments and other important life events, not to mention a connection to the Juneau Airport. Having a reliable Alaska Class Ferry is vital to our community and quality of life. The Governor's decision to change from one reliable ferry to two smaller ferries astounds me. It is obvious to me that the Governor has not had to rely on ferry transportation and does not realize the impact it has on one's life when a ferry gets cancelled due to weather.

When traveling on a smaller ferry during 14 foot swells, the experience is uncomfortable to say the least. My children, husband and elderly mother in law have all succumbed to sea sickness, which is not something they (or anyone) should have to endure. An Alaska Class Ferry will provide residents with essential reliable transportation. It has been my experience that senior citizens and families with young children prefer to get state rooms, especially when the ferry is scheduled to leave in early morning hours or late at night. Even more so when the ferry is at (or over) capacity with sports teams.

The building of two smaller ferries versus one reliable ferry will end up costing the State even more money, since the inclement Southeast Alaska weather will render them useless.

Willeke Burnham

Dispatcher/TSO

Skagway Police Department

(907) 983-2232

w.burnham@skagway.org

Dana Owen

From: Martin Niemi <cmnlemi@gmail.com>
Sent: Wednesday, January 16, 2013 12:15 PM
To: Representative_Peggy_Wilson@akleg.gov; Representative_Eric_Feige@legis.state.ak.us;
Rep. Lynn Gattis; Representative_Bob_Lynn@legis.state.ak.gov;
Representative.Jonathan.Kreiss-Thomkins@akleg.gov;
Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us;
Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop;
Senator_Hollis_French@legis.state.ak.us
Subject: Alaska Class Ferry

Dear Alaska Senator/Representative:

Please be advised that as often time Alaska Ferry Passengers, we feel strongly that:

*We want a safe, reliable large Alaska Class Ferry and DO NOT WANT short, partially open deck shuttle ferries.

*We do NOT support unilateral decision making that circumvents the public process.

The Governor's decision, in my mind, raised questions about his ability to lead the state of Alaska in the proper directions.

Thanks,

Martin and Christine Niemi1

Dana Owen

From: Patty Fecteau <patty@hamiltonconst.com>
Sent: Wednesday, January 16, 2013 1:53 PM
To: Senator_Dennis_Egan@legis.state.ak.us
Subject: Travel on our Alaska Marine Highway

I am stunned by this, I truly am. Just this week on Monday January 14th, 2013 the Matanuska could not make it safely into Skagway due to the weather conditions and the Lynn Canals dangerous ways!!!! So he actually believes that we will be able to travel on smaller Ferries..... Well maybe he should try to ride the Leconte on up through from Juneau some morning when the weather is its normal cranky little self and see how much smaller he would like to vote for!!! Seriously disturbed by this information....

I really hope someone with a brain in their head can help Governor Parnell get a clue.....

Thank you,

Patty Fecteau

Hamilton Construction, LLC

Po Box 648

Skagway, Alaska 99840

907-983-2702 Office

907-983-2649 FAX

Dana Owen

From: Candace Cahill <candace_cahill@yahoo.com>
Sent: Wednesday, January 16, 2013 2:03 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis;
Representative_Craig_Johnson@legis.state.ak.us;
Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins;
Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us;
Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop;
Senator_Hollis_French@legis.state.ak.us
Subject: Joint Transportation Committee Hearing

I am writing in support of safe, reliable Alaska Class ferries, not short, partially open-deck shuttle ferries. I DO NOT support Governor Parnell's unilateral decision to drastically change the direction of the new Alaska Class Ferry.

Candace Cahill
PO Box 662
Skagway, AK 99840
907-983-6675



Alaska editorial: Ripple effect

Posted: January 20, 2013 - 12:11am

This editorial first appeared in the Ketchikan Daily News:

It seemed like smooth sailing toward updating the Alaska Marine Highway System ferry fleet until early December. Now, it's unclear what the ripples in the water represent, but it is a cause of unease in Ketchikan.

Bert Stedman, R-Sitka, representing Ketchikan and District Q had, while in the powerful position of co-chair of the state Senate Finance Committee, worked with the administration and House to secure \$120 million for the first of the Alaska Class ferries. The potential of more than one had been discussed. Another \$50 million had been secured for a second new ferry.

Ketchikan fully expected all of the ferries to be built at Alaska Ship & Drydock, bringing at least a dozen years of business to the shipyard and the community. Additionally, 129 full-time, year-round, high-paying jobs at the shipyard and another 76 jobs in businesses providing goods and services to the yard had been projected.

It was believed that the Alaska ships should be built at a State of Alaska-owned shipyard. It seemed nonsensical for a shipyard owner to utilize another shipyard.

For that result, the state had returned federal funds destined for the first ferry. The rules that apply to federal projects disappeared.

Then the state adopted a procurement method — construction manager/general contractor — in proceeding with the ferry project and Ketchikan Shipyard.

Elliott Bay Group — the ferry designers — the state Department of Transportation and the shipyard had been working on the final ferry design. That design was scheduled to be completed in the upcoming summer. It would be followed by a maximum price proposal from Alaska Ship & Drydock.

But the estimated cost of building the first ferry began to significantly exceed what Stedman had made available, the latest estimate being between \$150 million and \$167 million. That, combined with the operating cost that included a crew of between 21 and 25, caused DOT to re-evaluate the ferry project.

State officials decided to scrap the Alaska Class Ferry concept. Gov. Sean Parnell, along with DOT Commissioner Pat Kemp, announced the decision to a stunned audience at the Greater Ketchikan Chamber of Commerce weekly luncheon Dec. 4.

In one of Alaska's most fiscally and politically conservative communities, no one applauded when Gov. Parnell stated the decision would save the state money.

Instead, that queasy feeling acquired in rough seas set in collectively throughout the luncheon crowd and the community.

DOT planned to build two smaller ferries (between 250 and 300 feet), neither of which would be large enough to replace the 1960s-built, 350-foot, mainline ferries. The age of those ferries prompted the concern that initiated the new-ferry project. That those ferries provide the lucrative Ketchikan-Bellingham service was a consideration.

The state has not subjected the administration's new direction to the same economic analysis that it applied to the Alaska Class concept.

DOT has held up this analysis as proof that the Alaska Class ships would not save money. However, the report states the only way AMHS will save money is by significantly reducing the size of the fleet.

DOT says it will do this type of economic analysis for the proposed smaller ships once the design is available.

Parnell says he believes two smaller ships could be built for \$120 million. Not only will any cost for them undoubtedly increase, but it will never be less expensive than it is in the early part of this decade to build and replace aging mainline ferries. Delays only increase the cost. Of course, delays in new ships also mean the increased likelihood of necessary repairs, which would be expected to take place at the state's shipyard in Ketchikan.

Meanwhile, DOT has the Juneau Access Improvement Project on its schedule, with a \$520 million pricetag. That's one price. Other prices likely will be forthcoming.

The Juneau road would be a surfaced highway; the Alaska Class Ferry is a marine highway. It's highways and highways, transportation and transportation. Both are important to Southeast transportation. But, the Alaska Class Ferry price sounds like a good deal compared to the Juneau project.

Public hearings are scheduled in 2013 on the project that would improve the transportation system to and from Juneau within Lynn Canal. A final supplemental Environmental Impact Statement and a Federal Highway Administration Record of Decision are scheduled to be released, according to the DOT website for Alaska's Southeast Region.

This isn't to say that the Juneau project isn't worthwhile. It is. But, it's interesting that the new, smaller (hopefully less expensive) ferry will be utilized in Lynn Canal where a (much-more-expensive-than-an-Alaska-Class-ferry) road will be built.

It also begs the question of what happens when state officials determine that road project is too expensive.

It was pleasing when the Parnell administration came into office that it continued on the course set by the Murkowski administration in regard to ferry replacement. But Parnell says the project has evolved from its original small-boat concept into the larger Alaska Class project. Such a statement indicates Parnell is trying to keep what he views as the original concept on course. But through the years it seems like the idea of replacing ferries takes a new turn with each change at the helm.

The recent announcement that the captain in charge of the Marine Highway System, Michael Neussl, is retiring -- not shortly after Parnell's December speech to the Chamber -- adds to the ripples surrounding the ferry replacement project.

Part of the problem with changing course, regardless of the reason, is that it creates a certain amount of apprehension. With Ketchikan's experiences when the state studies and then changes direction and studies again, i.e. the Gravina Access Project over the past couple decades, it isn't surprising the community is wondering what's next.

It had been full speed ahead with the Alaska Class Ferries from Ketchikan's perspective. Maybe the community expected more than had been promised. But whatever the case, there is a nervousness, and it is hoped the Parnell administration can make enough progress with its new plan of two small ships to replace one large one -- or is it three large ones? -- to calm the sea of concern.



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CLOSE X



House Transportation Committee
Alaska State Legislature
House of Representatives
Representative Peggy Wilson - Chair

MEMORANDUM

Date: January 24, 2013
To: Suzi Lowell, Chief Clerk
From: Representative Peggy Wilson, Chair
House Transportation Committee
Re: Notice of Joint House and Senate Transportation Committee Meeting

Tuesday January 29, 2013 Room 124 1:00-3:00 pm

+DOT/PF Aviation Overview - Deputy Commissioner Steve Hatter

+Mobility Coalition Update - David Levy

+ Confirmation - Pat Kemp DOT/PF

*waiting for OK from Gov's Office
(Heather)*

Thursday Jan 31, 2013 Room 124 1:00-3:00 pm

<No Meeting Scheduled>

+ Indicates will be teleconferenced

Rebecca Rooney

From: Kristin Hathhorn <hathbat@yahoo.com>
Sent: Wednesday, January 23, 2013 9:28 AM
To: Rep. Peggy Wilson
Subject: Ferry Comments

Dear Representative Wilson,

As a longtime resident of Haines and one that relies on the ferry system for dependable, safe, and comfortable transportation I appreciate the hard questions you and your committee are asking DOT. It is inconceivable that they would change the original design (and a possible compromise in safety) to save what amounts to a small amount of funds. In the end, 2 smaller vessels would show how insufficient they would be in handling the extreme conditions of Southeast and in particular the Upper Lynn Canal.

Let's hope DOT sees that their decision needs to be reversed.

Sincerely,

Mark Battaion

Rebecca Rooney

From: Emily Davis <edavis@kakeschools.com>
Sent: Wednesday, January 23, 2013 1:03 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis; Rep. Doug Isaacson; Representative_Craig_Johnson@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Hollis_French@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us
Subject: Ferry Ideas from kids

Students have written responses in regards to the ferry changes. Please take a moment to read how students who ride the ferry often for personal and for sports travel feel about it.

Student Responses to the Ferry System

Student 1

How often do you ride the Alaska State Ferry? Well for me, I usually ride it every few months or so. The ferry that I ride the most would probably be the Taku. The Taku is getting a little old now, so I think we should have one that has the same amount of cabins. It should also have a cafeteria, somewhere to just sit around, and public and private bathrooms. I think that the cafeteria is the most important place on the ferry.

The reason why I think we need a new ferry is because of how old the Taku is. It is over fifty years old. Private and public bathrooms are important because you need a private bathroom in the cabin and a public bathroom for when we are not in the cabins. Another reason why we need the ferry is because it gives you a place to sleep, eat, play, and use the bathroom. Planes cost a lot of money to travel on and ferries don't cost as much as air planes do.

In conclusion ferries are more fun than planes, this is why I think we need to a new ferry.

Student 2

The main topic I want to talk about is the ferry. I think that it is good that you guys are replacing the ferries with new ones because the ferries are about fifty years old and will start to break down rapidly.

What I think you guys should add to the new ferry is a bigger game room with new games. Also, you should make a separate room for basketball teams, and a room for the kids.

I think it would be better to have separate rooms for the basketball teams and the kids because the teams could probably bully the younger kids. What do you think should be done on the ferry? Overall, that is what I have to say.

Student 3

Did you know the Taku is over fifty years old? it was built around 1960. I think it's time to make a new ferry.

If I got to remodel the Taku, I would keep all of the staterooms and the lounges. However, I'd change the bathrooms, solarium, the game room, all of the doors, the cafeteria, the gift shop, and the floors. The bathroom stalls are old and they should get 2-ply tissue paper. I would remove the solarium and put a basketball court up there. I'd d' that because a lot of people in SE Alaska play basketball and especially during Gold Medal time. The Taku gets crowded when all the basketball teams are on there and it would keep kids/teens out of trouble. The cafeteria could use better tables and chairs too. On the taku, the floors are super hard. There are many people that have to sleep on the floor, so might as well make it comfortable. I'd also suggest wifi.

In conclusion, I really want to make the Taku a better ferry ride. Many people would love it!

Rebecca Rooney

From: jennifer talley <mellingbar@gmail.com>
Sent: Tuesday, January 22, 2013 1:59 PM
To: Sen. Anna Fairclough
Subject: alaska class ferry comment

Hello,

My name is Jennifer Talley. I am a nurse who resides in Haines and works in Juneau. The ferries are an integral part of, not only my life, but of several others, who, like myself, commute to work from smaller Southeast Alaska communities.

Winter weather in Upper Lynn Canal can be hideous. There were a few times this winter when the ferry had to cancel due to weather and that was the LeConte. Originally, the Fairweather was "designed" to do sailings in the Upper Lynn Canal and that has not worked.

Obviously, Governor Parnell has never ridden the ferry in the winter, or, he would not have unilaterally made a decision to change the design of them, unless there is a hidden agenda here undermining the marine transportation system of Alaska. This is not representative of what the communities of Southeast Alaska want.

Creating a smaller ferry is a bad mistake and should not happen. There is no need to waste money on a ferry that will not be able to function in the winter. Nor is there a need to waste money on an access road which is not supported. With the money that would be needed to create a road from Juneau to, another ferry terminal, you could build at least two reliable, large, state of the art ferries. This seems like a more pragmatic use of money.

Our fleet of ferries is aging and they need work.

The ferry system is one of the best things about Southeast Alaska. It is part of the fabric of the culture and should be a priority to maintain. The ferries continue to see a steady rise in ridership.

As my representatives, I would like you to fight to maintain the original design of the Alaska Class Ferry. I want something that is reliable for me to get back and forth to work. I want funds going to the ferry system to be well spent and thought out. I want money to go into a transportation system that already exists.

Thank you for your time,

Jennifer Talley
PO Box 1086
Haines, Alaska
99827
907 314 0864

Rebecca Rooney

From: Richard Folta <rjfolta@yahoo.com>
Sent: Tuesday, January 22, 2013 2:48 PM
To: Rep. Peggy Wilson
Subject: alaska class ferries

Representative Wilson:

Having lived by Lynn Canal almost all my life and frequently traveling on the ferries during the often severe Canal winter weather, we were shocked by the Governor's unilateral action to ignore the public input and change the new ferry design to shuttles instead of building the Alaska Class ferries for Haines and Skagway destination.

Dick and Julie Folta, Haines

Rebecca Rooney

From: Rob Goldberg <artstudioalaska@yahoo.com>
Sent: Monday, January 21, 2013 12:00 AM
To: Rep. Jonathan Kreiss-Tomkins
Cc: Rep. Peggy Wilson
Subject: Shuttle ferries

To: Alaska State House and Senate Transportation Committee Members

Re: Shuttle Ferries

On Thursday, January 17th I listened with interest to the transportation committee's hearing on the Alaska Class ferry. As a bit of background, I am a business owner in Haines and chairman of the Haines Borough Planning Commission. I have been following, and commenting on, the Juneau Access project for twenty years or more. Here are my comments on what I heard at the meeting:

Shuttle Ferries – I was glad to hear Capt. Falvey say that the state is not considering open-decked Puget Sound type ferries. They will not work in Lynn Canal, and the concept should not be brought up again. I think the 280 foot ships he described can work, provided they are designed with ocean-type hulls and are powerful enough to fight the winter winds in Lynn Canal. Since a ship's size increases by a factor of the cube, a ship this size would have considerably more volume than the 235 foot LeConte.

The LeConte is a great ship, and has served Alaska well, but it is sometimes overmatched by the wind and sea conditions in Lynn Canal. As was mentioned at the meeting, four runs of the LeConte have been canceled so far this winter (two right before Christmas as Haines and Skagway residents were trying to make connections for holiday visits), and many more probably should have been. From October 15th through December 29th, 2012 Lynn Canal had incessant gale, storm and hurricane force winds. Many of the LeConte's runs from Juneau to Haines arrived late, with the ship heavily laden with ice and nearly everyone on board sick. As a passenger, it is not a fun experience. A larger ship is needed.

In these times of economic uncertainty it is very important that the State of Alaska make a well researched choice and a good investment. As a small business owner, I can't afford to buy an expensive tool that can't do the job. The state can't either. The mistakes that were made with the purchase of the fast ferries Fairweather and Chenega must not be repeated.

The fast ferries were advertised as being capable in all sea conditions, but early on, running against a gale in Lynn Canal, the Fairweather suffered structural damage. Friends of mine who were on that run said that the seas were breaking against the windows of the forward observation lounge. Now those ships are kept in port when the wind is greater than 35 kts. and the seas are 7 feet or more. They are not reliable even in summer in Lynn Canal.

Routing – The two ship concept brought up at the meeting is a new one to me. I don't remember it being in any of the Juneau Access documents. At the meeting it was said that one ship would run between Juneau and Haines and the other between Haines and Skagway.

I have long advocated for a ferry that would overnight in Haines. In addition to the employment for Haines residents, it also means convenient connections to the jet in Juneau. Currently, with the ferry leaving Juneau in the early morning and returning in the evening, travelers from Haines and Skagway have an expensive hotel stay in Juneau on both ends of a trip Outside.

Costs - If the cost of one, 350 foot Alaska Class ferry has exceeded the state's budget of \$120 million, how is it that DOT thinks we can buy two, 280 foot ships for that price? I understand from what I heard at the meeting that the smaller ships would not have staterooms, crew quarters or a galley. Does the omission of these items from the shuttle ferries really save enough money to buy an entire additional ship?

Rep. Kreiss-Tomkins asked a good question at the meeting about additional costs involved with the roll-on, roll-off design. Currently only the Juneau terminal can accommodate an end-loading ferry. How much will it cost to retrofit the existing terminals? Will the Haines terminal have to be greatly expanded to accommodate two ferries at once?

Fiscal Responsibility and the Road – If the newly designed shuttle ferries are capable of operating daily, year-round, between Juneau, Haines and Skagway, there is no reason to build a road. In these times of economic uncertainty it will be very difficult for DOT to justify spending \$520 million or more on a road if we have two new ferries operating efficiently with low maintenance and operating costs. As I stated in a previous letter, I am one of the few people who have spent time on the east side of Lynn Canal. I can tell you from my mountaineering experience there that it is no place to try to force a road. Rocks and boulders fall constantly. Avalanches roar all winter and spring. The road would be closed much of the winter. An East Lynn Canal road would consume vast quantities of the state's money that would be better used in other places.

In an early version of the Juneau Access EIS, DOT estimated the cost of the East Lynn Canal road at about \$260 million, but they had made their estimates without ever putting people on the ground. After repeated questions from myself, and others, the state hired Golder Engineering to analyze the proposed route. Their report detailed over one hundred major geologic hazards along the route. Largely because of Golder's report, the construction estimate for the road is now \$520 million. The Golder Engineering report should be required reading for all legislators.

Thank you for reading my comments.

Sincerely,
Rob Goldberg

Rob Goldberg and Donna Catotti
Catotti and Goldberg Art Studio
PO Box 1154 Haines, AK 99827 USA
907-766-2707
artstudioalaska.com

Rebecca Rooney

From: Kathleen Menke <ci@akmk.com>
Sent: Sunday, January 20, 2013 1:20 PM
To: Senator_Dennis_Egan@legis.state.ak.us; Rep. Peggy Wilson
Cc: Rep. Lynn Gattis; Sen Hollis French; Rep Bob Lynn; Sen Fred Dyson; Rep Craig Johnson; Rep Eric Feige; Sen Anna Fairclough; Rep Jonathan Kreiss-Tomkins
Subject: Transportation/AMH

Dear Senator Egan and Rep. Wilson: (and copied to others on the Transportation Committees in the Senate and House):

Please include this written testimony as my official record of testimony before the Transportation Committee, as next Thursday afternoon I will be preparing to board the ferry to Juneau and will be unable to testify in person.

We, in Haines, appreciate the questions and oversight of the House Transportation Committee. I was one of the people watching from live videoconference your last meeting.

Items necessary for the Committee to address are public process, substantive issues such as carry capacity and safety, reliability, and flexibility, and costs.

I address each here below as a twenty year resident of Haines and a thirty-year traveler of the Alaska Marine Highway.

#1 Public process

Public process is not being followed on appointments to oversight of AMH. The public being served requires that intent of state statute be applied with regard to appointment of Director, or Deputy Commissioner, or one accountable person in charge of AMH. The current change of organization actually makes no one clearly accountable or answerable to the public. That was clear at your last meeting when Reuben Yost said little, Pat Kemp alluded to the Governor (but not to another actually pulling many of the strings--Robin Taylor), and John Falvey was asked to speak on operations but was given neither title nor authority over decision making.

Following is an e-mail exchange between Robin Taylor and Pat Kemp that clearly shows decisions are being made without regard to public process. If our legislators won't ensure public process is followed and that we have one person clearly accountable for decisions made regarding the Alaska Marine Highway, who will? Clearly not our Governor.

Having Robin Taylor calling the shots out of the eye of any public accountability is disturbing. Questions should be asked as to why Mike Nuessl was asked to resign after Pat Kemp was appointed? Why someone with no ferry experience (Reuben Yost) is now the Deputy Director over the AMH? And about the Parnell, Taylor, Kemp, Yost avenue of communications with each other but not with the public? At the very least our legislators should be ensuring adequate public process.

----- Original Message

From: taylork13@c
omcast.net

Rebecca Rooney

From: mark and mardell <mardiz@aptalaska.net>
Sent: Saturday, January 19, 2013 1:27 PM
To: Rep. Peggy Wilson
Subject: Please support Ak Class Ferry

Representative,

Please support the proposed Ak Class Ferry. I am against Gov. Parnell's unilateral decision to build a smaller vessel. A safe, reliable, large Ak Class Ferry is a better option for S.E. Alaska's severe winter weather.

Thank You very much, Mark S. Kistler

Rebecca Rooney

From: mark and mardell <mardiz@aptalaska.net>
Sent: Saturday, January 19, 2013 1:14 PM
To: Rep. Peggy Wilson
Subject: Please support Ak Class Ferry

Representative,

Please support the proposed Ak Class Ferry that Gov. Parnell has unilaterally stopped in leu of a smaller vessel. A safe, reliable, large Ak Class Ferry will be a better option for the sometimes severe winter weather in S.E. Alaska.

Thank you very much, Mark S. Kistler Haines, Ak

Rebecca Rooney

From: John MacKinnon <John@agcak.org>
Sent: Wednesday, January 16, 2013 7:51 PM
To: Rep. Peggy Wilson; Rep. Bob Lynn; Rep. Craig Johnson; Rep. Eric Feige; Rep. Lynn Gattis;
Rep. Doug Isaacson; Rep. Jonathan Kreiss-Tomkins
Cc: Rep. Beth Kerttula; Rep. Cathy Munoz
Subject: Shuttle Ferry

I had the honor of being a Deputy Commissioner of Transportation for Highways and Facilities from 2003 to 2008. During that time, I had the pleasure of working with the first two Deputy Commissioners for Marine Highways, Tom Briggs and John Torgerson. Tom Briggs was one of the principle founders of the Inter-Island Ferry Authority (IFA), and John Torgerson...well, he needs no introduction. Although in 2003 the Department had been split into three silos – Highways, Aviation and Marine Highways – there was considerable collaboration within the Department among the silos.

Budgets were lean in those years, and we all were looking for ways to keep services up and costs down, both short and long-term. In Marine Highways, Tom Briggs was a proponent of the two highly successful operations in southern Southeast Alaska – The IFA owned and operated M/V Prince of Wales operating between Ketchikan and Hollis on Prince of Wales Island, and the AMHS owned M/V Lituya operating between Metlakatla and Ketchikan. Both ships are considered “shuttle ferries”; they operate with smaller crews on 12 hours days; they have a “home port” they return to; they provide excellent service, and, most important, they have the lowest operating subsidy by just about any relevant metric.

We looked at operating costs, subsidies (operating and capital) and the service levels. The decision and the direction then was to copy the successful shuttle ferry model of the IFA and the M/V Lituya in southern southeast and apply it to northern southeast. Funds appropriated to the vessel replacement fund were put on the street for the design of a shuttle ferry class vessel for southeast.

Like sometimes happens, this shuttle ferry changed course and became an “Alaska Class Ferry”, a larger and more expensive ship with a 24/7 crew. More expensive to build and more expensive to operate. a 50 year old model. If there were a commensurate increase in service with the increase in operating costs, then we should continue with the Alaska Class Ferry. But there isn't. It's the AMHS culture that drives it.

In the 1990's, when Canada changed some of their laws, it became difficult for some of our citizens to cross their border to travel from the contiguous US to Alaska. As a result, ferry travel from Bellingham increased significantly, the boat filled and they started leaving people behind. A business model would have been to increase the cost of fares until no traveler was left behind. A business model would have a ship on a route actually making a profit. The AMHS solution was to keep the ticket costs the same and add a second ship to the route. The result was two ships losing money on a potentially profitable route. It's the AMHS culture.

In conclusion, because of the volume of traffic and the length of the routes, the AMHS will always need a subsidy, but changes to the operating model can keep service levels at an acceptable level - or even improve them. The shuttle ferry model works and will help achieve a more sustainable transportation system. Apply a business model to the AMHS. DOT and the Administration did not hijack the "Alaska Class Ferry" they put the well thought out and sustainable "shuttle ferry" back on course.

John MacKinnon, Executive Director

ASSOCIATED GENERAL CONTRACTORS

OF ALASKA

8005 Schoon Street

Anchorage, AK 99518

Tel: (907) 561-5354 Fax: (907) 562-6118

Email: john@agcak.org



This message has been scanned for malware by Websense. www.websense.com

Rebecca Rooney

From: Gerard Garland <garlandg@aptalaska.net>
Sent: Friday, January 18, 2013 6:08 PM
To: Rep. Peggy Wilson; Rep. Jonathan Kreiss-Tomkins
Subject: Governor Parnell's Ferry Plan

Dear Representative Wilson and Representative Kreiss-Tompkins:

The governor's plan for two small, drive-on, drive-off ferries instead of a larger Alaska Class ferry is a bad idea. I live in Haines, and there are many days, especially in the winter, when anything smaller than the LeConte would not be up to the task. Some days, even the LeConte sailing is cancelled due to strong winds, heavy seas, and occasional icing conditions. Like 95 mph sustained winds at Eldred Rock.

Honestly, what is the matter with the man? Has he ever ridden the ferry to Haines in marginal conditions? If so, he would know that the smaller ferry would be inadequate.

Thanks so much for your consideration.

Sincerely,

Gerard Garland

Haines

Rebecca Rooney

From: Sherrie <riversidealaska@yahoo.com>
Sent: Friday, January 18, 2013 11:44 PM
To: Rep. Jonathan Kreiss-Tomkins; Rep. Peggy Wilson; Rep. Lynn Gattis;
Representative_Craig_Johnson@legis.state.ak.us;
Representative_Bob_Lynn@legis.state.ak.us; Representative_Eric_Feige@legis.state.ak.us
Subject: SUPPORT ALASKA CLASS FERRY PURCHASE

Dear Chairman Wilson and Members of the House Transportation Committee,

I am writing to urge you to not support the Governor's recent proposal to purchase two small roll on-roll-off ferries rather than the Alaska Class ferry approved by the legislature to replace aging vessels.

If you have spent any time in the Lynn Canal in rough weather, especially in winter, you know why this is a ill conceived plan. There have been recent sailings, even on the smaller ferries in the current fleet, when nearly every passenger was ill from the rough seas. Imagine if they'd been sailing on a little ferry of the type you'd see in Seattle crossing mild Puget Sound.

The proposal would seem ludicrous except for the fact that it is a solution based on a need that doesn't exist today. The promoters hope to use these small ferries at the end of their dream of the Juneau Access Road. The State Department of Transportation has dreamed of building a road up the Lynn Canal since before I got here almost 40 years ago. Its been proposed, studied and studied again and it hasn't happened.

Now with the national debt and calls for reigning in discretionary spending it is unlikely the Juneau Access Road will be built with federal funds until after these small ferries purchased in 2013 are beaten up by winter in the Lynn Canal.

Please continue to support the purchase of an Alaska Class Ferry with a regular car deck to improve transportation in Southeast Alaska for the people living here now. Thank you for your support of southeast transportation needs and thank you for your consideration of my request.

Sincerely,

Sherrie Goll
PO Box 261
Haines, AK 99827

Rebecca Rooney

From: Sherrie <riversidealaska@yahoo.com>
Sent: Friday, January 18, 2013 11:44 PM
To: Rep. Jonathan Kreiss-Tomkins; Rep. Peggy Wilson; Rep. Lynn Gattis;
Representative_Craig_Johnson@legis.state.ak.us;
Representative_Bob_Lynn@legis.state.ak.us; Representative_Eric_Feige@legis.state.ak.us
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Sincerely,

Sherrie Goll
PO Box 261
Haines, AK 99827

Rebecca Rooney

From: Gerard Garland <garlandg@aptalaska.net>
Sent: Friday, January 18, 2013 6:08 PM
To: Rep. Peggy Wilson; Rep. Jonathan Kreiss-Tompkins
Subject: Governor Parnell's Ferry Plan

Dear Representative Wilson and Representative Kreiss-Tompkins:

The governor's plan for two small, drive-on, drive-off ferries instead of a larger Alaska Class ferry is a bad idea. I live in Haines, and there are many days, especially in the winter, when anything smaller than the LeConte would not be up to the task. Some days, even the LeConte sailing is cancelled due to strong winds, heavy seas, and occasional icing conditions. Like 95 mph sustained winds at Eldred Rock.

Honestly, what is the matter with the man? Has he ever ridden the ferry to Haines in marginal conditions? If so, he would know that the smaller ferry would be inadequate.

Thanks so much for your consideration.

Sincerely,

Gerard Garland

Haines

Rebecca Rooney

From: Erv Hagerup <solo9619@yahoo.com>
Sent: Saturday, January 19, 2013 9:15 AM
To: Rep. Jonathan Kreiss-Tomkins; Rep. Peggy Wilson; lynncanalconservation@gmail.com;
Rep. Lynn Gattis; Representative_Craig_Johnson@legis.state.ak.us;
Representative_Bob_Lynn@legis.state.ak.us
Subject: Alaska Class Ferry

Good Morning,

My name is Ervin L Hagerup, and I was employed by AMHS in May of 1966, and retiring in April of 2001. Twenty of those years were served as Master on all the AMHS ships sailing in SE Alaska.

My first Master's assignment in 1979 was on the M/V Chilkat, a 99 ft long ferry built by the Territory of Alaska in 1957. It replaced the Chilkoot on a run from Tee Harbor in Juneau to Haines.

Having been born and raised in Juneau, I spent my youth and subsequent career on the waters of Southeast Alaska, with fair amount of experience on the waters of Lynn Canal.

At present, I am Marine Pilot, licensed by the State of Alaska to Pilot Foreign Vessels on the waters of SE Alaska.

In all those years with AMHS, I ran in weather that slowed the Malaspina Class vessels to 6 knots in 80 to 90 knots of wind in Lynn Canal.

Now the political view of Juneau wanting a road connection to the Capital is understandable and even desirable. Someday that may actually happen.

But, to achieve that goal by having the State build inadequate vessels to deploy on the Lynn Canal run is counter to common sense and years of recommendations from the AMHS personnel and the Maritime Community in general.

I believe the best interest of the State of Alaska is served by building the Alaska Class Ferry that can be used on the Lynn Canal Run and if the road is ever built in Lynn Canal, then the Alaska Class Ferries could be used in other areas of Alaska. The State already has sold the M/V Chilkat, and the M/V Bartlett for that very reason. They were too small and not economically feasible to operate any longer. They were auctioned off to private owners at a fraction of their value.

On the other hand, the M/V Malaspina, which was scheduled to be scrapped a few years ago, is now being refurbished and put into service again with the ability to serve any where required by AMHS.

In my opinion to cancel the Alaska Class Ferry in favor of a couple smaller and less capable vessels is not the best use of limited State resources.

Thank you, I am,

Captain Ervin L. Hagerup, Master (retired 2001)
AMHS.

Rebecca Rooney

From: Bill Shattenberg <william@anchoralaska.com>
Sent: Thursday, January 17, 2013 2:21 PM
To: Rep. Peggy Wilson
Subject: Lynn Canal ferries

Rep. Peggy Wilson

I would like to voice some concerns regarding the Lynn Canal ferry plan.

1, A new ferry terminal at Cascade Point would be about forty miles out of Juneau. An estimated cab fare for a walk on passenger to downtown Juneau would be about \$90.00 minimum. A walk on fare Haines to Juneau is \$37.00. A air flight Haines to Juneau is about \$125.00 This is not affordable transportation.

2, The State is already looking at vanishing oil dollars. Having to build, maintain, and manage two ferry terminals both on the North end of Juneau is not practical.

3, While the travel time may be less on board the ship the overall time with getting to and from the terminal will offset the shorter boat trip. What is the point of building something new that will not work better than what is already in place.

Basically it looks like DOT and the Governor want to build roads regardless of what local Alaskans may want or need.

Thank you for your time.

Bill Shattenberg

5362 Commercial Blvd.

Juneau, Alaska 99801

Ph.907-780-3690

Cell 907-321-3691

2wks
Pam Vanni

Fax 907-780-3692

E-mail bill@anchoralaska.com

"The ordaining of laws in favor of one part of the nation, to the prejudice and oppression of another, is certainly the most erroneous and mistaken policy. An equal dispensation of protection, rights, privileges, and advantages, is what every part is entitled to, and ought to enjoy." Benjamin Franklin, 1774.

Rebecca Rooney

From: george figdor <figdor@aptalaska.net>
Sent: Friday, January 18, 2013 12:59 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis; Representative_Craig_Johnson@legis.state.ak.us; Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop; Senator_Hollis_French@legis.state.ak.us
Subject: Support Alaska-class Ferries

Dear Members of the Senate and the House Transportation Committees:

i urge you to fight for the Alaska-class ferry and reject the governor's slight of hand in trying to undermine our ferry system and incrementally impose a road on communities that have opposed it for decades.

I am a 40 year resident of Haines. While people in Haines do not often agree on issues, improved ferry service is one of those things that virtually everyone in this town sees eye to eye on. Like most residents of Southeast communities, we love our ferries and over the years have found the service pretty darn reliable, despite often harsh weather. Much community input went into the concept of the Alaska-class ferry.

Marine transportation is basic to the culture of Southeast. And we are proud to have this well run mass transit system in place, when so many other places in our nation are struggling to retrofit road-based transportation with mass transit.

George Figdor
Haines, Alaska

Rebecca Rooney

From: hopkins@kpunet.net
Sent: Friday, January 18, 2013 10:50 AM
To: venables@aptalaska.net
Cc: mark.eliason@ustravel.us; rbressette@bridgedeck.org; gerry.hope@sitkatriben-sns.com; josh@touralaska.net; timothyjoyce@yahoo.com; dkelly@lynden.com; mkorsmo@aptalaska.net; mayor@ci.unalaska.ak.us; jcc@alaska.com; twodmax@aol.com;
Subject: Rep. Peggy Wilson
Open Decks and Knighthood Bows

Dear Robert and MTAB:

I watched yesterday's meeting between D.O.T. and the Transportation Committee. It is very concerning that D.O.T. Commissioner Kemp bypassed MTAB, the sudden change in the Alaska Class Ferry Project, and the emphasis on smaller (less cargo carrying capacity), perhaps "open-deck" or "Knighthood bow roll on/roll off" type vessels, with an entire new dockside infrastructure to accommodate them.

If my memory is correct, the very reason the State of Alaska sold the M/V E.L. Bartlett was because of its Knighthood bow, roll on/roll off design.

It was shortly before the sale of the Bartlett on eBay that the Stena Line's ferry Estonia sank in the Baltic Sea due to a compromised Knighthood bow. When a Knighthood bow is compromised, water rushes in fast onto the car deck and the vessel sinks quickly.

There are also times when the Lituya covers for the IFA on the route between Ketchikan and Hollis. Crossing Clarence Strait with an open deck vessel assures a saltwater bath for the vehicles onboard. Passengers ride in their vehicles, and when going down a large wave, apply their brakes.

It was a sight to watch the brake lights coming on as the bow dropped over a large wave.

It is nearly the same story - D.O.T. not listening to their customers, in fact, somewhat mocking them, the landside highway part of the Department trying to subvert the marine side, resulting in an overall diminishment of the Alaska Marine Highway System. This tug-of-war within the Department never ends, regardless of changing hands.

MTAB should be aware, however, of reduced cargo carrying capacity, open car decks, and Knighthood bows and their history. I am no expert, but I do know that Lynn Canal, which seems to be everyone's focus for the 3700 mile long Marine Highway, can be a mean place in the winter for any vessel not designed to meet those conditions.

Yours truly,

Captain William M. Hopkins
Ketchikan, Alaska

Rebecca Rooney

From: Nick Olmsted <olmstedkemp@yahoo.com>
Sent: Thursday, January 17, 2013 3:40 PM
To: Rep. Peggy Wilson
Subject: Ferry

Dear Representative Wilson,

As a 35-year resident of Tenakee Springs on Chichagof Island, I believe I can offer some insight regarding winter travel in Southeast Alaska. I've been aboard the Le Conte many times when screaming wind and heavy icing caused the wide-eyed passengers to say almost in unison "I wouldn't want to be on a boat even one inch smaller!" Typical winter storms in Lynn Canal and Chatham Strait would be an emergency nearly anywhere else in the world.

Nothing can replace a large, displacement hull vessel for safe, reliable transportation. The idea of a small, open deck ferry would be laughable if it were not so dangerous.

We have come to depend on ferry service for our island community. I don't care if the ferry schedule is frequent or convenient. I just want some fixed points in the calendar in order to schedule a dentist appointment, a wedding or an employment obligation. I have no objection to paying state employees to provide that service, any more than I object to my federal tax dollars supporting the US Coast Guard. Government exists to provide services that the private sector cannot.

Please press the "delete" button on Governor Parnell's harebrained scheme of an open-deck ferry, and reinstate the product of years of planning and discussion.

Sincerely,
Molly Kemp

To: Pat J Kemp
(DOT)
Cc: al johnson ; John
Sandor ; Ernie
Christian
Sent: Tuesday,
January 15, 2013 3:28
PM
Subject: Re:

Dear Pat:

The shuttle ferries
is the only way to go
and if we only built
30 miles of
connecting road we
could make the whole
system ten times as
efficient and generate
a lot more
revenue. ie: Haines
to Cascade, point base
the ferry in Haines
and it can make two
round trips per day
morning and evening
working only a ten
hour day. Please
revisit the taylor plan
for connecting roads
and shuttle ferries it
will work. I'd love to
help you on this one.

I don't know if
you are aware of it
but last January 16-
17, 2012, the entire
electrical grid for
southern southeastern
Alaska was maxed
out for two
days! The only thing
that kept it from
crashing was that
each town Petersburg,
Wrangell and
Ketchikan all turned
off those municipal
facilities which they
had converted from
diesel to electricity

and put them back on diesel. Thus all the swimming pools, municipal and school buildings went back on diesel heat. The system was maxed out at 42 megawatts and thank God all systems worked and there was no failure. We have added significant additional electrical load to the system in the last year and already Ketchikan is paying huge diesel surcharges as they are operating the diesel -- Bailey power plant already -- they ran out of hydro power a lot earlier than expected. I'm told that Petersburg has already put one new "Cat in a Can" diesel generator on line and has ordered another. Pat, Southern southeastern is out of power and has no new generation even on the drawing board, as they continue to hold meetings and debate who will own and who will operate facilities that they only dream of building. All of which they believe will be funded by large state or federal grants. They don't understand that NO Money is available or likely to come their way.

The key to our economic future is cheap reliable energy and without it----- tell the last guy leaving town to turn off the lights. The only way that new generation will be developed is if there is a market to sell the excess power into and to buy needed power from when we run low. The only corridor for access into southern Southeastern Alaska is the Bradfield! The federal law (T-lue) requires the Feds to give Alaska the corridor as soon as we survey it and delineate it's boundaries. This corridor should accommodate a road, pipeline and electrical intertie. Once the right of way has been conveyed from the feds to the state, the State of Alaska, need only grant permits and the intertie will be built by the private sector! Just like the Alaska Pipeline was built without state or federal funds and without the huge cost overruns and delays that would result if we waited for the government to fund, design and build it for us. The Canadians are building the new powerline up Hy.37 and down to the

Forest-Kerr (run of river hydro project) on the Iskut. That puts the North American Electrical grid within 50 miles of Tyee Lake via the Bradfield Corridor.

I have met with Valard Const. (The contractor building the Hy 37 powerline) They are ready and willing to not only build the intertie to Alaska they are willing to help finance the project. I have also met with Puget Sound energy and they are also interested as is AP&T, and a whole series of other hydro developers. Pat we need to have the Bradfield corridor surveyed as soon as possible. It is a prerequisite that must occur before any new development of electrical energy can take place. Please let me know how I might assist you in getting this project (surveying the corridor) off the ground. Robin

From: "Pat J Kemp (DOT)"
<pat.kemp@alaska.gov>
To: taylork13@comcast.net
Sent: Friday, January 11, 2013 9:11:21 PM
Subject: Re:

Hey Robin. It is really good to hear from you. Yeah, I'm back and it's a great opportunity to do some things. One of the first things we did was change this Alaska Class Ferry back to the shuttle ferries we envisioned. It started out as a shuttle then changed to an oversized Aurora that does nothing for us. So, right off the bat there's controversy, oh well. Hope you and Kay are doing well, stay out of trouble, haha. Pat

On Jan 11, 2013, at 7:19 PM,
"taylorrk13@comcast.net"
<taylorrk13@comcast.net> wrote:

Dear
Pat:
Great
to hear
you
got the
DOT
job. Now
lets
build
the
Bradfield
Road,
and
Intertie
. Best
of luck
Robin

#2 Substantive issues on the real need for the 350-foot, Alaska-class ferry or two or three more clones existing mainliner ferries.

Safety, reliability, and carrying capacity are essential to the health, safety, and economic stability of our SE Alaska communities. Flexibility for the entire AMH system can best be served by additional Alaska-class/mainliner ferries.

The health and safety of our children, elders, pets, and citizens of SE Alaska require no less than the 350-foot, Alaska-class ferry be built. Without clear safety and reliability and capacity, the economies of our SE Alaska communities is going to suffer. Last January I had a critically ill dog. The planes were not flying. The LeConte was cancelled. It was three days before I could get to Juneau on a ferry with the dog who subsequently died. It could just as easily been a person.

I recently was aboard the Taku with several regional wrestling teams from SE, heading from Juneau to Haines. Teams were from Metlakatla, Wrangell, Yakutat and elsewhere. The Taku is 352 feet. As we rounded the bend from Juneau heading north after a pleasant breakfast, we started rocking and rolling. Before long, the Captain announced, "Absolutely do not go out on the outer decks. We are locking down the elevator. Find something that is bolted down and hang on. We are in for some weather." Indeed we were in for five long hours of 70 knot winds and 14 foot waves. Most wrestlers and others, including myself, remained curled up on the floor somewhere, hanging on tightly to the nearest bolted down seat for the entire hard, hard ride that ensued.

Anyone who thinks anything less than a 350-foot, Alaska class ferry, will meet the safety and reliability needs of weather in the Upper Lynn Canal, or our capacity needs in the summer and frequently other times, really doesn't know what they are talking about.

As Senator Egan stated, any open-deck design is a recipe for failure. We already also know that the Fairweather and LeConte are not reliable for weather conditions in the Upper Lynn Canal.

#3 The cost factor seems to be a strawman, as Governor Parnell sent back to the feds money our Congressional delegation secured for the funding of this 350-foot, Alaska-class ferry.

In addition, if we are to be committed to picking one ship builder in Alaska for a project, then we must be willing to accept the cost overruns that will inevitably occur.

Third there are no actual costs tied to the change to smaller shuttle ferries. And budget and costs, as we learned from Comm. Kemp, are "fungible". Sometimes they include extra design costs, sometimes not. Sometimes they include costs for additional terminals or terminal modifications needed for the shuttle ferries, sometimes not.

We need to invest in the ferries that will meet our needs.

Thank you for your open-public processes and for taking testimony from Alaska communities affected by the recent decision by the Parnell Administration to abruptly change without public process. Please urge the Governor to restore both public process and the 350-foot, Alaska-class ferry that was developed through public process.

Regards, Kathleen Menke, Haines

Kathleen M.K. Menke
Crystal Images Photography and Publishing
Haines, Alaska
<http://www.akmk.com>



THE STATE
of **ALASKA**
GOVERNOR SEAN PARNELL

Department of Transportation and
Public Facilities

OFFICE OF THE COMMISSIONER
Patrick J. Kemp, P.E., Commissioner

3132 Channel Drive
P.O. Box 112500
Juneau, Alaska 99811-2500
Main: 907.465.3900
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dot.state.ak.us

January 22, 2013

The Honorable Dennis Egan
Chair, Transportation Committee
State Senate
Alaska State Capitol
Juneau, Alaska 99801-1182

The Honorable Peggy Wilson
Chair, Transportation Committee
House of Representatives
Alaska State Capitol
Juneau, Alaska 99801-1182

Dear Senator Egan, Representative Wilson, and members of the House and Senate Transportation Committees:

Recently you received a communication from a Haines resident that makes several allegations about the recent change to the Alaska Class Ferry (ACF). In order to facilitate a more thorough response to your constituent, I would like to share some additional factors for consideration.

The main three points that I would ask you to consider while evaluating her concerns are issues of public process, the need for a 350-foot ferry for Lynn Canal, and the associated costs.

The email from Senator Robin Taylor on January 11 was the first contact I have had with him since June of 2006. Senator Taylor has not offered advice to the change of the scope of the ferry nor was he consulted for advice.

Deputy Commissioner Reuben Yost has oversight of the Alaska Marine Highway System (AMHS) for the Department of Transportation and Public Facilities but, as described in my testimony at the January 17 Joint Transportation Committee meeting, I have eliminated the assignment of a transportation mode to describe the department's deputies. The department's three deputies and the commissioner will provide executive oversight for the department but will not be involved in the day-to-day management of a division or a system; that responsibility now rests at the director level. In the case of AMHS, that individual is Captain John Falvey and he will manage and have complete authority over the system from his Ketchikan office.

The ACF shuttles will be designed to handle the conditions in Lynn Canal, as well as other routes in Southeast Alaska. The vessel size will be based on the capacity needs of various routes. As I mentioned at the meeting last week, we are preparing a Design Concept Report that will address these and all other major aspects of the two-shuttle concept. While public involvement is important information for the

"Get Alaska Moving through service and infrastructure."

design process, the department needs to develop the vessel criteria using marine architects, based on marine engineering science, weather conditions, United States Coast Guard (USCG) regulations, masters input, etc. These parties are very well versed in regard to vessel length. These vessels will have the same or better sea keeping characteristics than the Motor Vessel (M/V) Taku, which rarely misses a scheduled sailing anywhere in the system.

Capital and operating costs concerns are never a "strawman" and particularly not given the state's current financial projections. The estimated cost of the previous Alaska Class Ferry far exceeded the funding available and that cost estimate was anticipated to grow even higher. Governor Parnell was very concerned with escalating costs and about a year ago warned the department to modify the vessel's design to bring it within budget or provide an alternative concept that would fulfill the vessel's intended mission. The department subsequently attempted to reduce costs by eliminating or modifying certain features but it was clear that even with significant modifications, the cost of this particular design could not be built for \$120 million.

As I testified, the original vessel concept in 2006 was to be a roll-on, roll-off with a bow door and would be operated as a day boat. Over a several year period the design morphed from that concept to a 350' vessel that can best be described as a class between an Aurora size and a mainliner (Malaspina, et.al.). The public involvement process was a factor in this morphing and it appears that budgetary constraints were not a significant factor as the scope of the vessel changed.

After determining that the 350' design could not be delivered within budget, the department next reviewed the needs for the system and concluded that the best course of action was to return to the original concept of constructing a roll-on, roll-off vessel. In 2006, the estimated cost to construct such a vessel was \$30 million. With inflation and other factors, we believe two of these vessels could be constructed within the \$120 million appropriated.

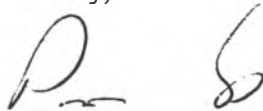
The Governor reviewed the information and made what I consider to be a very forthright decision by directing the Department to return to the concept envisioned in 2006. Fiscal considerations were, indeed, a significant factor for the Governor as well as his desire to have an Alaska ferry built in Alaska at a reasonable cost.

Returning the scope of this project to that originally envisioned, a day boat shuttle, will reduce capital costs and annual operating costs. A soon-to-be published Design Concept Report will provide a cost estimate for the ACF shuttles and the terminal modifications required. The ACFs will have an estimated useful life of approximately 60 years. Also, as explained, no funds were returned to the federal government in order to build the ACFs in Alaska. The federal funding originally programmed for the ACF were used for other Alaska transportation projects.

I have no idea what Ms. Menke is describing as fungible costs and the allegation that the department manipulates those estimates. The department always includes development and overhead costs in project estimates.

I trust this response will address many of the concerns that surround the change to the ACF concept. I look forward to providing additional information as it becomes available.

Sincerely,



Patrick J. Kemp, P.E.
Commissioner

Cc: Senator Dennis Egan
Vice-Chair Senator Fred Dyson
Senator Click Bishop
Senator Anna Fairclough
Senator Hollis French
Representative Peggy Wilson
Representative Doug Isaacson
Representative Craig Johnson
Representative Eric Feige
Representative Bob Lynn
Representative Lynn Gattis
Representative Jonathan Kreiss-Tomkins

Rebecca Rooney

From: William M. Hopkins <hopkins@kpunet.net>
Sent: Wednesday, January 23, 2013 10:20 AM
To: Rep. Jonathan Kreiss-Tomkins
Cc: Rep. Peggy Wilson
Subject: Fw: Knighthood bows

From: William M. Hopkins
Sent: Monday, January 21, 2013 10:58 AM
To: Representative.Jonathan.Kreiss-Tomkins@akleg.gov
Subject: Knighthood bows

Good morning, Representative Kreiss-Tomkins

Thank you for your inquiry. I will try to answer them in the order you gave them. I make no claims to be an expert.

1. I made a grammatical error by capitalizing the term "knighthood bow." This a general term used by seamen to describe a visor bow, much like the visor on a knight's helmet that lifts upward above the eyes. The Bartlett had a visor-type bow, that is, it could be lifted so vehicles drove strait onboard to the car deck. There were no side doors on the Bartlett. Vehicles could also access the ship through the stern door, much like the Aurora or Le Conte. Both Aurora and Le Conte are expanded versions of the Bartlett without visor-type bows, but side doors instead.

See: http://en.wikipedia.org/wiki/MV_Bartlett

The Bartlett was sold on ebay in August 2003. I no longer know anyone associated with the sale. They have all moved on, most down South. I do remember that Captain George Capacci was leading AMHS when this occurred, and he subsequently became a high official with the B.C. Ferries in Vancouver. This is one of the weaknesses of DOT management - they come and go, and institutional memory is lost. When the Bartlett was decommissioned, the reasons given were that it was as "too expensive to upgrade." Press accounts point out expensive upgrades to the lifesaving equipment.

However, this is not completely true as a reason for selling the Bartlett. I would point out that all of the other older ships have upgraded lifesaving equipment. At the time of the sale (August 2003), I do remember that the driving force behind the sale was to avoid pending Federal regulations concerning visor-type bows. This was in response to the sinking of the Stena Line's ferry Estonia in the Baltic Sea with the loss of 852 lives in 1994. This ship had a visor bow, and traveling too fast in rough seas, the bow came open, and water filled the car deck. The

ship tried to turn to avoid the seas and capsized. Some say this was due to a procedural error in securing the visor bow for sea while in port.

See: http://en.wikipedia.org/wiki/MS_Estonia

A watertight bulkhead (wall) stands behind the visor bow, with watertight doors that vehicles must pass through to get on the car deck. If this second line of defense is not secured properly due to human error, bad things can happen in rough seas. There are a number of web sites detailing the ferry Estonia disaster.

2. Prior to the arrival of the the IFA's Stikine, the AMHS Lituya would relieve the Prince of Wales when it had to go into shipyard for annual maintenance. I made several voyages on the Lituya as a pilot with Captain Steve Booth between Ketchikan and Hollis in 2005. There was no vehicle damage because the weather was not severe enough to cause vehicles to slide, however, it was rough enough (wind SE 30 - 35 knots) that sea spray flew about the ship, and vehicles on the open deck were bathed in salt water. Passengers were riding inside their cars, and as the bow dipped while going over a wave, their automatic response was to apply their brakes. It was interesting to watch, to see the lights coming on almost in unison. Myself, if had a nice car, I would not want it soaked in salt water, but these folks had no other choice. I was serving as the pilot as I had also served as the Captain of the Aurora for eight years when it was on the "Hollis Run." If the weather was stronger than 30 - 35 knots, we probably would not have sailed. Additionally, wooden chocks are placed behind vehicle wheels to keep them from rolling fore and aft, but a wave overboard could wash those away, and a vehicle can slide sideways as well. On the Tustumena and Kennicott we lash every vehicle to the car deck, chocks would be of little help in the Gulf of Alaska.

Sincerely yours,

Captain William M. Hopkins, AMHS ret.
Ketchikan

Rebecca Rooney

From: Paul Davis <gallantadventures@yahoo.com>
Sent: Wednesday, January 23, 2013 3:16 PM
To: Rep. Peggy Wilson; Representative_Eric_Feige@legis.state.ak.us; Rep. Lynn Gattis; Rep. Doug Isaacson; Representative_Craig_Johnson@legis.state.ak.us; Representative_Bob_Lynn@legis.state.ak.us; Rep. Jonathan Kreiss-Tomkins; Senator_Dennis_Egan@legis.state.ak.us; Senator_Fred_Dyson@legis.state.ak.us; Senator_Anna_Fairclough@legis.state.ak.us; Sen. Click Bishop; Senator_Hollis_French@legis.state.ak.us
Subject: An Alaskan's comments on the upcoming Joint Transportation Committee Hearing

My wife and I are Alaskans who split our time between living in both Kake and Sitka. I was very disappointed to have heard that the Governor and the DOT changed the original plan of building the 350ft ferry for 2 smaller ferries that I feel will not properly serve SE Alaska. The integrity of the public process was not maintained and that decision was not warranted. Personally, I believe that the State should construct a ferry similar to the Taku. Alaska is a diverse and wonderful place. One of the things that makes us unique is our special and world-class ferry system. Often times during the winter I will travel from Kake to Juneau. This trip sometimes goes through Sitka and takes nearly 24hrs as the ferry travels through Peril Straits and back before completing its journey up Chatham and into Auke Bay. Where else in the World can a person travel in such a way where they can comfortably socialize with "real people" over a few Amber's in a quaint lounge, eat a wonderful meal made with love, sit in a cozy chair while basking in the beautiful surroundings and then go to their quaint stateroom for a good night's rest in preparation for the big day in our State's Capitol? No where but here! Our ferries are special and a unique part of our existence. And they're safe too. Our ferries are so special. I feel at home when I am on them. The workers are sincere, helpful, caring and treat both locals and visitors like family. I once received a Christmas card from a Taku bartender, out of the blue, hoping that winter life was treating us well in Kake. Now that's personable. Do we, as a State, want to diverge from this unique charming way of life that we live up here? I certainly hope not! Aside from our beautiful landscape, this is a trait that we possess that separates us from the disinterested and impersonal South. Many of us feel that our hard earned tax dollars are being squandered now a days and that our voices are small. Sure we can vote for someone who seems like they might have my interests in mind. But then what? I guarantee that our Governor doesn't use the ferry like my family does. What am I supposed to do on a long ferry ride in February when it's blowing NE 45kts and I'm trying to get to Juneau or Haines in order to work and put food on the table and our replacement ferry doesn't have the amenities and comforts that the Taku has? Does the State want a traveller from New Zealand to ride that ferry and have nothing to do while stepping over people sleeping on the floor while getting tossed around on a boat not quite made Alaskan Tough? I'd rather have that New Zealander take home a unique and special experience with them. And that unique and special experience is what I want to continue encountering as well. And let's not forget the safety of our school teams traveling these dangerous winter waters either. That's how I want my tax dollars spent.

-Paul Davis



Organized Village of Kake

P.O. Box 316

Kake, Alaska 99830-0316

Telephone 907-785-6471

Fax 907-785-4902 / email KeexKwaan@KakeFirstNation.org

(Federally Recognized Tribal Government serving the Kake, Alaska area)



January 23, 2013

Representative Jonathan Kreiss-Tomkins

State Capitol Room 426
Juneau AK, 99801

Dear Representative Kreiss-Tomkins,

The Organized Village of Kake (OVK) is a federally recognized Indian Tribe under federal law organized pursuant to the authority of the Federal Indian Reorganization Acts of 1934 & 1936 with the IRA Council as the duly elected governing body formed under its *Constitution & By-Laws*. The Organized Village of Kake (OVK) was organized under the Indian Reorganization Act with the current Constitution & By-Laws ratified by the local native population in 1947. Our community is located on Kupreanof Island and depends on our weekly ferry service for medical and economic reasons. It is often our only option for travel when the weather does not permit the small commuter planes from Juneau and Sitka to fly to Kake.

Over the past five years, the Alaska Department of Transportation sought input from the public on what Southeast communities needed out of a new ferry. Over the course of those five years of public involvement, the Department of Transportation decided on a 350 foot Alaska Class Ferry. This ship would be safe, reliable, and comfortable in all seas in Southeast, including Lynn Canal, Chatham Strait, Frederick Sound, and Dixon Entrance. Those plans were approved by the Alaska Legislature, and money was given to DOT to build the new ship. This new ship was under contract with Alaska Ship and Drydock in Ketchikan, and its design was about half completed by this winter. Construction was scheduled to begin early this summer.

A couple weeks ago, Governor Parnell and DOT made a surprise announcement that they were terminating the contract to build the safe, reliable, 350 foot Alaska Class Ferry. Instead, the Governor announced that DOT will now build two smaller, partially open-deck "shuttle" ferries (around 280 feet) as a way to save money. These ferries would be a different design than most current ferries, and would require expensive ferry terminal renovations (about \$15 - \$20 million per terminal). The Governor wants us to believe that two 280 foot ferries and associated ferry terminal improvements would be less expensive than one 350 ferry.

In this decision, the Governor and DOT accepted no public input, and they ignored their own Marine Transportation Advisory Board. They also did not address any concerns for safety, reliability, or comfort in high seas.

For those of us who have taken a smaller ferry in Lynn Canal, Chatham Strait, or Frederick Sound, we know how uncomfortable they can be in high seas. Just this winter alone, the LeConte was canceled four times in Lynn Canal due to high winds and seas.

Letter from OVK regarding ferry service

Page 2

January 23, 2013

It is a disservice to Southeast Alaskans for the Governor to ignore all public involvement from the past 5 years and make a decision on his own to build smaller ferries for the region.


It is a legitimate concern to think that the state will build one of these shuttle ferries and then determine there is not enough money to build the second. If that is the case, Southeast Alaska will be left with one shuttle ferry that is less versatile and less reliable than the originally planned ship.

Our requests are:

- Southeast Alaska needs safe, reliable, comfortable ferries
- DOT needs to maintain the integrity of the public process
- DOT needs to consider *all* communities when building new ships, not just Juneau

Thank you in advance for your consideration of our comments.

Regards,



Joel Jackson
OVK Vice-President

CC to:

House Transportation:

Representative Peggy Wilson (chair): Representative.Peggy.Wilson@akleg.gov

Representative Eric Feige: Representative_Eric_Feige@legis.state.ak.us

Representative Lynn Gattis: Representative.Lynn.Gattis@akleg.gov

Representative Doug Isaacson: Rep.Doug.Isaacson@akleg.gov

Representative Craig Johnson: Representative_Craig_Johnson@legis.state.ak.us

Representative Bob Lynn: Representative_Bob_Lynn@legis.state.ak.us

Representative Jonathan Kreiss-Tomkins: Representative.Jonathan.Kreiss-Tomkins@akleg.gov

Senate Transportation:

Senator Dennis Egan (chair): Senator_Dennis_Egan@legis.state.ak.us

Senator Fred Dyson: Senator_Fred_Dyson@legis.state.ak.us

Senator Anna Fairclough: Senator_Anna_Fairclough@legis.state.ak.us

Senator Click Bishop: Senator.Click.Bishop@akleg.gov

Senator Hollis French: Senator_Hollis_French@legis.state.ak.us

Rebecca Rooney

From: Linda Kadrlik <adventuresafloat@gci.net>
Sent: Thursday, January 24, 2013 7:12 AM
To: Rep. Peggy Wilson
Subject: Ferries

Dear Rep. Wilson,

I am appalled at the actions of the DOT in regards to the plans to completely change the direction of our new ferry. I think very little thought has been put into this, especially when considering the southeast maritime weather and design of our present ferry terminals. There was no public involvement. How could the new commissioner NOT KNOW about the MTAB? It seems like an end run to get the road built up Lynn Canal, which is another environmental and costly disaster in the making. If the costs of the original Alaska Class ferry have risen, perhaps the design could be modified somewhat to reduce the costs and still have the originally planned ferry built. I can't attend the hearing today, but I hope my comments will be taken into consideration when the Transportation Committees discuss this problem. Thanks. Linda

Adventures Afloat
Francis and Linda Kadrlik
4950 Steelhead, Juneau, AK 99801
(800) 3AFLOAT, (907) 789-0111
FAX: (907) 789-3312
<http://home.gci.net/~valkyrie>

Rebecca Rooney

From: Mike Korsmo <mkorsmo@aptalaska.net>
Sent: Wednesday, January 16, 2013 7:59 AM
To: Rep. Peggy Wilson; Rebecca Rooney
Subject: AMHS Takeover
Attachments: LTR MTAB.pdf; 01_04_2013 STIP Amendment 4 Comments.pdf; Governor Parnell.docx

Rep Wilson, Becky,

Since we've worked so well together these past several years i thought Id share my thoughts of what is really going on with DOT and AMHS.

To me it is not a good scenario and does not bode well for the long term viability of AMHS.

Basically the old gaurd at DOT Kemp, Otteson, Hughes, Yost, Potivin etc.

have been allowed to take over. This is the same bunch that has been responsible for some of the screwed up policies that got AMHS going in bad directions in the past. They do not like ferries, Over the past 5 or more years with a huge amount of input from your office. other regional legislators, MTAB, SE Conference. Jim Beedle, AMHS Staff etc. AMHS has stabilized, improved, and come up with some long term plans that will allow AMHS to be the kind of system the people of coastal Alaska desire.

Thanks to your efforts MTAB was established in statute and worked hand in hand with the Administration, AMHS, the legislature, and the public to move responsible goals and budgets forward for the viability of the system.

That all changed very rapidly here recently. I think we started losing control of AMHS when Jim Beedle retired. Jims real reason for retiring was because he could not handle the frustration of fighting the Mile 7 folks mentioned above. He would not state that publicly but did to me privately and i confidentially share that with you. Before our most recent MTAB meeting I asked Neussl how it was going working with DOT. He gave me a stare that basically said "not good" and said he would rather not say.

I have to admire this very well choreographed takeover by this group -They pushed Luiken out who seemed like a very practical man who worked well with MTAB -They convinced the Governor to change directions on the Alaska Class without any input from anybody. I would ask them for specifics on there

170 million price tag for the Alaska Class. A friend from Elliot Bay design and another from within AMHS do not agree with that figure. The cost had risen above the 120 mil which we sort of expected but I dont believe the 170. But it worked for their agenda/ -They asked Neussl to resign and by eliminating the AMHS Deouty Commissioner position they took out MTABs input into leadership hiring and input. They just appointed Yost to oversee AMHS, Commercia Vehicle Inspections and Enforcement, and Special Projects. They do not consider AMHS that important and they do not have someone with a strong marine background in charge, He has an anthropology and education degree.

Their real adenda is Juneau Access. Whether you support it or not the 2 communities of the Upper Lynn Canal do not and neither does at at least half the folks from Juneau This is where their smaller shuttle concept is coming from, the problem is that they need three shuttles to make it work and those vessels as discussed will be very specific to Lynn Canal.

There have been at least 5 cancellations due to wether by the Le Conte this fall in Lynn Canal so you ^{build} ~~buy~~ ferries of that size or even slightly bigger, you add bow doors and you have a recipe for failure.

Let alone the convoluted way you would have to het up the Canal. It will take 2 shuttle ferries to get to Skagway. I would also like to see the efficiencies and costs of building 3 ferries. crewing them, running multiple trips in lynn Canal per day and compare it to the ACF day boat projections. They may say they can build 2 boats for the price of one but, -Will the smaller boats work in Lynn Canal (really Bow doors) -Are they including the costs of a new ferry terminal (Katzehin) and modifications to the existing terminals for their roll on roll off scenario.

-Can they operate 2-3 shuttle ferries on multiple trips as efficiently as 1 Alaska Class -What is the real cost of a shuttle ferry that can operate adequately in the Lynn Canal

The reality is is that once this agenda and concept is put in place it leads them to the over 400 million dollar road (price not including shuttle ferries). There will not be any money for system wide vessel replacement. The Tustemena needs replace, the mainliners need to eventually be replace, At least the Alaska Class was gonna allow us to eventually retire one of the old mainliners and could have also been used as a fill in on other parts of the system.

It is a bad road were going down (no pun intended!) and does not look good for the long term future of AMHS and coastal communities that depend on it

I enclosed a letter I put together for the SEC Transportation Committee for discussion at their last meeting. Keep in mind this has not been adopted bu SEC it was my attempt to get the discussion going and reflects my thoughts on the recent change in direction. I also enclosed Skagways STIP comments. It has been a ploy of this group for years to make amendments to the STIP during holiday breaks so the public does not have time to formulate their response.

Good luck at your hearing and feel free to share this with Sen Egans staff.

MiKe Korsmo

Skagway Assembly

MTAB Board (former chair)

Past SEC President

Past Chair of SEC Transportation Committee

Oh I almost forgot their has been an inquiry to my position on mtab. I will dixcuss that with you and maybe some changes we could make to the sta~~te~~te in the future

January 28, 2013

**The Honorable Sean Parnell
Governor, State of Alaska
Alaska State Capital
Juneau, Alaska, 99801-1182**

**Re.: Alaska Class Ferries
DOT&PF Commissioner Appointment**

Dear Governor Parnell,

With all that has been reported by our local Juneau Empire, statewide radio and television news coverage, I must commend your two recent decisions of;

- **Resizing the Alaska class ferry and**
- **Appointing lifelong Alaskan and department career employee Pat Kemp to the Commissioners position of the Department of Transportation and Public Facility.**

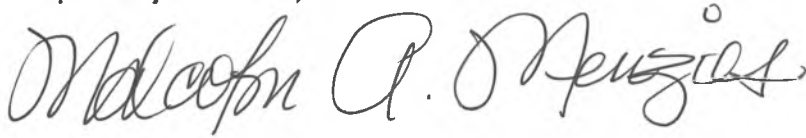
As a 53 year Alaska resident that had owned a private engineering and surveying business for 35 of those years and the majority of my Alaska residence in Southeast Alaska I understand the importance of both the Alaska Marine Highway System and budgets. Since the 2006 genesis of the "Alaska Class Ferry", such a ship had a specific size and purpose. The purpose was to supplement our roadway transportation system in several regions of Southeast Alaska. It was to be a ship of day service with limited running times and crew. It also had a budget that amongst other goals, helped to reduce Alaska's subsistence to the marine transportation system. That ship through the public hearing process had no business adding both crew and passenger state rooms and over 50 feet to its length to the 2006 outlined budget process.

Your recent action to bring this needed project back to both economic and need reality can only be commended.

Your choice of Pat Kemp as Alaska's Commissioner of Transportation and Public Facilities is also a decision that must be commended. Pat is an individual with the engineering education and close to 40 years of experience, almost all being in the transportation field. He has a lifetime of training to lead Alaska's need in ground, air and marine transportation. That training started at the bottom of transportation engineering. Pat Kemp's education, experience and personal abilities led to his rise in leadership. His recent manner and choice of the department's reorganization and Deputies reflects his wisdom in Alaska needs for the department.

Again, your recent decision to appoint Pat Kemp Commissioner of the Department of Transportation and Public Facilities must also be commended. Pat Kemp's leadership, along with his Deputies and key management staff will thrust Alaska's ground, air and marine transportation system's far into the future, serving and aiding ail Alaskan's.

Respectfully submitted,

A handwritten signature in black ink that reads "Malcolm A. Menzies". The signature is written in a cursive style with a small circle above the letter 'i' in "Menzies".

Malcolm A. Menzies, P.E. & L.S.

**xc. Senator Dennis Egan, Chairman, Senate Transportation Committee and Committee Members
Senators Dyson, Fairclough, Bishop and French**

**Representative Peggy Wilson, Chairperson, House Transportation Committee and Committee
Members Representatives Feige, Gallis, Johnson, Lynn and Kreiss-Tompkins**