

**2012 ALASKA
BRIDGE
REPORT &
CENTRAL
COUNCIL
TRANSPORTA-
TION PROGRAM**

<TARGET><BILL></BILL><SUBJECT>2012 ALASKA BRIDGE REPORT
and CENTRAL COUNCIL TRANSPORTATION
PROGRAM</SUBJECT><COMM>STRA28</COMM></TARGET>

Alaska Department of Transportation and Public Facilities

2012 Alaska Bridge Report



June 2012

“Get Alaska Moving Through Service and Infrastructure”

Cover photo of the Sitka Harbor Bridge by Steve Lee, ADOT&PF

Table of Contents

| | |
|--|----|
| Introduction | 1 |
| Bridge Rating | 2 |
| Bridge Program | 6 |
| Maintenance | 7 |
| Rehabilitation/Replacement | 7 |
| Preservation | 7 |
| Design | 7 |
| Scour Monitoring | 8 |
| Seismic Retrofit | 9 |
| Inspection | 11 |
| Bridge Asset Management | 13 |
| Bridge Age and Construction Materials | 13 |
| Project Schedule and Funding | 15 |
| Schedule of Improvements | 15 |
| STIP Bridge Funding | 15 |
| Future Funding | 17 |
| The Alaska Factors | 17 |
| Closing | 19 |
| Appendix A: Glossary | 21 |
| Appendix B: 2011 Structurally Deficient DOT Bridges | 24 |
| Appendix C: Structurally Deficient Bridges in the 2012-2015 STIP | 27 |
| Appendix D: Map of Structurally Deficient and Functionally Obsolete Bridges Statewide | 29 |
| Appendix E: Status of Bridge Work on the National Highway System | 30 |
| Appendix F: Map of Bridges Seismically Retrofitted Between 1996 and 2009 | 31 |

Introduction

The Alaska Department of Transportation and Public Facilities is responsible for inspecting 983 bridges on publicly-owned roads in Alaska under requirements established by the Federal Highway Administration (FHWA). These include 805 bridges owned by the Department, 23 owned by other state agencies, and 155 owned by cities and boroughs. Federal agencies inspect the 186 bridges under their jurisdiction, while the Alaska Railroad Corporation is responsible for the inspection of most bridges on the rail system. This report addresses all bridges for which the Department has inspection responsibility, but focuses on the 805 bridges owned and operated by the Department.

The federally funded bridge inspection program has not applied to pedestrian and bicycle bridges, even if within the road right-of-way; these bridges have been inspected and periodically rehabilitated or replaced using another source of funding. The department is analyzing the recently-adopted surface transportation bill, "Moving Ahead for Progress in the 21st Century", or MAP-21, for new requirements pertaining to the bridge program.

The 805 bridges the department is responsible for maintaining includes 70 culverts twenty feet or greater in diameter, 7 drive-down ramps to seaplane floats and 23 ramps at Alaska Marine Highway System terminals. All of these structures in the department's inventory are in FHWA's National Bridge Inventory System (NBIS), however, only road and highway bridges are subject to discussion in this report. Drive-down ramps at small boat harbors are not included in the inventory.

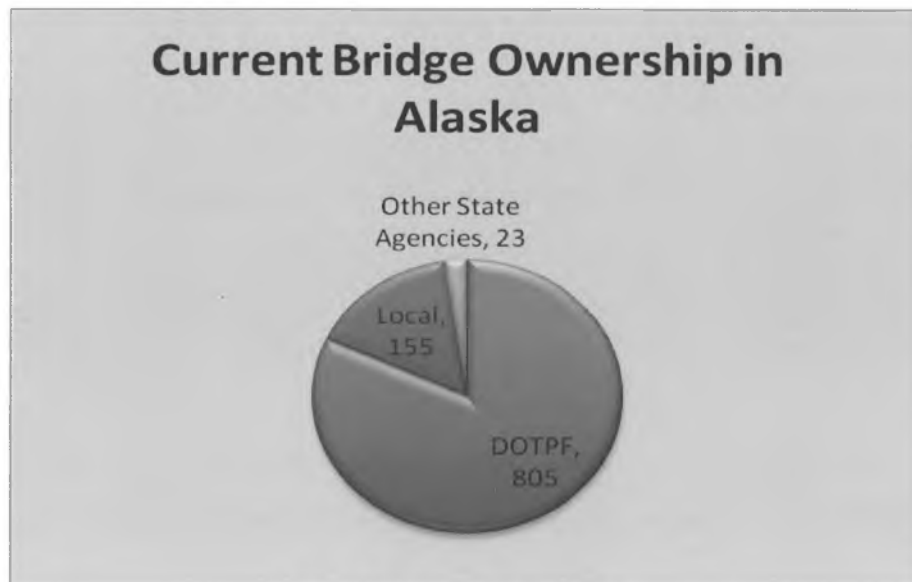


Figure 1. Bridge Ownership

Alaska bridges are in overall good condition. Department engineers annually inspect about 500 bridges on public roads to spot problems and engage in a corrective work program that assures Alaska bridges are safe¹. Bridge inspection and remediation is ongoing and will always face challenges. About one-third of the bridges in the state are past the mid-point of their 75-year design life. Industrial development, including mining and oil or gas field development and future construction of oil or natural gas pipelines, may require rehabilitation and replacement of existing bridges to carry the significant traffic loads such development generates. Population growth, increased traffic volumes and environmental factors such as runoff and thawing permafrost also place demands on the bridge inventory.

The FHWA funds almost all bridge rehabilitation and replacement through the Highway Bridge Program and other highway funding sources. As this report reveals, this source is no longer sufficient to meet all of Alaska's bridge needs. Still, the safety of the traveling public, and the vital role played by the highway system supporting business, industry and society, depends on the department's diligence in the inspection, preservation and maintenance of the state's bridge inventory.

Bridge Rating

The FHWA has established national standards for the structural condition of bridges in the *Recording and Coding Guide for the Structure Inventory and Appraisal of the Nation's Bridges*. The FHWA has developed standards and methods to determine whether a bridge is "structurally deficient" (deteriorating), or "functionally obsolete" (out-of-date design). The FHWA bases these classifications on evaluation of the bridge deck (the driving surface); the bridge superstructure (the components of the bridge supporting the deck such as the girders); and the bridge substructure (the components of the bridge below the superstructure such as the abutments and piers).

Structurally Deficient. A bridge is structurally deficient if inspection reveals that primary load-carrying elements are in poor (or worse) condition due to deterioration and/or damage. Likewise, a bridge is structurally deficient if the adequacy of the waterway opening provided by the bridge is determined to be extremely insufficient to the point of causing intolerable traffic interruptions (for example, water spills over the roadway). A structurally deficient rating does not mean that a bridge is likely to collapse or that it is necessarily unsafe.

¹Bridge is defined in Appendix 'A'. Federally-owned bridges are excluded from all data in this report.

Functionally Obsolete. A bridge that does not meet the current design standards (for example, for lane width, number of lanes, shoulder widths, vertical clearances or load capacity) is functionally obsolete. While structural deficiencies typically result from deterioration of the bridge components, functional obsolescence generally results from changing traffic demands on the structure. Bridges conform to the design standards in place at the time they are constructed. The degree of difference between current design standards, and those in place for a bridge constructed at a prior time, determines whether a bridge receives a functionally obsolete classification.

While the terms “structurally deficient” and “functionally obsolete” can imply unsafe conditions, bridges with these classifications are in safe operating condition to meet the required level of service, or else they are weight-restricted or lane-restricted (reduced to a single lane) to assure safe operation.

The two graphs below show the percentage of the bridge inventory that is structurally deficient and functionally obsolete by year from 2000 to 2011. Figure 2 shows DOT-owned bridges only, while Figure 3 shows DOT-owned bridges as well as bridges owned by municipalities and other state agencies.

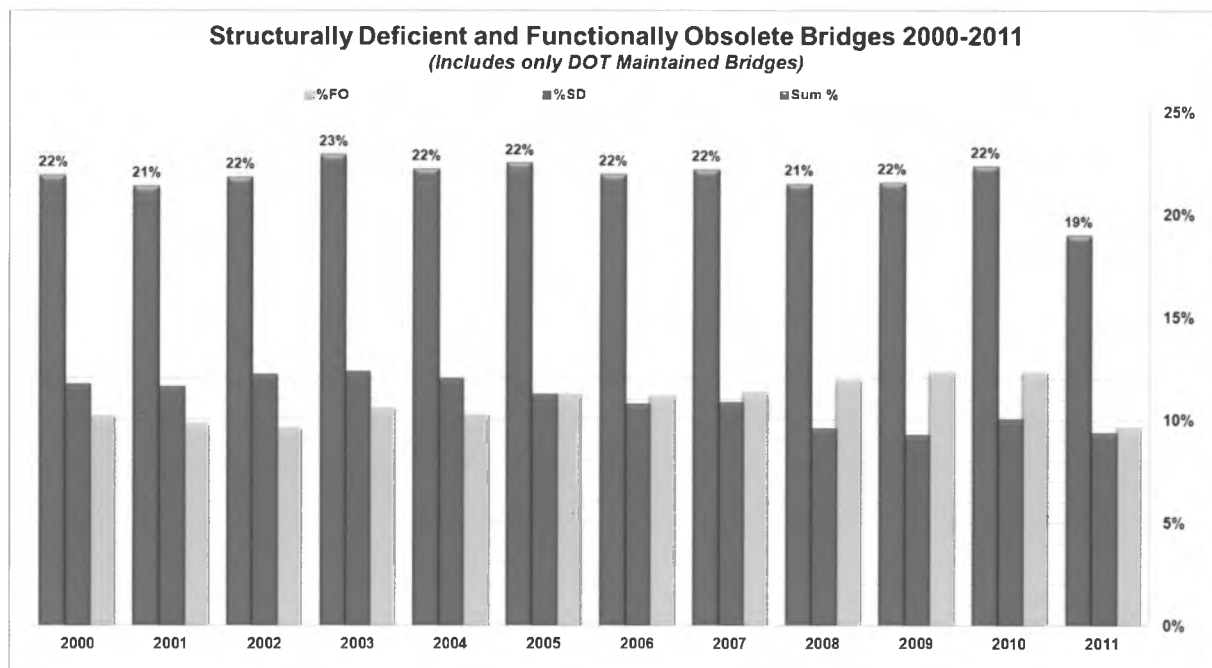


Figure 2. Structurally Deficient and Functionally Obsolete Bridges DOT-Owned Only

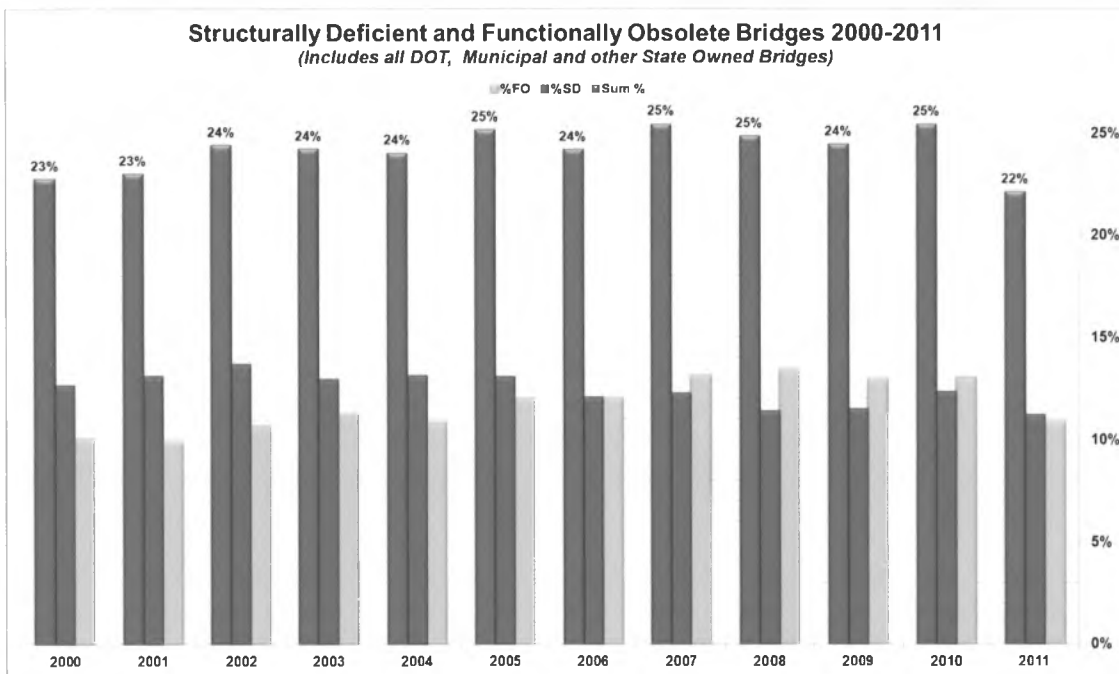


Figure 3. Structurally Deficient and Functionally Obsolete Bridges DOT and Other

The Department evaluates bridges using FHWA numerical rating formulas that indicate a bridge's condition and its sufficiency.

Condition Rating. The condition rating describes the existing, in-place status of a bridge component such as the deck, superstructure or substructure, compared to the bridge's original, or as-new, condition, using a '0' to '9' scale, 9 equaling excellent and 0 equaling failed. Bridge inspectors assign condition ratings by evaluating the severity of the deterioration of individual bridge components and the extent to which it affects the rated component.

The Department annually calculates the deck area of structurally deficient bridges. Between 2000 and 2011, total deck area of state and municipal bridges increased from 6,052,366 square feet, to 6,714,637 square feet, an 11% increase. During the same period, the deck area of structurally deficient bridges decreased from 850,000 square feet to 679,000 square feet. Viewed as percentages, the deck area of structurally deficient bridges decreased from 14% of total deck area in 2000, to 10% in 2011.

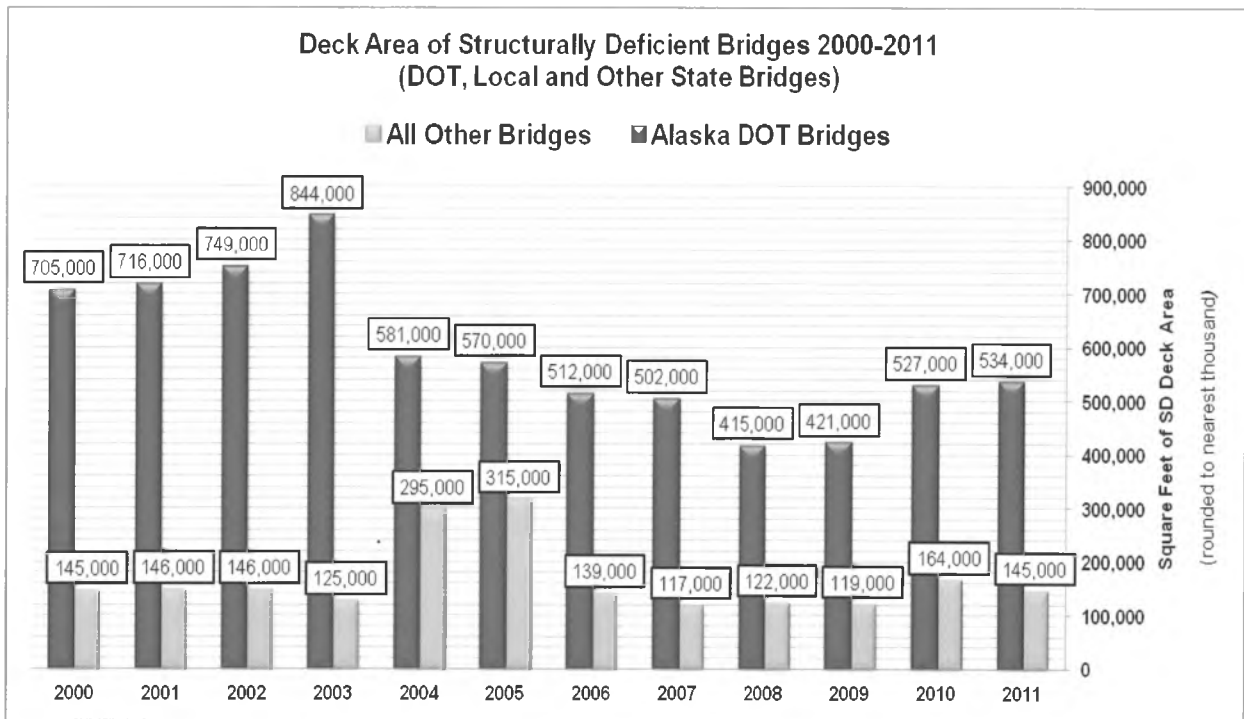


Figure 4. Deck Area of Structurally Deficient Bridges 2000-2011

DOT-owned bridges saw a 50% reduction in the deck area of structurally deficient bridges between 2003, when the metric was at a high point, and 2008. Since 2008, the deck area of structurally deficient bridges has increased by 29%. Structurally deficient bridges identified in the STIP for replacement or rehabilitation between 2012 and 2015 should help reduce this number. As shown in Figure 5, the deck area of structurally deficient bridges can change significantly from year to year. As bridges are rehabilitated or replaced, other bridges will continue to deteriorate with age, adding an unknown amount of deck area to the structurally deficient total.

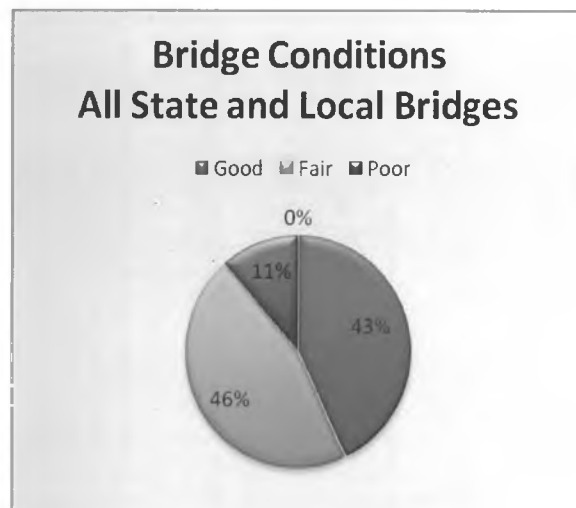


Figure 5. Condition of Alaska Bridges

Using numerical rankings from the condition rating (7-9=good; 5-6=fair; 0-4=poor), Department engineers classify the condition of Alaska bridges as good, fair, or poor condition. Bridges in the good-condition category are in very good to excellent condition and may have minor problems that can be addressed with preservation or maintenance practices. Bridges in the fair-condition category are structurally sound, but show minor deterioration, cracking, spalling or scour that can be corrected through repair. Bridges in the poor-condition category show advanced deterioration, may not be structurally sound, are candidates for rehabilitation or replacement, and may require weight or lane restrictions.

Sufficiency Rating. The FHWA bases funding for bridge rehabilitation and replacement on the sufficiency rating and condition ratings. The sufficiency rating is a combined numerical rating formula based on structural adequacy and safety, serviceability and functional obsolescence, and essential importance for public use. The result of the formula is a percentage in which 100% represents an entirely sufficient bridge, and 0 represents an entirely deficient bridge.

To qualify for federal bridge replacement funds, a bridge must have a sufficiency rating of <50, and to qualify for federal bridge rehabilitation funding, a bridge must have a sufficiency rating <80. In addition, the deck, superstructure or substructure must be in poor condition. Bridges rated 80 -100 do not qualify for federal bridge rehabilitation or replacement funds, but are eligible for state and other federal funding.

Bridge Program

The bridge program comprises a group of activities from maintenance, preservation and design, to inspection and seismic monitoring and retrofitting, meant to build and preserve a safe, functional inventory. Bridge inspection is a crucial component of the program.



Figure 6. Deteriorating Concrete Trail River Bridge

Maintenance. This includes ongoing tasks such as overlaying bridge decks, restoration of guardrails on the bridge approaches, sweeping, paint striping, patching, or repairing or replacing faulty expansion joints. The Department's bridge crews conduct an annual preventive maintenance program.

Rehabilitation. This includes replacement of deteriorated bridge elements caused by rusting or spalling (flaking or crumbling concrete), repair of collision damage, painting, replacing damaged decking and replacing or repairing structural elements.

Replacement. Economic and lifecycle analysis may indicate that bridge replacement is the most cost-effective choice.

Preservation. Bridge preservation comprises the Department's pro-active efforts to keep bridges safe and operational. Distinguished from maintenance, preservation comprises work that aims to extend bridge service life and forestall the need for more corrective, reactive maintenance, and includes activities such as painting, cleaning joints to prevent deterioration and/or failure and sealing surfaces to prevent water penetration. Continuing implementation of asset management practices based on data entered in the department's bridge management system (see PONTIS below) will assure timely attention to preservation and help control costs.

Design. Design is an important component of bridge preservation that accounts for environmental conditions, traffic volumes, vehicle weight and other factors, and helps to assure longer bridge life, greater ease of routine maintenance and greater safety. The Department is currently preparing a bridge manual that emphasizes design as a means of assuring bridge safety and service life.



Figure 7. Consulting Engineers Inspect the Rex Bridge

Scour Monitoring and Retrofit. “Scour” is the engineering term for the erosion caused by water removing the material supporting the bridge foundation (the piers and abutments). The most common cause of bridge failures is from floods scouring streambed material from around bridge foundations. Bridges that are structurally vulnerable to scour are termed “scour-critical”.

National Bridge Inspection Standards (23 CFR 650) require states to identify scour critical bridges and their owners to prepare a plan of action to monitor scour conditions and to address potential deficiencies and critical findings. Bridge scour countermeasures may include increased inspection frequencies, the installation of active monitoring systems, and structural improvements to resist scour.

The Department has identified 112 “scour-critical” bridges in 2011, one fewer than reported in 2010, with the replacement of the Tanana River bridge near Tok. The department closed bridge #339 at Milepost 36 of the Copper River Highway in 2011 due to extreme scouring. A shifting river channel has resulted in flows beneath the bridge of 85,000 cubic-feet-per-second (CFS), well in excess of the bridge’s design flow of 18,500 CFS. Design is underway for a replacement structure.

DOT engineers inspect state-owned scour-critical bridges annually, rather than the 24-month cycle used for routine bridge inspections. Nineteen bridges feature remote scour monitoring systems that provide near real-time scour data at a bridge pier(s). The Department has also collaborated with other agencies, notably the U.S. Geological Survey, to conduct complex scour and bridge hydraulics studies at selected bridges.

The 2012-2015 STIP has programmed \$3,800,000 over four years to sustain the Bridge Scour Monitoring program.



Figure 8. Active Scour on Bridge #339, Copper River Highway



Figure 9: Earthquake Damage on the Richardson Highway

Seismic Bridge Retrofit. Alaska is the most seismically active state in the union. The Department implemented a seismic retrofit program for Alaska bridges in 1995, using seismic hazard data from the U.S. Geological Survey. This data, together with a seismic vulnerability assessment of Alaska bridges and a determination of priority highway routes, has resulted in the prioritization of bridges for seismic retrofit.

Consistent with national standards adopted by the American Association of State Highway Transportation Officials (AASHTO), the Department



Figure 10. Phase 1 Seismic Retrofit Using Cable Restrainers
Twenty Mile River Bridge

retrofits bridges to further prevent the possibility of collapse during an earthquake, with public safety the foremost consideration. The Department designs new bridges to the “no collapse” standard contained in the current AASHTO specifications.

The department has adopted a two-phase seismic retrofit program. Phase One of the program addresses the most critical bridge deficiencies that can be accomplished for the least cost. Typically, the department can retrofit about ten bridges annually with a budget of approximately \$2.4 million. Phase One retrofits improve a bridge’s anticipated seismic performance but do not necessarily bring the bridge into compliance with current “no collapse” standards. The intent of Phase One is to retrofit as many bridges as is economically feasible with the available funds.

Phase 2 of the retrofit program is intended to address vulnerabilities in the bridge columns and foundations. These deficiencies are typically much more expensive to correct, resulting in fewer Phase 2 retrofits for the same amount of funds. The department has addressed many Phase 1 priorities and has completed Phase 2-type improvements on bridges in Kodiak and Sitka. Phase 1 work will not cease, but overall priorities may shift toward Phase 2 work, particularly for



Figure 11. DOT Inspectors on the Kuskalana Bridge

critical bridge links on the National Highway System.

The 2012-2015 STIP has programmed \$8,400,000 over four years to implement the Seismic Bridge Retrofit program.

Inspection. Regular inspection of the state’s bridges provides up-to-date information on their physical condition, ensures public safety, and provides a factual basis for public investment in bridge preservation, replacement, and rehabilitation. Federal regulations mandate bridge inspections on a 24-month interval for the above-water, accessible portions of the bridge, and on a 60-month rotation for the portion of bridges that is continuously underwater.

The state’s bridge inspection program seeks compliance with the National Bridge Inspection Standards, to assure high-quality inspections. Bridge inspections can range from routine to in-



Figure 12. Contract Dive Inspector at the Kenai River Bridge

depth, depending on a bridge’s individual characteristics and needs. The Department uses its bridge design engineers as inspectors, giving the design group valuable information on bridge conditions and performance based on use, “wear and tear” and other factors.

Engineers may inspect smaller bridges on foot, while others require the use of a special under-bridge-inspection vehicle with a jointed arm and bucket, or platform, that allows access to otherwise unreachable locations. The length and size of a bridge, weather conditions, and

location on the road system can vary the length of an inspection from an hour to as much as several days.

DOT inspects approximately 500 bridges per year. Inspectors enter data into PONTIS, an FHWA-approved bridge management system that stores inspection data for each of the structural elements examined (see below). Forty-five transportation agencies in the United States use the PONTIS program.

Alaska traffic volumes are low by national standards, making traffic-generated deterioration a less significant factor in Alaska than in other states. Additional factors affecting Alaska bridges include age, rot and related deterioration of timber bridges, vehicle collisions that can sometimes result in structural damage, scour and overweight loads, and environmental damage from corrosion and the effects of freeze-thaw cycles.

A bridge closure or posting of reduced load capacity typically occurs when advanced deterioration or impact damage reduces structural capacity below state legal loads. Some bridges designed under old codes and standards may also require load posting. Regular inspections typically spot problems in time for implementation of corrective measures, making closure a last option. Currently, eight DOT-owned bridges are closed to traffic. Six of the eight



Figure 13. Highway Overpass Damaged by a Vehicle Collision

bridges are on the Copper River Highway, five of them unreachable as they are past bridge # 339, which is closed due to extreme scour. An additional fifty-six DOT-owned bridges have load and/or lane (ie, one-way traffic) restrictions (a number that includes eight seaplane float ramps). Three of these bridges –the Trail River, Falls and Ptarmigan Creek bridges- are being replaced during 2012, while almost all the rest are on (relatively) low-volume rural or local routes.

Bridge Management System (PONTIS). PONTIS is a software program that houses, and has the capability to analyze, data collected on the condition of bridge elements including the deck, beams, piers, railings and other features. Primarily used to inventory bridge conditions at present, PONTIS has the potential to support the department’s emerging commitment to asset management by analyzing condition data to model bridge deterioration and recommend optimal preservation strategies.

PONTIS stores complete bridge inventory and inspection data, including detailed conditions of bridge elements. It can identify system-wide preservation and improvement strategies for use in evaluating the needs of all bridges in the system. It can make project recommendations that derive maximum benefit from available funds, report on system-wide and project-level results, and forecast system-wide and individual bridge life-cycle deterioration and costs.

After entering inspection data, PONTIS can be used for maintenance tracking and federal reporting. PONTIS integrates the department’s goals for public safety, risk reduction, user convenience and preservation of investments, to produce both budget and maintenance policies. It provides an organized process for allocating resources by calculating both the costs and the benefits of maintenance and preservation strategies against more costly improvements or replacement.

Full utilization of PONTIS will enable the department to make sound, defensible, and repeatable investment decisions for the 805 DOT-owned bridges. It will support a preservation approach by identifying a cost-effective strategy for preserving the overall bridge system and quantifying the costs of deferring needed maintenance, repair, and rehabilitation activities.

Bridge Age and Construction Materials

The state’s bridge inventory continues to age. As of November 2011, 50% of publicly owned bridges in the state are 33 years or older and 10% are more than 50 years old. This indicates that about one-third of the publicly owned bridges in the state are past the mid-point of their 50 to 75-year design life. Thus, it is critical to address the existing inventory of structurally deficient bridges, as over time, additional bridges are likely to show signs of distress as they deteriorate with age.

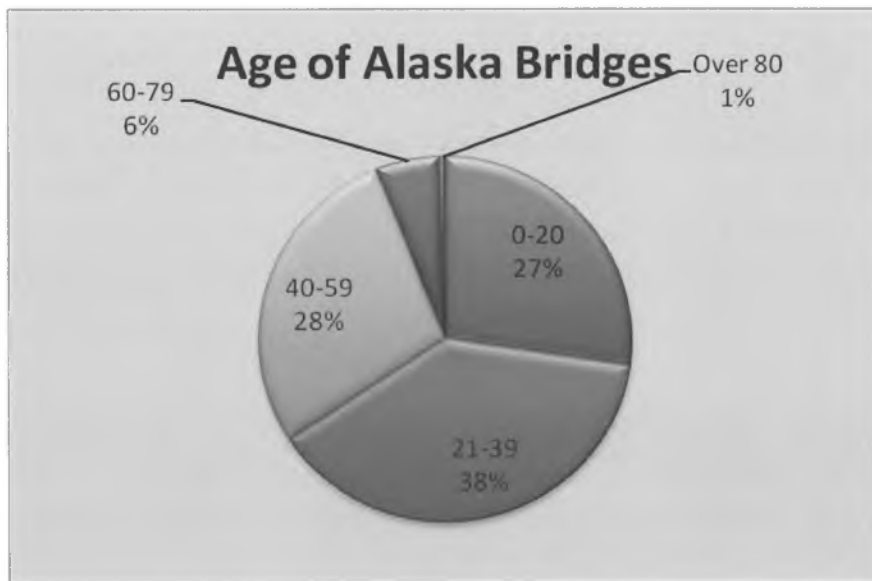


Figure 14. Age of Alaska Bridges

The majority of publicly owned bridges in Alaska have been constructed using steel, followed by pre-stressed concrete bridges, then timber bridges, which typically comprise the older and shorter spans. Because of their relatively low maintenance requirements and relatively low cost, pre-stressed concrete girders are the preferred choice for new construction. See Figure 15 for bridges classified by construction material.

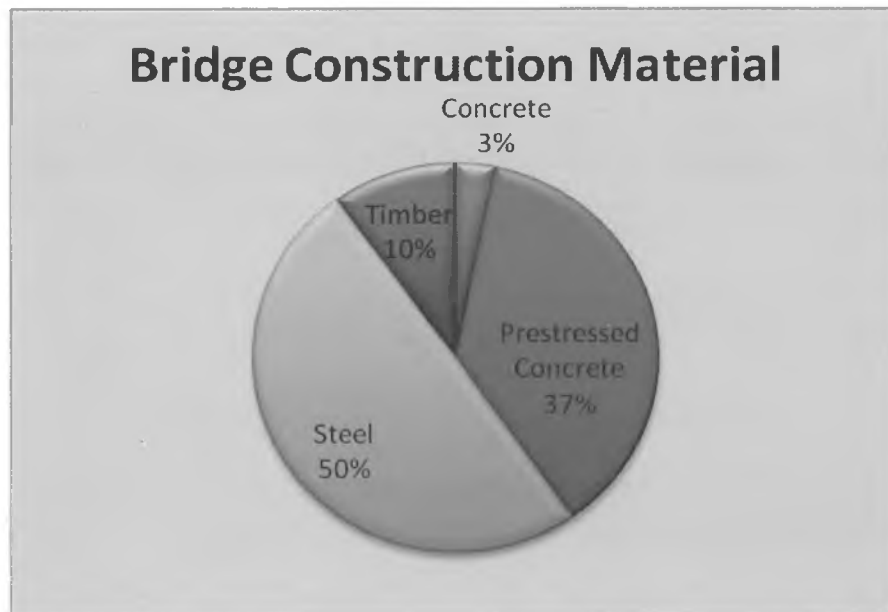


Figure 15. Bridge Construction Material

Project Schedule and Funding

Schedule of Improvements. As of December 2011, seventy-two DOT-owned bridges were classified as structurally-deficient (See Appendix B). The STIP, or Statewide Transportation Improvement Plan, has identified thirty-one of these bridges for replacement or rehabilitation between 2012 and 2015 (See Appendix C).

Historically, bridge rehabilitation and replacement has occurred in connection with highway improvement projects; however, recent national attention to bridge conditions has resulted in increased funding for 'stand-alone' bridge projects. One of the factors leading to this shift in strategy in Alaska is the need to improve the state's infrastructure for support of energy and resource development, together with the recognition that the backlog of deficient bridges was growing too rapidly and required a greater emphasis on bridge rehabilitation and replacement.



Figure 16. New and Old Tanana River Bridges

STIP Bridge Funding. The STIP has identified \$12,280,950 annually in federal funding between 2012 and 2015 for inspection, monitoring, rehabilitation and replacement of bridges eligible for federal highway bridge program funding. This is in addition to funds for seismic retrofit, scour monitoring and specific bridge projects. Federal bridge funds in the 2012-2015 STIP average \$39,306,492 annually for all bridge work, from \$52,257,570 forecast in 2013, to \$26,057,593 forecast in 2015.

Bridge rehabilitation and replacement comes primarily from the FHWA, but also from other fund sources including federal earmarks, state general fund appropriations (including matching funds) and state general obligation bonds.

Bridge replacement projects, once authorized, can take a significant number of years to complete and to count toward a reduction in structural deficiency. The Tanana River Bridge on the Alaska Highway, completed in 2010, illustrates this fact. Despite being a high priority, and with several special steps taken to accelerate the project, the timeline below documents a seven-year effort before the bridge could be included in a measurement of satisfactory structural condition.

| Development Timeline: Tanana River Bridge | |
|---|---|
| 2004 | First budgeted for design |
| 2008 | NEPA Environmental Assessment approved |
| 2008 | Design approved and federal funding for construction approved |
| 2009 | Construction begins |
| 2010 | Construction ends, open to traffic |
| 2011 | Inspected and placed on inventory in 2011 |

The lengthy timeframe resulted from environmental factors such as fisheries, wetlands, archeological sites and wildlife corridors, as well as the fact that the bridge was constructed during World War II, and as an historic structure underwent a lengthy historic analysis. The federal-aid process using federal funding is also a factor that contributes to project duration.

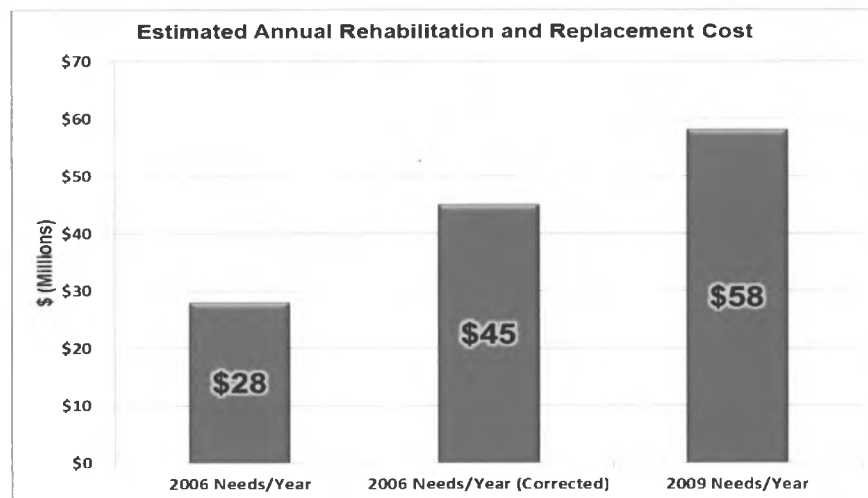


Figure 17. Long-Range Transportation Plan Data Refresh December 3, 2010

Source: Long-Range Transportation Plan Data Refresh, Dye Management, December 3, 2010

Future Funding. While the STIP has identified \$157,000,000 in federal funding between 2012 and 2015 for bridge work eligible for federal highway bridge program funding, the actual level of funding required to address bridge conditions in the state is greater.

The funding level for bridges established in the Statewide Long-Range Transportation Policy Plan, based on data updated in 2010, shows the need for an annual investment of \$58 million per year over the next twenty years to achieve the lifecycle management goals of the plan. This figure is greater than the \$45 million in annual expenditures identified in the Plan in 2006 as corrected, and significantly greater than the \$28 million originally forecast.

DOT has few options for meeting this identified need. The Department can hope that additional programmatic funding for bridges above current levels will be available at the federal level, or that the congressional delegation earmarks additional funds for bridge rehabilitation and replacement. It can request additional state General Funds on a sustained basis until the need is met, or increase the commitment to bridge work in the Statewide Transportation Improvement Plan, but at the expense of other federally funded projects.

With Congress and the FHWA pressing for a more rigorous asset management approach from each state, it is likely that a sustained and higher level of funding must be found to ensure that the bridge inventory can be brought to an overall good condition.

The Alaska Factors

Lack of Redundancy in the Highway System. It is vital to maintain the bridges that link Alaska's surface transportation routes. Unlike other states, Alaska does not have a high degree of redundancy, or alternate routing, in its highway system. If critical bridges are out of service, depending on location it can result in severe constraints in the movement of goods and people. This heightens the importance of bridge inspections and the related investments that help maintain a highly functioning surface transportation system.

Natural Resource Development. Energy and metal price increases are providing a renewed focus on the constraints of existing bridges and their ability to handle large module and construction loads for the energy and resource development industries. Bridges on major NHS routes that provide the corridors to these developments cannot be the limiting factors within the highway infrastructure. Many of these bridges were constructed in the 1940's and 1950's and are reaching the end of their design life.

They are either structurally deficient or functionally obsolete and need to be replaced to meet current use needs. See Appendix E for a list of bridges on National Highway System routes scheduled for work.

Environmental Factors. Alaska's environment presents unique conditions. Freeze-thaw cycles, coastal storms, melting permafrost, harsh winter conditions, the high potential for earthquakes, all pose challenges to bridge designers, to the engineers charged with bridge inspection and preservation, and to the maintenance crews. Also, due to widespread steep terrain along many high-velocity rivers and streams, scour of bridge foundations is more prevalent in Alaska than in many other states.



Figure 18. New and Old Gustavus Causeways

Specialized Structures. Significant travel occurs in the state via the state ferry system and by seaplane. The state's ferry vessels link to the uplands at state ferry terminals with ramps that the FHWA classifies as bridges in the National Bridge Inventory System (NBIS). The FHWA classifies these specialized structures as functionally obsolete because they handle only one-way traffic at low speeds; however, they are well suited to their intended purpose and are safe to use.



Figure 19. Ketchikan Ferry Terminal Ramp



Figure 20. Hoonah Seaplane Float Ramp

Likewise, many seaplane floats in the state have drive-down ramps for delivering freight, passengers and luggage directly to the aircraft. These ramps, also in the NBIS, are not designed to FHWA bridge standards as they are usually one-lane wide, handle low volumes of traffic and only accommodate light-duty vehicles such as four-wheelers, vans and pick-up trucks. Discussions with the FHWA may lead to de-listing seaplane float ramps from the NBIS. However de-listing would mean that another funding source will be needed to keep up with inspections and rehabilitation or replacement needs.

Inclusion of these specialized structures in the NBIS results in their classification as functionally obsolete, despite their high suitability for their intended purpose. It demonstrates that broad national indicators of infrastructure condition, such as the NBIS rating system, can misstate actual conditions.

Short Inspection Season. The Department conducts bridge inspections seasonally between April and September. The short inspection season, along with a vast geographic area and many bridges located in rural and remote areas, places unique demands on the program. Notwithstanding, DOT engineers inspect an average of 500 bridges per year.

Closing

An effective bridge design, inspection and preservation program is essential for the safety and security of the traveling public, and for the social, commercial and economic welfare of the state. As limited federal dollars cover growing infrastructure needs, partnerships will become increasingly important for the implementation of the state's highway program, including

rehabilitation, replacement and construction of the state's bridges. Industry, government at the federal, state and local levels, DOT leadership together with staff on the "front lines", and the Legislature, must build relationships that foster success and the wise and efficient use of available funds.

To ensure the long-term reliability of the highway system overall, adequate funding for bridge related work is vital. The state must continue to preserve existing inventory, and replace aging inventory, particularly on high-volume routes and on routes vital to state commerce.

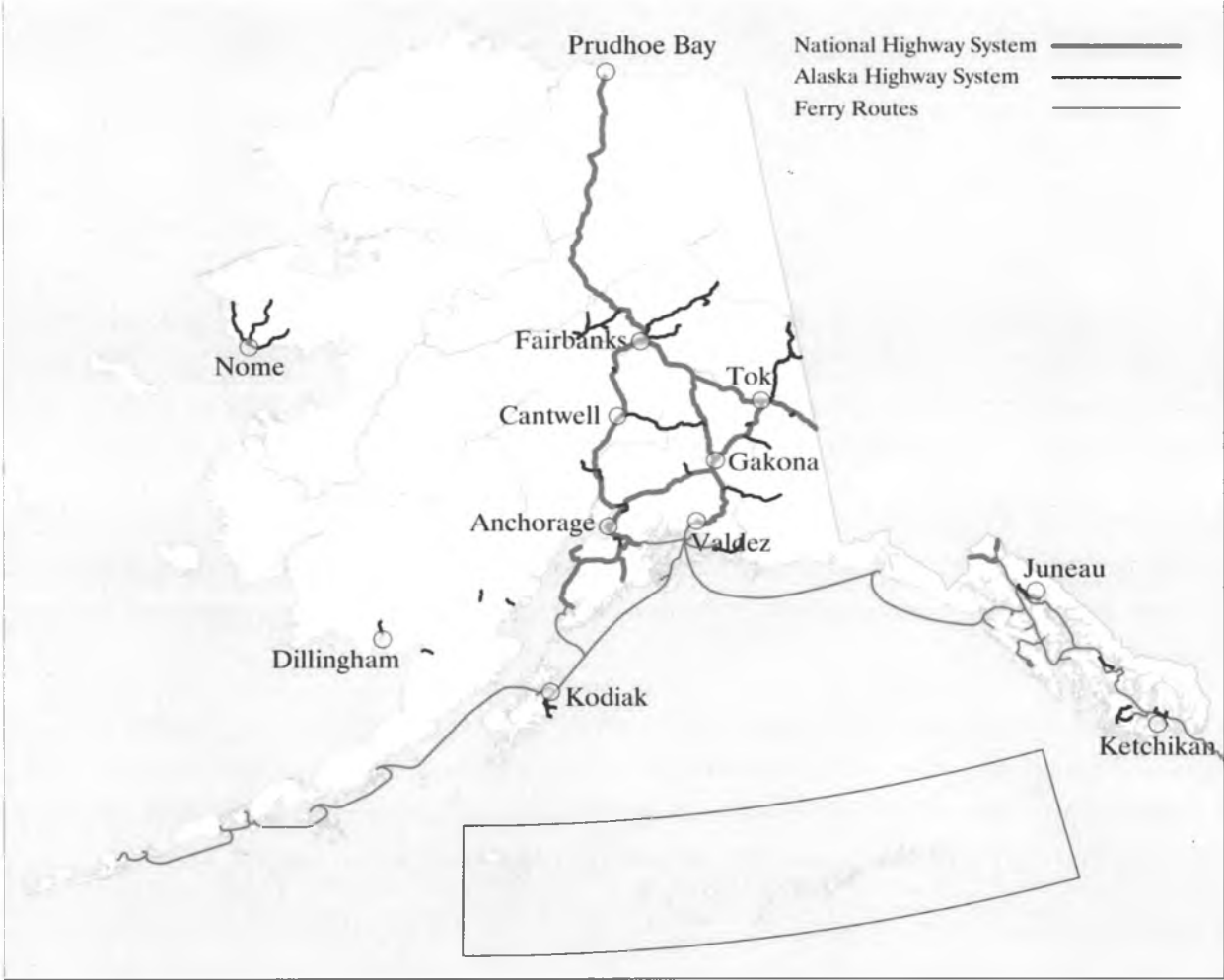


Figure 21. State Highway System Map

Appendix A: Glossary of Terms

Asset Management. Asset management is a business model based on maximizing system performance, minimizing lifecycle costs, improving customer satisfaction, and assuring measurable performance standards. State Departments of Transportation recognize it as a valuable approach to preserving assets at a time of growing demand and shrinking resources.

Bridge. Bridges referenced in this report are structures carrying highway traffic which are 20 feet or longer. This may include multiple pipe culverts where the soil separating adjacent pipes is less than half the adjacent pipe diameter.

Functionally Obsolete. A bridge that does not meet the current design standards for lane width, number of lanes, shoulder widths, vertical clearances or load capacity, presence of guardrails on the approaches, or for some other feature, is considered functionally obsolete. A functionally obsolete bridge may be perfectly safe to use, just out-of-date.

Maintenance. Ongoing, routine tasks such as restoration of guardrails on the bridge approaches, sweeping, paint striping, patching, or repairing or replacing faulty expansion joints, typically performed by department maintenance crews.

National Bridge Inspection Standards (NBIS). These are the federal regulations that establish the requirements for inspection procedures, frequency of inspections, qualifications of inspectors, inspection reports, and load rating. The NBIS applies to all bridges longer than 20 feet on public roads.

National Bridge Inventory. Structural inventory and rating information collected by the states and submitted to the Federal Highway Administration to fulfill the federal NBIS requirement.

National Highway System. Those elements of the surface transportation network that are designated by Congress. These include The Dalton, Parks, Glenn, Seward, Richardson, Alaska, Glacier, Klondike, Haines, Steese and Tok Cutoff Highways, a dozen ferry terminals, and major rail, air and marine port facilities linked to these highways and terminals.

Off-System Bridge. This is a bridge that is off the federal-aid system. However, as part of the Highway Bridge Program (HBP), States are required to expend not less than 15 percent of the amount apportioned . . . each fiscal year for eligible projects on bridges located off the Federal-aid highway system, unless the State has inadequate needs to justify the expenditure. Typically, but not always, these are city and/or borough-owned bridges.

On-System Bridge. This is a bridge that is on the federal-aid system and qualifies for federal program funding through the FHWA. Typically, but not always, these bridges are state-owned.

PONTIS. PONTIS is a bridge management system software program that stores complete bridge inventory and inspection data, including detailed conditions of bridge elements. It can calculate system-wide preservation and improvement strategies for use in evaluating the needs of each bridge in the system. It can make project recommendations that derive maximum benefit from available funds, report on system-wide and project-level results, and forecast individual bridge life-cycle deterioration and costs.

Preservation. Preservation comprises work that aims to extend bridge service life and forestall the need for more expensive repair or rehabilitation.

Rehabilitation. Bridge improvements that exceed bridge preservation and regular maintenance activities, and which include retrofitting or replacing decking and/or structural elements.

Scour. Erosion caused when floodwaters or swiftly flowing waters wash sand, gravel and/or rocks away from bridge foundations. It is one of the three main causes of bridge failure in the country and a significant maintenance concern in Alaska, where floodwaters can pose significant risks.

Seismic Retrofit. The improvements made to existing bridges by which they are more resistant to damage or collapse resulting from earthquakes.

Spalling. Concrete that breaks up, flakes or becomes pitted. This is often the result of environmental factors such as freezing and thawing that stress and damage the concrete. On a low level, concrete spalling can be purely cosmetic in nature. However, it can also result in structural damage when the reinforcing steel (rebar) inside the concrete becomes exposed.

STIP: Statewide Transportation Improvement Program. The STIP is the Department's federally mandated plan for initiating federal aid highway projects. Projects must be included in the STIP and approved by the FHWA (and, in the case of urban projects, the Federal Transit Administration), before development authorization is granted. The STIP is a dynamic document, with regular revisions.

Structurally Deficient. A bridge is considered structurally deficient if ratings for the deck (driving surface), superstructure and substructure are poor. Examples of poor condition include corrosion that has caused significant section loss of steel support members, movement of substructures, or advanced cracking and deterioration in concrete bridge decks.

Appendix B

Structurally Deficient DOT-Owned Bridges December 31, 2011

| <u>Bridge/Number</u> | <u>Route</u> | <u>Year Built</u> | <u>Priority</u> | <u>Status</u> |
|--------------------------|---------------------|-------------------|-----------------|---------------|
| Peterson Creek-383 | Dotson Landing Rd | 1940 | 1 | Design/ROW |
| Livengood Creek-229 | Old Elliott Highway | 1959 | 2 | Design |
| No Name Creek-327 | Halibut Point Road | 1959 | 3 | Design |
| Banner Creek-526 | Richardson Highway | 1975 | 4 | Design |
| Slana River-654 | Tok Cutoff Highway | 1951 | 5 | Design |
| Tulsona Creek-1250 | Tok Cutoff Highway | 1974 | 6 | Design |
| Tok River-663 | Tok Cutoff Highway | 1963 | 7 | Design |
| Hyder Dock Trestle-1238 | Salmon River Road | 1923 | 8 | Construction |
| Twenty Mile River-634 | Seward Highway | 1967 | 9 | Design |
| S. Fork Anchor River-666 | Sterling Highway | 1959 | 10 | Design |
| Riley Creek-695 | Parks Highway | 1969 | 11 | Design |
| Klehini River-1216 | Porcupine Crossing | 1969 | 12 | Design/ROW |
| Water St. Viaduct-797 | S. Tongass Highway | 1955 | 13 | Construction |
| Hoadley Creek-725 | S. Tongass Highway | 1957 | 14 | Design |
| Snake River-881 | Nome | 1979 | 15 | Construction |
| Tolsona Creek-552 | Glenn Highway | 1950 | 16 | Construction |
| Mendenhall River-737 | Glacier Highway | 1965 | 17 | Design/ROW |
| Gerstle River-520 | Alaska Highway | 1944 | 18 | Planning |
| Phelan Creek-579 | Richardson Highway | 1958 | 19 | Design |
| Portage Creek #1-630 | Seward Highway | 1966 | 20 | Design |
| Portage Creek #2-631 | Seward Highway | 1967 | 21 | Design |

| | | | | |
|-----------------------------|----------------------|------|----|--------------|
| Peterson Creek-636 | Seward Highway | 1967 | 22 | Design |
| Virgin Creek-638 | Seward Highway | 1966 | 23 | Design |
| Placer River Overflow-627 | Seward Highway | 1967 | 24 | Design |
| Placer River Main Cross-629 | Seward Highway | 1966 | 25 | Design |
| Holden Creek-1520 | Dalton Highway | 1982 | 26 | Construction |
| Snow River W. Channel-603 | Seward Highway | 1965 | 27 | Design |
| Snow River C. Channel-605 | Seward Highway | 1965 | 28 | Design |
| Chickaloon River-545 | Glenn Highway | 1956 | 29 | Planning |
| Gulkana River-574 | Richardson Highway | 1974 | 30 | Planning |
| O'Connor Creek-303 | Goldstream Road | 1967 | 31 | Design |
| Moose Creek-401 | Petersville Road | 1974 | 32 | Planning |
| Copper Delta-339 | Copper River Highway | 1977 | 33 | Design |
| Bear Creek-593 | Richardson Highway | 1952 | 34 | Planning |
| Ruby Creek-594 | Richardson Highway | 1952 | 35 | Design |
| Upper Miller Creek-581 | Richardson Highway | 1958 | 36 | Planning |
| Castner Creek-583 | Richardson Highway | 1958 | 37 | Planning |
| Eklutna Overcrossing-1374 | Eklutna Village Road | 1978 | 38 | Construction |
| Crooked Creek-431 | Steese Highway | 1957 | 39 | Design |
| Blowback Creek-1541 | Tofty Road | 1981 | 40 | Design |
| Anchor River-910 | Old Sterling Highway | 1949 | 41 | Planning |
| Trail Creek-660 | Old Tok Highway | 1951 | 42 | Planning |
| Ninilchik River-427 | Ninilchik Road | 1972 | 43 | Design |
| Fish Creek-1217 | Salmon River Road | 1965 | 44 | Construction |
| Jack Creek-861 | Nabesna Road | 1969 | 45 | No Project |

| | | | | |
|----------------------------|-----------------------|------|----|----------------|
| Trollers Creek-864 | Knudson Cove Road | 1938 | 46 | Design |
| Otter Creek-461 | Happy Creek Road | 1947 | 47 | Planning |
| Archangel Creek-915 | Fern Mine Road | 1995 | 48 | Planning |
| Salmon River-444 | Gustavus Airport Road | 1976 | 49 | Planning |
| Gold Creek-473 | Airfield Road | 1972 | 50 | Design |
| Fish Camp Creek-940 | Northway Road | 1987 | 51 | Planning |
| S. Fork 40 Mile River-839 | Taylor Highway | 1977 | 52 | Design |
| Barbara Creek-433 | Jakolof Bay Road | 1968 | 53 | Planning |
| Chokosna River-1193 | Edgerton Highway | 1973 | 54 | Planning |
| Kodiak Harbor Channel-1189 | Near Island Road | 1986 | 55 | Planning |
| Mineral Creek-944 | Mineral Creek Road | 1970 | 56 | No Project |
| Seattle Creek-690 | Denali Highway | 1954 | 57 | Design |
| Rock Creek-684 | Denali Highway | 1955 | 58 | No Project |
| Tatalina River-462 | Sterling Landing | 1947 | 59 | Design |
| Takotna River-463 | Sterling Landing | 1941 | 60 | Planning |
| Tenakee City Dock-1451 | Marine Highway Route | 1977 | 61 | Completed |
| American Creek #1-841 | Taylor Highway | 1988 | 62 | Planning |
| Chena River-532 | Wendell Street | 1953 | 63 | Planning |
| Buskin River #7-988 | Anton Larson Bay | 1960 | 64 | Planning |
| Iliamna River-2137 | W'port-Pile Bay Road | 2003 | 65 | Completed |
| Little Tok Overflow-659 | Old Tok Highway | 1954 | 66 | Planning |
| Noyes Slough-209 | Aurora Drive | 1960 | 67 | Planning |
| S. Fork Anchor River-1199 | North Fork Road | 1968 | 68 | Planning |
| Perryville Creek-1512 | Airport Road | 1981 | 69 | Reconnaissance |

| | | | | |
|---------------------------|----------------|------|----|-----------|
| Nenana River at Rex-216 | Parks Highway | 1963 | 70 | Completed |
| N. Fork 12-Mile Creek-275 | Steese Highway | 1961 | 71 | Completed |
| Taiya River-309 | Dyea Road | 1948 | 72 | Completed |

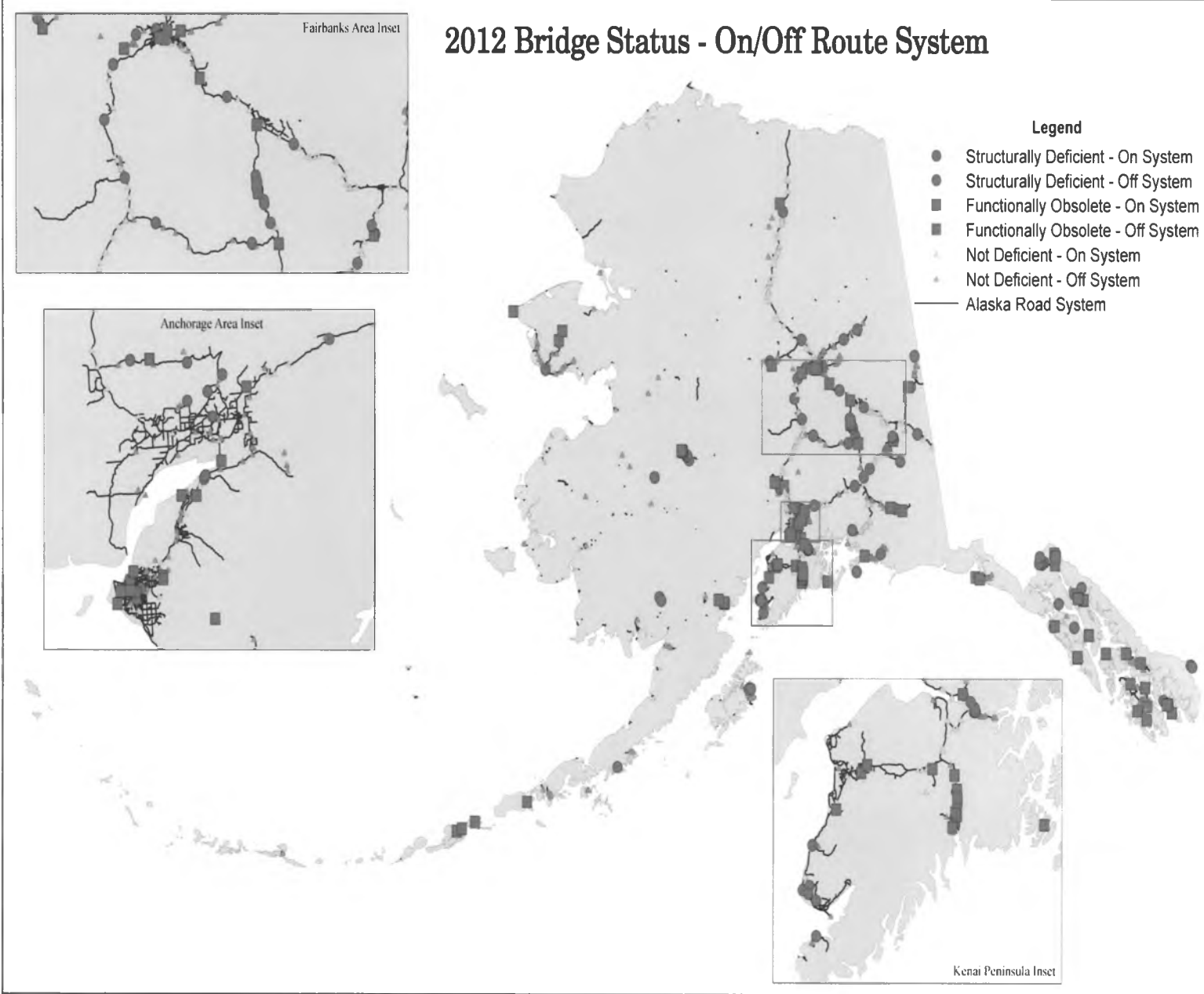
Appendix C

Structurally Deficient Bridges Identified for Replacement or Rehabilitation in the 2012-2015 Statewide Transportation Improvement Plan, the STIP

| <u>Bridge/Number</u> | <u>Route</u> | <u>Year Built</u> | <u>Fiscal Year</u> | <u>Action</u> |
|-----------------------------|---------------------|-------------------|--------------------|---------------|
| Snow River-603 | Seward Highway | 1965 | 2012 | Rehabilitate |
| Snow River-605 | Seward Highway | 1965 | 2012 | Rehabilitate |
| Peterson Creek-383 | Dotson Landing Road | 1940 | 2013 | Replace |
| Livengood Creek-229 | Old Elliott Highway | 1959 | 2012 | Replace |
| No Name Creek-327 | Halibut Point Road | 1959 | 2012 | Replace |
| Slana River-654 | Tok Cutoff Highway | 1951 | 2014 | Replace |
| Tulsona Creek-1250 | Tok Cutoff Highway | 1975 | 2012/15 | Design |
| Tok River-663 | Tok Cutoff Highway | 1963 | 2014 | Replace |
| Twenty Mile River-634 | Seward Highway | 1967 | 2013/14 | Replace |
| Riley Creek-695 | Parks Highway | 1969 | 2013 | Replace |
| Klehini River-1216 | Porcupine Crossing | 1969 | 2013 | Replace |
| Nenana River-Rex-216 | Parks Highway | 1963 | 2011 | Rehabilitated |
| Mendenhall River-737 | Glacier Highway | 1965 | 2014 | Replace |
| Phelan Creek-579 | Richardson Highway | 1958 | 2012 | Replace |
| Portage Creek #1-630 | Seward Highway | 1966 | 2013/14 | Replace |
| Portage Creek #2-631 | Seward Highway | 1967 | 2013/14 | Replace |
| Peterson Creek-636 | Seward Highway | 1966 | 2013/14 | Replace |
| Virgin Creek-638 | Seward Highway | 1966 | 2013/14 | Replace |
| Placer River Overflow-627 | Seward Highway | 1967 | 2013/14 | Replace |
| Placer River Main Cross-629 | Seward Highway | 1966 | 2013/14 | Replace |
| Tolsona Creek-552 | Glenn Highway | 1950 | 2012 | Replace |

| | | | | |
|--------------------------|--------------------|------|-------|--------------|
| Holden Creek-1520 | Dalton Highway | 1982 | 2012 | Replace |
| Ruby Creek-594 | Richardson Highway | 1952 | 2015 | Replace |
| Ninilchik River-427 | Ninilchik Road | 1972 | 2015 | Replace |
| Gold Creek-473 | Airfield Road | 1972 | >2015 | Replace |
| S Fork 40 Mile River-839 | Taylor Highway | 1977 | 2014 | Replace |
| N Fork 12 Mile Creek-275 | Steese Highway | 1961 | >2015 | Rehabilitate |
| Tatalina River-462 | Sterling Loop | 1947 | >2015 | Replace |
| Hyder Dock Trestle-1238 | Hyder | 1923 | 2012 | Replace |
| Snake River-881 | Nome | 1979 | 2012 | Replace |
| Banner Creek-526 | Richardson Highway | 1975 | 2015 | Replace |

2012 Bridge Status - On/Off Route System



**Appendix D: Structurally Deficient and Functionally Obsolete Bridges,
 On-System and Off-System 2011**

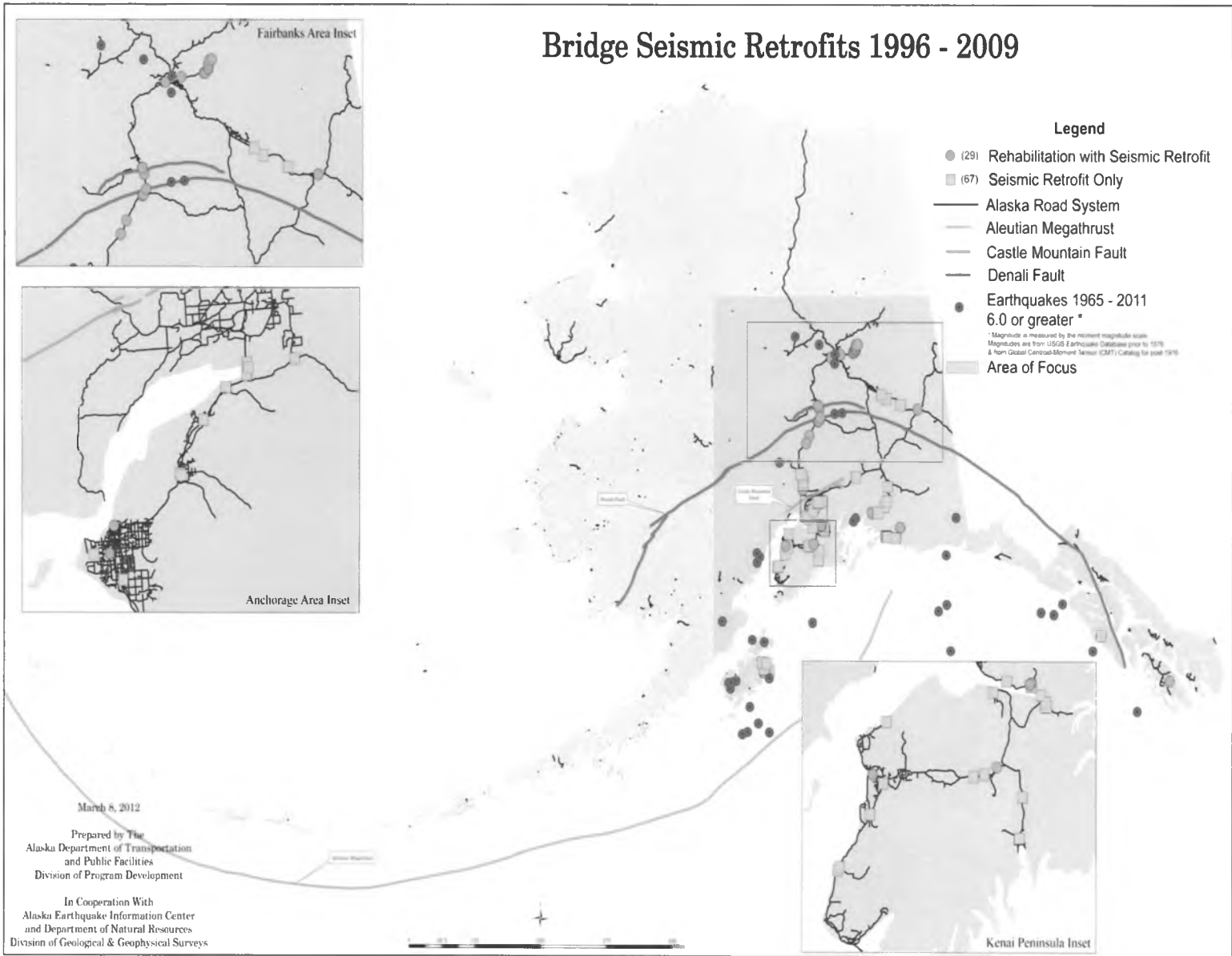
Appendix E

Status of Bridge Work on Selected National Highway System Routes, December 31, 2011²

| <u>Water Body</u> | <u>Bridge #</u> | <u>Route and Milepost</u> | <u>Status</u> |
|------------------------|-----------------|-----------------------------|---------------|
| Moose Creek | #541 | Glenn Highway MP 54.7 | Design |
| Riley Creek | #695 | Parks Highway MP 236.6 | Design |
| Tok River | #663 | Tok Cutoff Highway MP 98.2 | Design |
| Slana River | #654 | Tok Cutoff Highway MP 75.6 | Design |
| Chilkat River | #742 | Haines Highway MP 23.3 | Design |
| Jarvis Creek | #595 | Richardson Highway MP 264.8 | Design |
| Ruby Creek | #594 | Richardson Highway MP 234.7 | Design |
| Tok River | #506 | Alaska Highway MP 1309.4 | Design |
| Banner Creek | #526 | Richardson Highway MP 295.3 | Design |
| Tanana River | #524 | Richardson Highway MP 275.4 | Pre-Design |
| Tulsona Creek | #1250 | Tok Cutoff Highway MP 17.6 | Construction |
| Douglas Creek | #1560 | Dalton Highway MP 141.3 | Construction |
| Tolsona Creek | #552 | Glenn Highway MP 172.9 | Construction |
| Phelan Creek | #595 | Richardson Highway MP 201.5 | Design |
| Holden Creek | #1520 | Dalton Highway MP 267.4 | Design |
| Julius Creek | #317 | Parks Highway MP 250.1 | Completed |
| Shaw Creek | #525 | Richardson Highway MP 286.7 | Completed |
| One Mile Creek | #591 | Richardson Highway MP 184.7 | Construction |
| Capt. Wm. Moore Bridge | #1304 | Klondike Highway MP 11.2 | Design |

² Includes the Parks, Glenn, Richardson, Alaska, Haines, Dalton, Klondike and Tok Cutoff Highways


Bridge Seismic Retrofits 1996 - 2009



March 8, 2012

Prepared by The
Alaska Department of Transportation
and Public Facilities
Division of Program Development

In Cooperation With
Alaska Earthquake Information Center
and Department of Natural Resources
Division of Geological & Geophysical Surveys



The annual Bridge Report was produced by the Alaska Department of Transportation and Public Facilities in Juneau, Alaska at a cost of \$13.33 per copy.

MEMO

To: Legislative Contact File

From: Mary A Miller, PE

RE: Discussion with Sen. Egan and Rep. Munoz

Date: Jan 9, 2013, 3 pm

I have requested a meeting with the Transportation Committee Chair Egan and finance Committee Cathy Munoz to update them on Statewide and local tribal transportation issues.

TOPICS

A. Update on Central Council's Transportation Program; Projects and Plans

Program

- Tribal Transportation, active since 2005
- Completed Long Range Juneau Plan and are implementing
- Engaged on National level re: policy and legislation impacting the State
- Focus on Juneau tribal needs first then expanding as warranted

Projects

- **Hope Parking lot: Current in design, need construction funds - \$500K**
 - See Village Property Layout Sheet
 - See Concept Design for Hope Lot
 - See Estimate Sheet for Hope Parking Lot Construction
- Gajaa Hit parking expansion/demo area: In planning
- Expanded enhanced thoroughfare in Indian Village: In planning
- Coogan subdivisions housing: In design

Near term Plans

- Complete draft Transit and Marine Ferry Study Currently with McDowell and PN&D (Note Resolution/Correspondence to AMHS on Schedules)
- Expand Partnerships with Native and Non-Native Entities
- Continue to build capacity and secure resources for local and Statewide Tribes/villages
- Update tribal transportation plan to include newly identified needs
- Paratransit pending study outcome

B. Federal legislation changes (MAP 21 replaced SAFETEA-LU in June/July 2012) will adversely impact the recent progress in addressing transportation needs in small Alaska communities.

Funding impacts, and need for replacement funding:

- National Tribal Shares are funded at \$450 Million annual since 2009: MAP 21 funding held at \$450M,
- Review Alaska Tribal Transportation Distributions (See Summary FY06-FY 12 sum \$299 Million) -
- Review SE Alaska Tribal Shares for same period (See Summary: FY06-FY12 sum \$52 Million)

Denali Commission Local Surface Transportation NOT funded:

- Denali Commission website copy for Denali Transportation Program
- FHWA Fact sheet on Denali Surface Transportation, authority and funding
- Printout of Statewide Transportation projects funded by program-

BIA High Priority Project (HPP) not funded in MAP 21
(Impact will be to us in Construction Phase of Hope Parking)




- Refer to FHWA program Fact sheet for HPP program description
- See Summary of AK funded projects 2005-2012 to sum \$95 M.(See Attached)

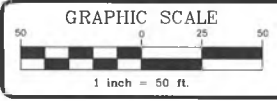
Note: The Denali Commission Transportation and BIA HPP both addressed significant unmet local transportation needs in the State, not addressed by DOT or others. Replacement funding consideration is warranted for rural surface transportation projects.

Summary Message:

- **Central Council is implementing an active transportation program bringing value to the Community and State.**
- **Federal tribal transportation funding changes in MAP 21 will adversely impact small AK communities.**
- **Absent the Federal HPP or Denali Commission funding, Central Council is seeking construction funding assistance for its Hope Parking Lot project.**

JUNEAU INDIAN VILLAGE LOT STATUS

-  RESTRICTED LOT
-  TLINGIT AND HAIDA OWNED
-  UNRESTRICTED LOT



Rodney P. Kinney Associates, inc.
 CONSULTING ENGINEERS & SURVEYORS
 19515 Centerfield Dr., Suite 101
 Eagle River, Alaska 99577
 (907) 884-2322 • www.rpha.net



CENTRAL COUNCIL TLINGIT & HAIDA INDIAN TRIBES OF ALASKA
 300 W. WILLOUGHBY AVE., SUITE 300
 JUNEAU, ALASKA 99801
 PHONE (907)586-1432 FAX (907)586-8970

U.S.S SURVEY 4694
 JUNEAU INDIAN VILLAGE LOT STATUS

| | |
|--------------|---------------|
| DESIGNED BY: | BLP |
| DRAWN BY: | SPK |
| APPROVED BY: | BLP |
| DATE: | JULY 23, 2012 |
| SCALE: | 1"=50' |

FIGURE 1

Most spaces 9' x 20'
Front row (nearest Hoop Bldg) 8.5' x 20'
Junon Lot req. 8.5' x 17'

Demo or Relocate
Yellow House

Relocate Courthouse

Demo Shed

Landscape
Areas

Local Crosswalk
& Accessible Parking
Nearest to
Entrance

↑
N
1" = 50'

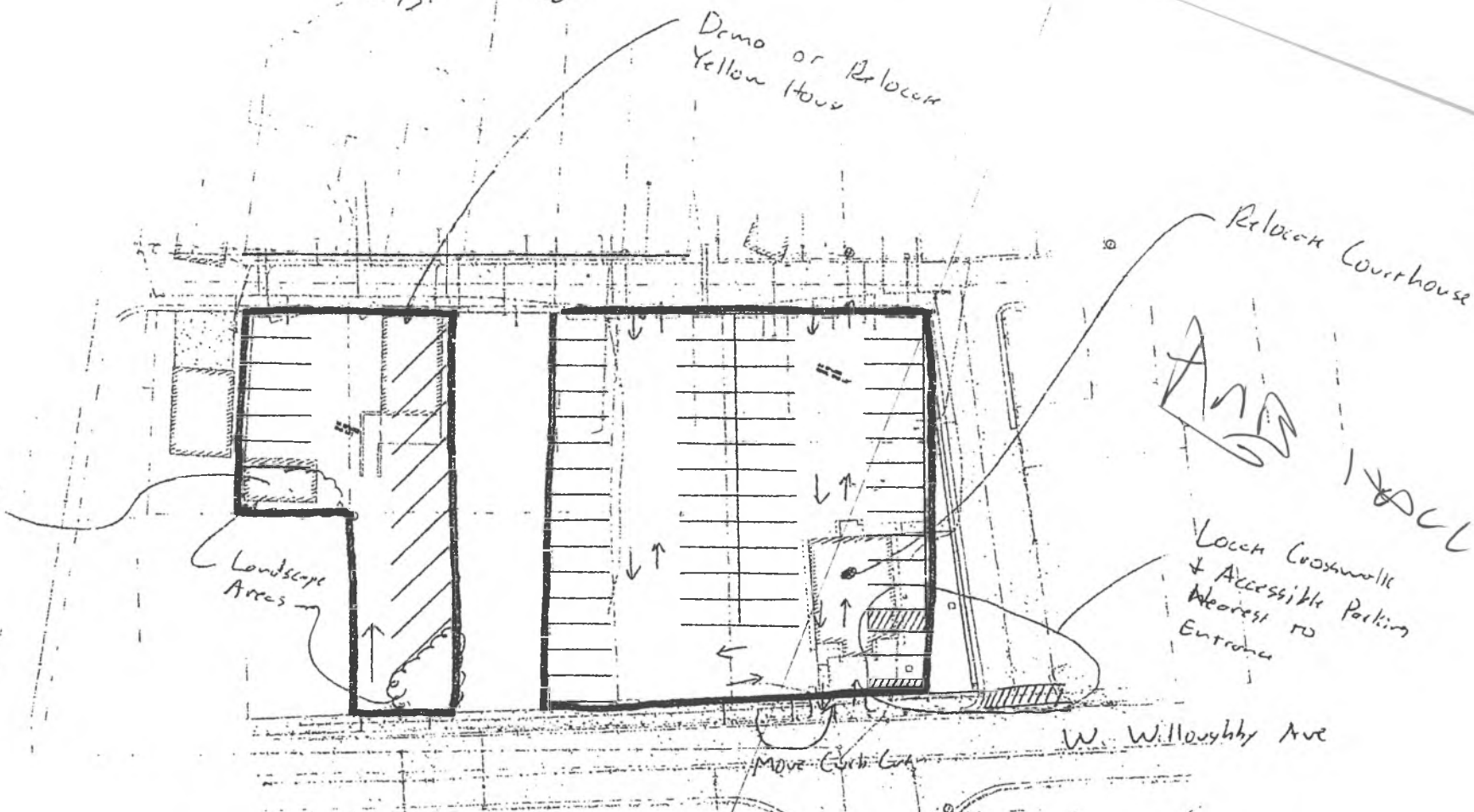
W. Willoughby Ave

15 Spaces West (Overflow) Lot

53 Spaces East Lot
(3 Accessible)
(could make 4 Accessible)

Move Courthouse

(B)



Hope Building Parking Lot
Preliminary Cost Estimate - Schematic Design Level
 January 9, 2013

Phase I

| Item No. | Description | Quantity | Pay Unit | Unit Price | Total |
|----------|--|--------------|-------------|------------|----------|
| 202(1) | Removal of Structures and Obstructions | All Required | Lump Sum | \$30,000 | \$30,000 |
| 202(3) | Removal of Sidewalk | 45 | Square Yard | \$20 | \$900 |
| 202(9) | Removal of Curb and Gutter | 72 | Linear Foot | \$15 | \$1,080 |
| 203(3) | Unclassified Excavation | 400 | Cubic Yard | \$25 | \$10,000 |
| 304(2) | Subbase, Grading B | 400 | Cubic Yard | \$45 | \$18,000 |
| 301(2) | Aggregate Base Course, Grading D-1 | 272 | Cubic Yard | \$50 | \$13,600 |
| 401(1) | Asphalt Concrete | 260 | Ton | \$140 | \$36,400 |
| 401(2) | Asphalt Cement, Grade 64-28 | 16 | Ton | \$1,200 | \$18,720 |
| 608(1a) | Concrete Sidewalk, 4 inches thick | 83 | Square Yard | \$85 | \$7,055 |
| 609(2) | Curb & Gutter | 142 | Linear Foot | \$45 | \$6,390 |
| 640(1) | Mobilization and Demobilization | All Required | Lump Sum | \$50,000 | \$50,000 |
| 642(1) | Construction Surveying | All Required | Lump Sum | \$15,000 | \$15,000 |

Subtotal: \$207,145
 Contingency (35%): \$72,501
 Construction Administration (15%): \$31,072
Total Phase I: \$310,718

Phase II

| Item No. | Description | Quantity | Pay Unit | Unit Price | Total |
|----------|--|--------------|-------------|------------|----------|
| 202(1) | Removal of Structures and Obstructions | All Required | Lump Sum | \$30,000 | \$30,000 |
| 202(3) | Removal of Sidewalk | 48 | Square Yard | \$20 | \$960 |
| 202(9) | Removal of Curb and Gutter | 36 | Linear Foot | \$15 | \$540 |
| 203(3) | Unclassified Excavation | 220 | Cubic Yard | \$25 | \$5,500 |
| 304(2) | Subbase, Grading B | 220 | Cubic Yard | \$45 | \$9,900 |
| 301(2) | Aggregate Base Course, Grading D-1 | 62 | Cubic Yard | \$50 | \$3,100 |
| 401(1) | Asphalt Concrete | 58 | Ton | \$140 | \$8,120 |
| 401(2) | Asphalt Cement, Grade 64-28 | 4 | Ton | \$1,200 | \$4,800 |
| 608(1a) | Concrete Sidewalk, 4 inches thick | 22 | Square Yard | \$85 | \$1,870 |
| 609(2) | Curb & Gutter | 36 | Linear Foot | \$45 | \$1,620 |
| 640(1) | Mobilization and Demobilization | All Required | Lump Sum | \$50,000 | \$50,000 |
| 642(1) | Construction Surveying | All Required | Lump Sum | \$10,000 | \$10,000 |

Subtotal: \$126,410
 Contingency (35%): \$44,244
 Construction Administration (15%): \$18,962
Total Phase II: \$189,615

Total Project Cost: \$500,333

Background

MAP-21 - Moving Ahead for Progress in the 21st Century

Tribal Transportation Program Questions & Answers

Posted 9/25/2012

GENERAL

Question 1: Where can I get access to the final legislation?

Answer 1: The legislation is known as the Moving Ahead for Progress in the 21st Century Act, Public Law 112-141. The acronym is MAP-21. The legislation, along with additional information, can be found on FHWA's website at: <http://www.fhwa.dot.gov/map21/>.

Question 2: Is the Indian Reservation Roads (IRR) Program included in MAP-21?

Answer 2: The IRR program is not provided new funding under MAP-21. However, the newly established Tribal Transportation Program (TTP) contains similar provisions and eligibility requirements as the IRR program.

∴ NEW PROGRAM = TRIBAL TRANSPORTATION PROGRAM (TTP)

FUNDING

Question 3: What is the authorized funding level for the Tribal Transportation Program?

Answer 3: The TTP is authorized at \$450 million/year, which is subject to the overall Federal-aid obligation limitation.

Question 4: How will the TTP funds be administered and distributed?

Answer 4: The TTP funds made available will be administered in accordance with Chapter 2 of Title 23. MAP-21 contains a statutory formula which replaces the Relative Need Distribution Factor (RNDF) formula included in Subpart C of 25 CFR Part 170. The new formula will calculate tribal shares using three different factors (as percentages of national or regional totals):

- 27% of funding based on the Tribe's approved road mileage (national percentage)
- 39% of funding based on the Tribe's most recent Native American Housing Assistance and Self Determination Act (NAHASDA) population (national percentage)
- 34% of funding based on the Tribe's RNDF and Population Adjustment Factor (PAF) amounts from FY05 to FY12 (regional percentage)

MAP-21 contains a set-aside for another factor which will be included in a Tribe's share of TTP

funds. The Tribal Supplemental Allocation, which totals approximately \$104 million, will be utilized initially within each Bureau of Indian Affairs (BIA) Region to supplement those Tribes that experience a negative impact from the new statutory formula in any year when compared to the funding a Tribe received under the old formula in Fiscal Year 2011.

TTP funds are 100% Federal share, and like IRR funds, can be used to satisfy local match requirements on construction projects.

Question 5: What inventory is used for determining a Tribe's road mileage?

Answer 5: The mileage used to generate a Tribe's share of funding includes the following:

- Any BIA owned road included in the FY12 Inventory;
- Any tribal owned road included in the FY12 Inventory; and
- Any road owned by another entity that was included in the BIA System of Roads utilized to generate funding prior to October 1, 2004. These are generally known as "grandfathered roads."

Question 6: Are there any other funding set-asides?

Answer 6: Yes. MAP-21 includes set-asides for the following programs:

- 2% for Transportation Planning
- 2% for Tribal Bridge Program
- 2% for a Tribal Safety Program
- 6% for Program Management and Oversight (PM&O)
The PM&SO is used by BIA and FHWA to carry out stewardship and oversight of the program, including those functions that are deemed as being "not otherwise contractible" per Appendix A to Subpart E of 25 CFR Part 170.

Question 7: Is there a transition in implementing the new formula?

Answer 7: Yes. In FY13, 80% of the net funding will be incorporated into tribal shares utilizing the RNDF formula of 25 CFR Part 170, while the remaining 20% will utilize the new statutory formula of MAP-21. In FY14, the percentages change to 60% (RNDF) and 40% (MAP-21). In FY15, they change again to 40% (RNDF) and 60% (MAP-21). In FY16 and thereafter, the tribal shares will be calculated utilizing 20% (RNDF) and 80% (MAP-21).

Question 8: How will funding get to the Tribes?

Answer 8: The funds will be apportioned to FHWA. FHWA will withhold the funding amount associated with the Tribes operating under FHWA/Tribal TTP Program Agreements and will then re-allocate the remainder to the BIA.

OTHER PROGRAMS AND ACTIVITIES

Question 9: Does the IRR High Priority Program (IRRHPP) continue?

Answer 9: MAP-21 includes a new Tribal High Priority Program (THPP). The IRRHPP under 25

CFR Part 170 will need to be revised to reflect the new MAP-21 program. However, the THPP and the IRRHPP are very similar. The THPP is not contract authority. An appropriation is needed from the general fund before funding can be made available for the THPP.

Question 10: Are there additional transportation planning requirements in MAP-21?

Answer 10: The transportation planning requirements of the IRR Program continue in the TTP. Funds must only be expended on projects and activities identified on an FHWA approved Transportation Improvement Program (TIP). Road Inventory data will continue to be a major part of transportation planning. However, MAP-21 will now require the inspection of all public bridges, including tribal bridges, and the reporting of data into FHWA's National Bridge Inspection System (NBIS), which will be another planning effort.

Question 11: How will the new Tribal Safety Program function?

Answer 11: Two percent of the authorized funding will be made available to tribes to carry out safety specific activities and projects. The exact procedures and guidance will be developed in coordination with the TTP Coordinating Committee.

Question 12: Will the IRR Bridge Program Continue to operate under its current regulations?

Answer 12: Yes, the TTP Bridge Program will function in the same manner as the IRR Bridge Program established in SAFETEA-LU.

Question 13: Can Tribes use TTP funds for maintenance?

Answer 13: Yes, in accordance with 25 CFR Part 170 and MAP-21, Tribes may use up to 25% of their TTP funds or \$500,000, whichever is greater, for eligible and approved maintenance activities. These activities must be included on the Tribe's TIP approved by FHWA.

Question 14: Is Question 10 (25 C.F.R. 170, Appendix C to Subpart C) still a factor in the TTP funding formula?

Answer 14: No. The tribal shares generated by the TTP funding formula are not influenced by Question 10, which is obviated by MAP-21.

Question 15: Does FTA provide public transit assistance to Indian tribes?

Answer 15: Yes. MAP-21 continues the Public Transportation on Indian Reservations Program, also referred to as "TTP," under Chapter 53 of title 49, but now includes funds to be allocated by formula. MAP-21 authorizes \$25 million in formula funds and \$5 million for discretionary awards.

FTA will issue interim guidance and publish its plans for consultation with Indian tribes and stakeholders in October.

See: http://fta.dot.gov/documents/MAP-21_Fact_Sheet_-_Formula_Grants_for_Rural_Areas.pdf

FTA's Tribal website: http://fta.dot.gov/grants/13094_3553.html

To view PDF files, you can use the [Acrobat® Reader®](#).

Office of Federal Lands Highway

Location: [Home](#) > [Programs](#) > [Transportation](#) > [IRR](#)

Indian Reservation Roads / Bridges (IRR)

SUPERCEDED
By MAP 21 TIP

The Indian Reservation Roads Program addresses transportation needs of tribes by providing funds for planning, designing, construction, and maintenance activities. The program is jointly administered by the Federal Highway Administration's Federal Lands Highway Office and the Bureau of Indian Affairs (BIA) in accordance with an interagency agreement.

The Indian Reservation Roads are public roads which provide access to and within Indian reservations, Indian trust land, restricted Indian land, and Alaska native villages. Approximately 41,000 miles are under the jurisdiction of BIA and tribes and another 81,000 are under State and local ownership. IRR funds can be used for any type Title 23 transportation project providing access to or within Federal or Indian lands and may be used for the State/local matching share for apportioned Federal-aid Highway Funds.

The BIA and Tribal governments undertake most of the design and construction of IRR projects. Under Public Law 93-638 contracts, Tribal governments can develop and operate portions of the IRR Program within its boundary.

Publications / Documents

- **FY12 Tribal Consultation Files**
 - [IRR Program Update](#) (Acrobat, 879 KB)
 - [BIA IRR Program Class 1-7 Definitions](#) (Acrobat, 65 KB)
 - [Proposed and Access Roads – June Consultation Meetings](#) (Acrobat, 785 KB)
 - [Tribal Consultation and Informational Meetings – Question 10 Update](#) (Acrobat, 766 KB)
- [FY 2012 Tribal Transportation Allocation](#) (Excel/xlsx, 122 KB)
- [Tribal Transportation Program Delivery Guide - 2011](#)
- [Comprehensive Tribal Inventory Assessment](#)
- [ERFO Manual \(Updated April 2011\)](#)

IRR Sections

- [IRR Bridge Program](#)
- [High Priority Projects](#)
- [Tribal Safety](#)
- [Contracting Procedures](#)
- [Tribal Technical Assistance Program](#)
- [IRR SAFETEA-LU Changes](#)
- [Contact Information](#)

Alaska Tribal Transportation Funding

Alaska Tribal Shares of National Tribal Transportation \$

| FFS ID | Short Name | 06SumF | 07SumF | 08SumF | 09SumF | 10Sum | 11Sum | 12Sum | Total FY06-FY12 Sum |
|--------|---------------------|------------|------------|------------|------------|------------|------------|---------|---------------------|
| E01002 | Afognak | 13,200.88 | 82,027.36 | 96,610.36 | 99,471.00 | 93,686.00 | 66,419.00 | 59,856 | \$ 511,270.60 |
| E01252 | Agdaagux | 90,515.66 | 81,365.16 | 139,840.92 | 132,239.00 | 120,817.00 | 75,865.00 | 68,884 | \$ 709,526.74 |
| E01004 | Akhiok | 132,988.22 | 100,744.77 | 78,652.93 | 86,387.00 | 78,386.00 | 74,366.00 | 70,496 | \$ 622,020.92 |
| E02005 | Akiachak | 147,391.59 | 135,975.06 | 169,122.83 | 211,702.00 | 191,648.00 | 182,148.00 | 162,334 | \$ 1,200,321.48 |
| E02006 | Akiak | 49,147.55 | 51,094.23 | 53,601.13 | 82,094.00 | 77,124.00 | 74,395.00 | 67,861 | \$ 455,316.91 |
| E01007 | Akutan | 145,203.41 | 113,576.90 | 91,972.15 | 113,382.00 | 103,223.00 | 98,145.00 | 85,857 | \$ 751,359.46 |
| E02008 | Alakanuk | 163,037.41 | 136,692.60 | 133,878.12 | 131,308.00 | 558,780.00 | 700,398.00 | 567,427 | \$ 2,391,521.13 |
| E03010 | Alatna | 7,702.94 | 13,709.32 | 18,836.46 | 24,451.00 | 24,136.00 | 29,991.00 | 28,691 | \$ 147,517.72 |
| E01011 | Aleknagik | 110,778.81 | 91,654.74 | 78,855.52 | 79,594.00 | 74,089.00 | 71,411.00 | 63,689 | \$ 570,072.07 |
| E02456 | Algaaciq | 22,500.29 | 33,437.06 | 42,284.88 | 51,530.00 | 58,134.00 | 56,738.00 | 51,278 | \$ 315,902.23 |
| E03013 | Allakaket | 67,231.47 | 86,555.51 | 73,734.00 | 74,710.00 | 69,522.00 | 66,658.00 | 60,089 | \$ 498,499.98 |
| E04014 | Ambler | 43,519.29 | 91,038.12 | 79,886.32 | 81,701.00 | 76,584.00 | 113,174.00 | 100,484 | \$ 586,386.73 |
| E03016 | Anaktuvuk Pass | 48,795.88 | 76,449.04 | 69,544.53 | 73,072.00 | 68,873.00 | 78,257.00 | 74,088 | \$ 489,079.45 |
| E02019 | Andreafski | 43,823.38 | 116,522.01 | 96,100.01 | 93,922.00 | 86,089.00 | 84,116.00 | 74,433 | \$ 595,005.40 |
| E09020 | Angoon | 160,796.95 | 344,307.86 | 255,887.41 | 239,425.00 | 221,233.00 | 208,896.00 | 180,133 | \$ 1,610,679.22 |
| E02021 | Aniak | 68,068.58 | 54,946.21 | 59,001.92 | 65,615.00 | 63,818.00 | 62,460.00 | 89,032 | \$ 462,941.71 |
| E03022 | Anvik | 19,713.84 | 26,614.38 | 32,688.71 | 40,440.00 | 70,469.00 | 67,616.00 | 51,094 | \$ 308,635.93 |
| E03025 | Arctic Village | 87,300.76 | 162,662.86 | 190,830.84 | 171,044.00 | 153,539.00 | 234,244.00 | 362,013 | \$ 1,361,634.46 |
| E02323 | Asa'carsarmiut | 60,690.95 | 67,999.67 | 73,899.14 | 104,052.00 | 98,567.00 | 79,628.00 | 70,531 | \$ 555,367.76 |
| E01027 | Atka | 323,685.51 | 236,331.20 | 221,378.49 | 191,909.00 | 170,332.00 | 160,210.00 | 141,218 | \$ 1,445,064.20 |
| E02029 | Atmautluak | 31,251.82 | 49,176.56 | 81,313.87 | 180,535.00 | 162,967.00 | 154,446.00 | 135,997 | \$ 795,687.25 |
| E03028 | Atkasuk | 45,539.29 | 58,498.51 | 56,798.13 | 62,178.00 | 59,216.00 | 57,383.00 | 65,208 | \$ 404,820.93 |
| E03033 | Barrow | 602,865.80 | 563,923.82 | 464,768.67 | 457,500.00 | 417,138.00 | 473,045.00 | 427,560 | \$ 3,406,801.29 |
| E03038 | Beaver | 50,193.33 | 43,874.77 | 40,158.22 | 42,275.00 | 43,554.00 | 41,895.00 | 42,478 | \$ 304,428.32 |
| E01041 | Belkofski | 8,691.46 | 14,900.44 | 20,125.72 | 25,892.00 | 29,336.00 | 28,816.00 | 27,100 | \$ 154,861.62 |
| E02047 | Bill Moore's Slough | 27,060.37 | 31,616.46 | 216,730.11 | 222,496.00 | 198,197.00 | 187,057.00 | 159,211 | \$ 1,042,367.94 |
| E03049 | Birch Creek | 75,116.67 | 60,062.23 | 50,138.46 | 50,280.00 | 46,641.00 | 44,703.00 | 40,165 | \$ 367,106.36 |
| E04056 | Brevig Mission | 327,645.45 | 242,446.00 | 715,320.37 | 736,941.00 | 647,991.00 | 618,936.00 | 490,611 | \$ 3,779,890.82 |
| E04059 | Buckland | 61,691.77 | 61,811.43 | 85,339.41 | 90,199.00 | 85,608.00 | 83,225.00 | 79,004 | \$ 546,878.61 |
| E01065 | Cantwell | 8,691.46 | 14,900.44 | 20,125.72 | 25,892.00 | 25,505.00 | 25,088.00 | 23,822 | \$ 144,024.62 |
| E09801 | CCTHITA | 291,684.90 | 400,451.91 | 677,714.98 | 664,839.00 | 786,730.00 | 706,870.00 | 775,065 | \$ 4,303,355.79 |
| E03072 | Chalkyitsik | 117,713.13 | 90,589.34 | 71,925.16 | 78,084.00 | 185,871.00 | 175,217.00 | 147,858 | \$ 867,257.63 |
| E01087 | Cheesh-Na | 57,621.84 | 48,670.58 | 43,083.08 | 44,564.00 | 41,813.00 | 40,315.00 | 236,037 | \$ 512,104.50 |
| E02077 | Chefornak | 25,063.22 | 36,525.16 | 45,675.16 | 54,670.00 | 53,724.00 | 57,117.00 | 54,229 | \$ 327,003.54 |
| E01578 | Chenega | 134,744.18 | 102,077.16 | 270,753.38 | 233,582.00 | 206,646.00 | 168,834.00 | 136,710 | \$ 1,253,346.72 |
| E02080 | Chevak | 39,451.49 | 53,862.61 | 93,616.13 | 98,045.00 | 537,249.00 | 690,434.00 | 536,354 | \$ 2,049,012.23 |

Alaska Tribal Shares of National Tribal Transportation \$

| | | | | | | | | | | |
|--------|----------------|--------------|--------------|--------------|--------------|--------------|--------------|-----------|----|--------------|
| E01081 | Chickaloon | 1,108,718.74 | 785,426.83 | 664,589.04 | 672,098.00 | 920,843.00 | 879,417.00 | 779,934 | \$ | 5,811,026.61 |
| E01083 | Chignik Bay | 157,757.98 | 117,514.06 | 107,130.38 | 97,560.00 | 87,860.00 | 81,814.00 | 68,394 | \$ | 718,030.42 |
| E01084 | Chignik Lagoon | 169,961.61 | 126,653.39 | 96,297.75 | 88,925.00 | 80,476.00 | 76,349.00 | 67,572 | \$ | 706,234.75 |
| E01085 | Chignik Lake | 107,613.37 | 87,739.27 | 74,418.99 | 75,892.00 | 70,321.00 | 67,376.00 | 61,671 | \$ | 545,031.63 |
| E09259 | Chilkat | 81,195.06 | 69,322.03 | 355,352.04 | 321,215.00 | 284,494.00 | 278,538.00 | 242,907 | \$ | 1,633,023.13 |
| E09186 | Chilkoot | 404,494.33 | 420,927.06 | 242,505.05 | 1,501,319.00 | 1,169,215.00 | 1,120,201.00 | 1,005,405 | \$ | 5,864,066.44 |
| E04177 | Chinik | 331,284.80 | 242,430.16 | 790,307.54 | 667,153.00 | 586,150.00 | 571,322.00 | 452,083 | \$ | 3,640,730.50 |
| E01088 | Chitina | 17,094.60 | 20,745.94 | 24,179.70 | 29,331.00 | 28,568.00 | 30,618.00 | 28,147 | \$ | 178,684.24 |
| E02841 | Chuathbaluk | 163,083.11 | 88,110.57 | 89,536.40 | 87,800.00 | 80,970.00 | 61,748.00 | 56,211 | \$ | 627,459.08 |
| E02092 | Chuloonawick | 8,215.50 | 14,326.94 | 19,504.96 | 25,223.00 | 24,870.00 | 24,429.00 | 23,324 | \$ | 139,893.40 |
| E03093 | Circle | 36,278.73 | 34,447.62 | 33,491.77 | 37,252.00 | 35,513.00 | 35,353.00 | 32,537 | \$ | 244,873.12 |
| E01096 | Clarks Point | 30,016.40 | 29,954.69 | 30,666.92 | 34,782.00 | 33,346.00 | 32,447.00 | 30,260 | \$ | 221,473.01 |
| E04107 | Council | 13,017.87 | 22,011.11 | 224,204.85 | 198,684.00 | 177,473.00 | 186,753.00 | 156,536 | \$ | 978,679.83 |
| E09108 | Craig | 565,230.76 | 1,444,764.45 | 1,005,412.30 | 850,559.00 | 747,289.00 | 703,389.00 | 596,875 | \$ | 5,913,519.51 |
| E02109 | Crooked Creek | 166,557.58 | 128,756.76 | 569,775.87 | 484,700.00 | 427,129.00 | 402,002.00 | 319,613 | \$ | 2,498,534.21 |
| E01121 | Curyung | 215,797.50 | 272,737.15 | 224,740.74 | 221,896.00 | 204,551.00 | 267,462.00 | 261,275 | \$ | 1,668,459.39 |
| E04117 | Deering | 219,625.74 | 165,596.95 | 118,803.30 | 112,134.00 | 102,315.00 | 119,588.00 | 116,283 | \$ | 954,345.99 |
| E04219 | Diomedede | 14,743.21 | 35,882.13 | 294,026.91 | 257,009.00 | 228,578.00 | 246,457.00 | 195,442 | \$ | 1,272,138.25 |
| E03127 | Dot Lake | 58,792.48 | 48,721.11 | 42,453.59 | 27,360.00 | 40,833.00 | 39,219.00 | 35,581 | \$ | 292,960.18 |
| E09128 | Douglas | 50,842.03 | 51,699.63 | 158,166.45 | 151,823.00 | 166,400.00 | 145,419.00 | 125,288 | \$ | 849,638.11 |
| E03130 | Eagle | 36,276.14 | 33,290.66 | 32,064.75 | 35,391.00 | 33,727.00 | 32,635.00 | 29,875 | \$ | 233,259.55 |
| E02132 | Eek | 76,072.74 | 70,722.19 | 67,100.44 | 71,778.00 | 870,718.00 | 993,746.00 | 800,004 | \$ | 2,950,141.37 |
| E01133 | Egegik | 316,907.34 | 228,260.89 | 164,554.94 | 203,417.00 | 180,249.00 | 315,379.00 | 254,881 | \$ | 1,663,649.17 |
| E01135 | Eklutna | 95,834.17 | 79,954.80 | 64,081.17 | 65,648.00 | 60,233.00 | 57,382.00 | 92,984 | \$ | 516,117.14 |
| E01136 | Ekuk | 11,563.28 | 10,230.99 | 62,373.59 | 54,010.00 | 47,854.00 | 45,073.00 | 35,751 | \$ | 266,855.86 |
| E01137 | Ekwok | 29,096.62 | 33,611.85 | 37,972.59 | 45,042.00 | 63,162.00 | 60,856.00 | 54,821 | \$ | 324,562.06 |
| E04139 | Elim | 464,729.77 | 339,119.37 | 953,246.79 | 803,445.00 | 705,456.00 | 672,858.00 | 536,841 | \$ | 4,475,695.93 |
| E02141 | Emmonak | 126,488.00 | 113,461.97 | 104,915.58 | 115,584.00 | 108,720.00 | 99,715.00 | 87,364 | \$ | 756,248.55 |
| E03146 | Evansville | 120,532.09 | 91,425.10 | 71,406.97 | 51,463.00 | 45,726.00 | 43,075.00 | 36,274 | \$ | 459,902.16 |
| E01148 | Eyak | 661,715.78 | 491,670.44 | 386,946.11 | 354,592.00 | 315,098.00 | 302,751.00 | 309,632 | \$ | 2,822,405.33 |
| E01149 | False Pass | 17,588.06 | 20,681.61 | 23,714.92 | 28,534.00 | 27,779.00 | 27,127.00 | 25,507 | \$ | 170,931.59 |
| E03162 | Fort Yukon | 325,611.31 | 245,616.87 | 188,900.52 | 173,954.00 | 157,770.00 | 149,540.00 | 130,845 | \$ | 1,372,237.70 |
| E01165 | Gakona | 48,752.89 | 36,200.09 | 33,621.55 | 34,649.00 | 31,051.00 | 29,405.00 | 25,266 | \$ | 238,945.53 |
| E03166 | Galena | 46,140.39 | 51,794.80 | 56,361.82 | 63,026.00 | 112,186.00 | 91,167.00 | 81,826 | \$ | 502,502.01 |
| E04167 | Gambell | 484,712.16 | 258,055.90 | 272,579.11 | 357,783.00 | 318,860.00 | 448,915.00 | 363,856 | \$ | 2,504,761.17 |
| E02170 | Georgetown | 15,556.18 | 70,775.75 | 118,000.43 | 99,877.00 | 87,926.00 | 82,078.00 | 67,631 | \$ | 541,844.36 |
| E02325 | Goodnews Bay | 60,517.60 | 57,353.51 | 56,184.99 | 61,668.00 | 58,901.00 | 57,199.00 | 60,766 | \$ | 412,590.10 |

Alaska Tribal Shares of National Tribal Transportation \$

| | | | | | | | | | | |
|--------|-----------------|------------|--------------|--------------|--------------|--------------|--------------|-----------|----|---------------|
| E03672 | Grayling | 77,626.37 | 68,015.42 | 62,148.43 | 65,358.00 | 61,581.00 | 59,351.00 | 54,129 | \$ | 448,209.22 |
| E01185 | Gulkana | 41,528.72 | 37,959.19 | 36,231.99 | 39,314.00 | 37,331.00 | 36,219.00 | 75,077 | \$ | 303,660.90 |
| E02189 | Hamilton | 7,336.81 | 13,268.16 | 18,358.95 | 23,988.00 | 260,806.00 | 294,994.00 | 228,774 | \$ | 847,525.92 |
| E03634 | Healy Lake | 77,811.97 | 62,011.97 | 234,356.24 | 202,755.00 | 179,478.00 | 169,427.00 | 157,514 | \$ | 1,083,354.18 |
| E03195 | Holy Cross | 73,886.92 | 66,572.39 | 251,327.33 | 222,668.00 | 198,873.00 | 196,878.00 | 177,350 | \$ | 1,187,555.64 |
| E09198 | Hoonah | 62,613.21 | 363,501.69 | 296,494.86 | 501,428.00 | 443,152.00 | 413,658.00 | 354,504 | \$ | 2,435,351.76 |
| E02199 | Hooper Bay | 51,979.01 | 70,855.18 | 85,263.25 | 99,615.00 | 167,945.00 | 367,466.00 | 293,231 | \$ | 1,136,354.44 |
| E03205 | Hughes | 30,841.03 | 56,822.31 | 79,199.68 | 98,519.00 | 88,914.00 | 84,072.00 | 71,828 | \$ | 510,196.02 |
| E03210 | Huslia | 32,995.30 | 55,199.67 | 145,795.37 | 374,973.00 | 332,105.00 | 311,352.00 | 260,662 | \$ | 1,513,082.34 |
| E09211 | Hydaburg | 201,144.23 | 2,986,021.93 | 2,040,798.19 | 1,700,474.00 | 1,537,553.00 | 1,432,878.00 | 1,206,870 | \$ | 11,105,739.35 |
| E01214 | Igiugig | 298,304.06 | 214,654.19 | 154,502.17 | 136,417.00 | 121,690.00 | 117,037.00 | 100,847 | \$ | 1,143,451.42 |
| E01216 | Iliamna | 533,249.42 | 289,125.24 | 190,097.09 | 166,169.00 | 147,527.00 | 180,570.00 | 154,726 | \$ | 1,661,463.75 |
| E02418 | Iqurmiut | 278,455.23 | 213,853.32 | 163,722.55 | 151,651.00 | 137,659.00 | 130,990.00 | 119,155 | \$ | 1,195,486.10 |
| E01222 | Ivanoff Bay | 8,588.16 | 8,533.23 | 8,662.20 | 9,832.00 | 9,369.00 | 9,101.00 | 8,619 | \$ | 62,704.59 |
| E01586 | Kaguyak | 1,981.17 | 3,652.25 | 5,047.59 | 6,589.00 | 6,518.00 | 6,462.00 | 6,177 | \$ | 36,427.01 |
| E09229 | Kake | 373,685.06 | 309,132.74 | 845,183.10 | 720,159.00 | 634,554.00 | 598,655.00 | 505,291 | \$ | 3,986,659.90 |
| E03230 | Kaktovik | 186,262.49 | 146,378.43 | 118,054.59 | 112,116.00 | 102,656.00 | 91,498.00 | 81,279 | \$ | 838,244.51 |
| E02232 | Kalskag (Upper) | 16,422.85 | 26,113.86 | 35,934.07 | 44,321.00 | 205,763.00 | 433,828.00 | 341,356 | \$ | 1,103,738.78 |
| E03233 | Kaltag | 103,396.71 | 86,632.54 | 75,429.05 | 76,743.00 | 94,622.00 | 92,599.00 | 82,119 | \$ | 611,541.30 |
| E01623 | Kanatak | 8,508.39 | 14,679.86 | 19,886.97 | 25,635.00 | 25,261.00 | 24,806.00 | 23,686 | \$ | 142,463.22 |
| E01237 | Karluk | 296,844.41 | 212,536.77 | 153,282.30 | 239,585.00 | 229,227.00 | 475,163.00 | 381,644 | \$ | 1,988,282.48 |
| E09238 | Kasaan | 160,565.05 | 2,289,172.65 | 1,573,859.11 | 1,304,100.00 | 1,137,211.00 | 1,066,109.00 | 895,349 | \$ | 8,426,365.81 |
| E02241 | Kasigluk | 31,461.45 | 43,903.86 | 53,665.85 | 64,387.00 | 135,579.00 | 129,519.00 | 218,329 | \$ | 676,845.16 |
| E01246 | Kenaitze | 113,375.20 | 140,972.06 | 156,257.26 | 145,119.00 | 142,744.00 | 135,846.00 | 129,937 | \$ | 964,250.52 |
| E09248 | Ketchikan | 109,599.94 | 156,181.17 | 739,150.14 | 671,481.00 | 614,305.00 | 624,598.00 | 592,975 | \$ | 3,508,290.25 |
| E04249 | Kiana | 111,591.20 | 95,441.09 | 84,872.56 | 87,452.00 | 82,133.00 | 88,421.00 | 110,401 | \$ | 660,311.85 |
| E04253 | King Island | 845,025.08 | 603,599.42 | 630,286.19 | 538,890.00 | 475,419.00 | 494,474.00 | 410,938 | \$ | 3,998,631.69 |
| E01254 | King Salmon | 30,210.95 | 29,579.42 | 29,838.74 | 33,697.00 | 32,270.00 | 31,604.00 | 29,431 | \$ | 216,631.11 |
| E02255 | Kipnuk | 45,985.88 | 56,120.46 | 64,187.11 | 74,177.00 | 513,897.00 | 590,710.00 | 483,660 | \$ | 1,828,737.45 |
| E04256 | Kivalina | 99,918.00 | 233,895.53 | 178,472.23 | 164,800.00 | 149,658.00 | 165,374.00 | 166,647 | \$ | 1,158,764.76 |
| E09257 | Klawock | 132,649.29 | 1,952,686.67 | 1,349,967.31 | 1,133,101.00 | 1,005,042.00 | 962,381.00 | 813,808 | \$ | 7,349,635.27 |
| E01102 | Kluti-Kaah | 18,571.69 | 27,967.52 | 35,710.44 | 44,513.00 | 65,166.00 | 62,938.00 | 56,019 | \$ | 310,885.65 |
| E01260 | Knik | 71,057.14 | 85,302.69 | 119,660.52 | 172,112.00 | 164,689.00 | 443,485.00 | 481,807 | \$ | 1,538,113.35 |
| E04261 | Kobuk | 13,310.65 | 204,424.48 | 154,128.12 | 193,455.00 | 173,028.00 | 194,173.00 | 167,737 | \$ | 1,100,256.25 |
| E01264 | Kokhanok | 76,127.86 | 66,581.70 | 87,332.27 | 85,846.00 | 79,031.00 | 76,627.00 | 69,632 | \$ | 541,177.83 |
| E02267 | Kongiganak | 23,617.23 | 35,541.68 | 116,674.69 | 165,625.00 | 173,392.00 | 259,364.00 | 218,903 | \$ | 993,117.60 |
| E02268 | Kotlik | 66,000.77 | 68,557.02 | 71,252.39 | 102,444.00 | 96,425.00 | 82,907.00 | 73,644 | \$ | 561,230.18 |

Alaska Tribal Shares of National Tribal Transportation \$

| | | | | | | | | | | |
|--------|--------------------|------------|------------|--------------|--------------|------------|------------|---------|----|--------------|
| E04269 | Kotzebue | 177,227.85 | 529,132.16 | 426,743.38 | 393,745.00 | 360,344.00 | 288,650.00 | 265,998 | \$ | 2,441,840.39 |
| E04270 | Koyuk | 310,842.88 | 231,853.05 | 1,262,414.47 | 1,059,080.00 | 928,533.00 | 870,540.00 | 682,289 | \$ | 5,345,552.40 |
| E03271 | Koyukuk | 31,593.82 | 31,331.18 | 31,861.81 | 35,986.00 | 34,488.00 | 33,480.00 | 31,004 | \$ | 229,744.81 |
| E02275 | Kwethluk | 75,746.52 | 82,638.67 | 83,351.16 | 90,462.00 | 86,646.00 | 85,565.00 | 80,234 | \$ | 584,643.35 |
| E02276 | Kwigillingok | 22,793.30 | 33,789.99 | 42,666.88 | 52,353.00 | 51,474.00 | 56,003.00 | 53,038 | \$ | 312,118.17 |
| E02407 | Kwinhagak | 69,179.63 | 88,259.13 | 82,474.57 | 130,734.00 | 121,232.00 | 121,331.00 | 106,516 | \$ | 719,726.33 |
| E01279 | Larsen Bay | 11,676.44 | 17,661.09 | 30,111.86 | 38,315.00 | 63,129.00 | 51,781.00 | 38,229 | \$ | 250,903.39 |
| E01531 | Leisnoi/Tangirnaq | 8,728.06 | 14,944.55 | 19,886.97 | 24,708.00 | 24,381.00 | 7,451.00 | 7,036 | \$ | 107,135.58 |
| E01283 | Levelock | 79,216.04 | 67,900.97 | 341,226.94 | 295,022.00 | 261,346.00 | 236,362.00 | 197,736 | \$ | 1,478,809.95 |
| E02288 | Lime | 71,927.67 | 57,980.62 | 48,891.22 | 49,394.00 | 45,907.00 | 44,070.00 | 38,904 | \$ | 357,074.51 |
| E02291 | Lower Kalskag | 35,218.58 | 40,791.29 | 41,339.72 | 54,658.00 | 52,854.00 | 72,104.00 | 67,996 | \$ | 364,961.59 |
| E03294 | Manley Hot Springs | 115,785.59 | 82,189.00 | 63,850.16 | 88,129.00 | 77,679.00 | 74,311.00 | 91,285 | \$ | 593,228.75 |
| E01295 | Manokotak | 149,751.71 | 122,085.33 | 103,054.69 | 111,701.00 | 103,091.00 | 100,582.00 | 107,503 | \$ | 797,768.73 |
| E02297 | Marshall | 315,780.99 | 236,244.22 | 180,283.66 | 165,907.00 | 150,454.00 | 145,312.00 | 125,533 | \$ | 1,319,514.87 |
| E04299 | Mary's Igloo | 13,054.38 | 22,055.22 | 905,119.63 | 761,467.00 | 668,054.00 | 626,304.00 | 484,822 | \$ | 3,480,876.23 |
| E03303 | McGrath | 44,152.42 | 266,397.46 | 197,615.12 | 178,204.00 | 160,148.00 | 151,602.00 | 127,403 | \$ | 1,125,522.00 |
| E02307 | Mekoryuk | 78,852.13 | 69,808.16 | 76,926.08 | 111,316.00 | 102,013.00 | 128,302.00 | 108,689 | \$ | 675,906.37 |
| E01309 | Mentasta | 39,970.63 | 40,569.90 | 32,951.32 | 30,406.00 | 29,637.00 | 60,238.00 | 214,711 | \$ | 448,483.85 |
| E03314 | Minto | 48,192.67 | 48,952.04 | 50,370.71 | 56,751.00 | 54,347.00 | 52,735.00 | 48,801 | \$ | 360,149.42 |
| E01330 | Naknek | 655,682.39 | 575,562.43 | 598,341.70 | 510,332.00 | 449,913.00 | 413,922.00 | 342,688 | \$ | 3,546,441.52 |
| E01142 | Nanwalek | 241,704.43 | 261,810.86 | 286,566.13 | 292,088.00 | 259,330.00 | 244,000.00 | 201,429 | \$ | 1,786,928.42 |
| E02332 | Napaimute | 9,167.46 | 15,473.95 | 20,746.48 | 30,981.00 | 52,127.00 | 50,134.00 | 44,111 | \$ | 222,740.89 |
| E02333 | Napakiak | 91,669.81 | 81,857.66 | 85,385.35 | 258,979.00 | 284,809.00 | 492,565.00 | 402,055 | \$ | 1,697,320.82 |
| E02334 | Napaskiak | 108,570.38 | 93,803.89 | 84,346.89 | 87,405.00 | 153,866.00 | 279,965.00 | 233,071 | \$ | 1,041,028.16 |
| E01336 | Nelson Lagoon | 12,981.82 | 18,033.85 | 52,461.73 | 52,586.00 | 48,802.00 | 50,145.00 | 44,882 | \$ | 279,892.40 |
| E03337 | Nenana | 30,674.75 | 35,920.53 | 73,415.93 | 96,364.00 | 88,562.00 | 84,913.00 | 75,552 | \$ | 485,402.21 |
| E01266 | New Koliganek | 84,079.26 | 72,477.49 | 65,151.49 | 68,220.00 | 64,094.00 | 61,881.00 | 56,741 | \$ | 472,644.24 |
| E01339 | New Stuyahok | 67,948.46 | 67,232.86 | 65,468.53 | 71,598.00 | 68,424.00 | 71,079.00 | 66,592 | \$ | 478,342.85 |
| E01340 | Newhalen | 106,126.64 | 87,285.56 | 884,991.12 | 744,701.00 | 653,392.00 | 612,647.00 | 518,850 | \$ | 3,607,993.32 |
| E02341 | Newtok | 21,951.25 | 32,775.33 | 383,987.00 | 631,096.00 | 556,132.00 | 523,833.00 | 485,808 | \$ | 2,635,582.58 |
| E02343 | Nightmute | 211,713.90 | 161,352.68 | 136,770.61 | 127,780.00 | 116,619.00 | 113,929.00 | 100,941 | \$ | 969,106.19 |
| E03345 | Nikolai | 264,220.05 | 191,403.28 | 140,135.26 | 125,260.00 | 112,253.00 | 106,005.00 | 91,753 | \$ | 1,031,029.59 |
| E01346 | Nikolski | 18,367.23 | 20,915.09 | 23,586.44 | 28,337.00 | 27,503.00 | 26,847.00 | 25,230 | \$ | 170,785.76 |
| E01348 | Ninilchik | 257,994.23 | 206,694.88 | 170,198.05 | 164,835.00 | 152,035.00 | 152,552.00 | 192,782 | \$ | 1,297,091.16 |
| E04350 | Noatak | 33,239.56 | 42,527.12 | 107,405.59 | 106,518.00 | 98,891.00 | 95,611.00 | 89,038 | \$ | 573,230.27 |
| E04352 | Nome | 173,505.60 | 167,588.65 | 246,186.81 | 248,081.00 | 244,663.00 | 272,126.00 | 410,533 | \$ | 1,762,684.06 |
| E01353 | Nondalton | 39,218.78 | 41,705.26 | 44,298.77 | 50,796.00 | 48,677.00 | 51,819.00 | 50,498 | \$ | 327,012.81 |

Alaska Tribal Shares of National Tribal Transportation \$

| | | | | | | | | | | |
|--------|------------------|--------------|------------|--------------|------------|------------|------------|---------|----|--------------|
| E04355 | Noorvik | 158,178.20 | 132,759.37 | 540,212.38 | 471,416.00 | 418,062.00 | 394,430.00 | 334,550 | \$ | 2,449,607.95 |
| E03358 | Northway | 43,980.95 | 48,149.29 | 51,652.75 | 69,984.00 | 66,629.00 | 61,246.00 | 72,317 | \$ | 413,958.99 |
| E03354 | Nuiqsut | 613,101.24 | 445,402.42 | 324,762.09 | 286,450.00 | 255,324.00 | 218,299.00 | 190,313 | \$ | 2,333,651.75 |
| E03359 | Nulato | 193,130.41 | 150,483.20 | 122,906.98 | 117,501.00 | 107,626.00 | 124,230.00 | 108,951 | \$ | 924,828.59 |
| E02490 | Nunakauyarmiut | 53,375.99 | 58,843.86 | 63,644.47 | 71,558.00 | 74,721.00 | 80,992.00 | 74,938 | \$ | 478,073.32 |
| E02436 | Nunam Iqua | 15,544.05 | 25,055.08 | 32,973.54 | 41,387.00 | 764,586.00 | 796,077.00 | 639,619 | \$ | 2,315,241.67 |
| E02361 | Nunapitchuk | 28,247.03 | 40,009.52 | 49,351.85 | 65,514.00 | 63,589.00 | 66,858.00 | 63,480 | \$ | 377,049.40 |
| E02366 | Ohogamiut | 9,386.99 | 15,738.64 | 21,032.98 | 26,870.00 | 178,156.00 | 944,549.00 | 762,343 | \$ | 1,958,076.61 |
| E01367 | Old Harbor | 22,651.28 | 30,831.65 | 37,768.98 | 44,533.00 | 43,591.00 | 81,806.00 | 72,129 | \$ | 333,310.91 |
| E02043 | Orutsararmiut | 183,004.74 | 219,534.50 | 195,039.98 | 223,033.00 | 212,998.00 | 313,252.00 | 310,688 | \$ | 1,657,550.22 |
| E02369 | Oscarville | 8,947.70 | 15,209.26 | 20,507.73 | 26,304.00 | 92,970.00 | 152,411.00 | 121,876 | \$ | 438,225.69 |
| E01370 | Ouzinkie | 169,093.65 | 131,499.69 | 105,743.58 | 217,557.00 | 194,652.00 | 325,624.00 | 265,426 | \$ | 1,409,595.92 |
| E02371 | Paimiut | 8,837.94 | 15,076.90 | 20,316.72 | 26,098.00 | 25,701.00 | 25,230.00 | 24,093 | \$ | 145,353.56 |
| E01324 | Pauloff Harbor | 119,368.98 | 90,685.11 | 70,943.00 | 67,462.00 | 61,596.00 | 58,747.00 | 51,935 | \$ | 520,737.09 |
| E01376 | Pedro Bay | 65,906.53 | 53,810.24 | 81,636.23 | 76,382.00 | 70,363.00 | 67,104.00 | 56,037 | \$ | 471,239.00 |
| E01378 | Perryville | 71,772.70 | 62,298.97 | 73,294.77 | 74,610.00 | 68,012.00 | 64,789.00 | 67,168 | \$ | 481,945.44 |
| E09380 | Petersburg | 1,291,111.08 | 939,592.25 | 1,071,866.22 | 919,884.00 | 804,968.00 | 750,243.00 | 684,934 | \$ | 6,462,598.55 |
| E01383 | Pilot Point | 231,875.77 | 172,949.43 | 132,206.26 | 149,623.00 | 134,585.00 | 126,184.00 | 110,992 | \$ | 1,058,415.46 |
| E02384 | Pilot Station | 44,041.01 | 52,847.34 | 59,987.55 | 69,580.00 | 483,231.00 | 659,464.00 | 511,552 | \$ | 1,880,702.90 |
| E02385 | Pitka's Point | 18,780.16 | 26,518.89 | 32,607.43 | 40,684.00 | 317,457.00 | 312,001.00 | 244,650 | \$ | 992,698.48 |
| E02388 | Platinum | 7,995.83 | 14,062.25 | 19,266.21 | 24,914.00 | 24,625.00 | 282,834.00 | 245,550 | \$ | 619,247.29 |
| E03389 | Point Hope | 200,707.54 | 479,404.16 | 350,930.17 | 310,011.00 | 277,863.00 | 261,827.00 | 229,133 | \$ | 2,109,875.87 |
| E03390 | Point Lay | 105,656.73 | 94,582.21 | 82,356.17 | 83,687.00 | 77,811.00 | 59,337.00 | 55,431 | \$ | 558,861.11 |
| E01396 | Port Graham | 113,641.58 | 92,353.82 | 78,258.97 | 78,557.00 | 258,323.00 | 243,558.00 | 221,004 | \$ | 1,085,696.37 |
| E01397 | Port Heiden | 310,431.60 | 222,833.70 | 161,085.06 | 141,727.00 | 126,533.00 | 118,836.00 | 172,517 | \$ | 1,253,963.36 |
| E01398 | Port Lions | 225,418.40 | 170,709.61 | 130,166.67 | 121,889.00 | 111,035.00 | 105,443.00 | 95,447 | \$ | 960,108.68 |
| E01402 | Portage Creek | 36,018.22 | 33,159.77 | 32,010.88 | 35,364.00 | 33,656.00 | 32,632.00 | 32,876 | \$ | 235,716.87 |
| E01424 | Qagan Tayagungin | 158,898.95 | 129,499.74 | 108,461.35 | 111,640.00 | 103,060.00 | 98,472.00 | 87,990 | \$ | 798,022.04 |
| E01510 | Qawalangin | 62,437.49 | 65,874.35 | 122,356.22 | 121,863.00 | 111,836.00 | 123,569.00 | 109,852 | \$ | 717,788.06 |
| E03410 | Rampart | 81,170.55 | 200,239.24 | 144,981.13 | 128,661.00 | 115,096.00 | 108,532.00 | 100,528 | \$ | 879,207.92 |
| E02411 | Red Devil | 45,402.43 | 39,557.47 | 36,271.26 | 38,621.00 | 36,458.00 | 35,245.00 | 32,262 | \$ | 263,817.16 |
| E03416 | Ruby | 102,326.18 | 84,567.15 | 72,974.59 | 83,147.00 | 76,997.00 | 149,337.00 | 129,515 | \$ | 698,863.92 |
| E01455 | Saint George | 271,226.47 | 201,267.77 | 152,076.17 | 139,628.00 | 127,129.00 | 125,645.00 | 116,117 | \$ | 1,133,089.41 |
| E04457 | Saint Michael | 141,125.48 | 116,230.17 | 528,007.18 | 453,372.00 | 400,813.00 | 363,613.00 | 289,957 | \$ | 2,293,117.83 |
| E01458 | Saint Paul | 167,697.36 | 137,890.11 | 171,079.60 | 160,781.00 | 146,540.00 | 139,432.00 | 163,559 | \$ | 1,086,979.07 |
| E01419 | Salamatof | 17,521.19 | 200,331.96 | 152,938.19 | 140,209.00 | 127,046.00 | 120,812.00 | 105,932 | \$ | 864,790.34 |
| E04425 | Savoonga | 121,383.35 | 107,591.65 | 1,037,376.77 | 878,149.00 | 785,895.00 | 737,789.00 | 581,109 | \$ | 4,249,293.77 |

Alaska Tribal Shares of National Tribal Transportation \$

| | | | | | | | | | | |
|--------|-----------------|------------|------------|------------|------------|------------|------------|---------|----|--------------|
| E09427 | Saxman | 78,488.69 | 71,162.69 | 66,975.59 | 70,596.00 | 67,427.00 | 64,535.00 | 60,496 | \$ | 479,680.97 |
| E02428 | Scammon Bay | 116,612.70 | 100,977.99 | 90,756.90 | 93,551.00 | 535,174.00 | 559,529.00 | 454,328 | \$ | 1,950,929.59 |
| E04429 | Selawik | 173,411.13 | 146,331.70 | 127,371.15 | 128,037.00 | 148,233.00 | 155,356.00 | 140,611 | \$ | 1,019,350.98 |
| E01431 | Seldovia | 275,328.19 | 204,123.28 | 143,301.94 | 160,433.00 | 144,234.00 | 181,038.00 | 154,830 | \$ | 1,263,288.41 |
| E03434 | Shageluk | 44,873.65 | 44,356.10 | 45,065.59 | 50,987.00 | 48,817.00 | 47,342.00 | 43,868 | \$ | 325,309.34 |
| E04435 | Shaktoolik | 54,088.03 | 53,211.16 | 840,029.89 | 709,508.00 | 623,467.00 | 585,209.00 | 455,826 | \$ | 3,321,339.08 |
| E04438 | Shishmaref | 285,685.42 | 220,456.55 | 491,557.88 | 430,813.00 | 382,141.00 | 910,569.00 | 713,284 | \$ | 3,434,506.85 |
| E04440 | Shungnak | 63,844.39 | 132,904.96 | 187,471.13 | 232,391.00 | 207,899.00 | 196,115.00 | 165,113 | \$ | 1,185,738.48 |
| E09442 | Sitka | 149,595.56 | 249,921.79 | 240,978.00 | 260,956.00 | 195,257.00 | 179,692.00 | 174,990 | \$ | 1,451,390.35 |
| E09443 | Skagway | 9,936.27 | 31,789.41 | 34,475.46 | 40,550.00 | 38,232.00 | 37,488.00 | 98,860 | \$ | 291,331.14 |
| E02446 | Sleetmute | 22,467.53 | 25,130.16 | 34,645.42 | 42,175.00 | 47,311.00 | 77,324.00 | 68,168 | \$ | 317,221.11 |
| E04447 | Solomon | 11,856.77 | 10,463.04 | 451,848.31 | 375,910.00 | 328,464.00 | 307,443.00 | 236,752 | \$ | 1,722,737.12 |
| E01450 | South Naknek | 177,317.11 | 135,874.39 | 107,201.76 | 102,228.00 | 93,470.00 | 89,211.00 | 79,450 | \$ | 784,752.26 |
| E04460 | Stebbins | 103,233.48 | 80,282.80 | 341,800.55 | 301,760.00 | 269,653.00 | 250,351.00 | 205,132 | \$ | 1,552,212.83 |
| E03463 | Stevens Village | 43,109.90 | 384,127.68 | 270,691.47 | 233,277.00 | 206,378.00 | 195,577.00 | 166,929 | \$ | 1,500,090.05 |
| E02464 | Stony River | 23,499.44 | 25,000.08 | 80,498.40 | 75,525.00 | 68,861.00 | 65,527.00 | 57,821 | \$ | 396,731.92 |
| E01850 | Sun'aq | 33,447.30 | 46,627.64 | 95,438.68 | 87,819.00 | 85,392.00 | 109,713.00 | 150,405 | \$ | 608,842.62 |
| E03472 | Takotna | 29,341.47 | 22,974.95 | 15,734.75 | 15,707.00 | 14,545.00 | 68,800.00 | 58,550 | \$ | 225,653.17 |
| E03474 | Tanacross | 34,247.44 | 37,275.11 | 39,626.31 | 46,522.00 | 45,087.00 | 46,375.00 | 43,275 | \$ | 292,407.86 |
| E03475 | Tanana | 26,136.81 | 33,968.68 | 104,234.46 | 101,180.00 | 93,111.00 | 89,045.00 | 79,271 | \$ | 526,946.95 |
| E01477 | Tatitlek | 61,903.74 | 52,226.96 | 46,112.22 | 57,585.00 | 54,548.00 | 52,765.00 | 38,686 | \$ | 363,826.92 |
| E01478 | Tazlina | 67,930.77 | 56,250.77 | 48,760.10 | 207,147.00 | 256,723.00 | 278,074.00 | 237,976 | \$ | 1,152,861.64 |
| E03479 | Telida | 7,929.12 | 7,739.15 | 7,850.44 | 8,957.00 | 8,587.00 | 11,729.00 | 10,884 | \$ | 63,675.71 |
| E04480 | Teller | 42,321.09 | 45,342.55 | 369,921.25 | 321,361.00 | 285,311.00 | 270,110.00 | 216,877 | \$ | 1,551,243.89 |
| E03483 | Tetlin | 192,531.66 | 146,284.38 | 114,288.81 | 315,913.00 | 279,785.00 | 263,654.00 | 225,516 | \$ | 1,537,972.85 |
| E01487 | Togiak | 118,318.49 | 107,874.76 | 207,175.58 | 191,153.00 | 173,712.00 | 163,105.00 | 143,591 | \$ | 1,104,929.83 |
| E02497 | Tuluksak | 58,963.74 | 60,169.42 | 62,034.66 | 69,486.00 | 74,665.00 | 72,409.00 | 66,890 | \$ | 464,617.82 |
| E02499 | Tuntutuliak | 56,809.70 | 57,892.35 | 59,694.26 | 67,035.00 | 645,101.00 | 653,839.00 | 530,345 | \$ | 2,070,716.31 |
| E02500 | Tununak | 22,325.05 | 51,566.49 | 54,398.65 | 61,467.00 | 59,271.00 | 64,028.00 | 59,420 | \$ | 372,476.19 |
| E01503 | Twin Hills | 169,644.57 | 126,268.02 | 95,947.63 | 88,314.00 | 79,972.00 | 76,004.00 | 66,772 | \$ | 702,922.22 |
| E01505 | Tyonek | 243,006.75 | 182,895.62 | 140,716.31 | 305,871.00 | 271,316.00 | 256,083.00 | 217,243 | \$ | 1,617,131.68 |
| E01506 | Ugashik | 578,670.87 | 212,482.32 | 124,394.01 | 105,882.00 | 92,801.00 | 147,520.00 | 124,862 | \$ | 1,386,612.20 |
| E02508 | Umkumiut | 2,347.28 | 4,093.41 | 5,572.85 | 7,207.00 | 7,106.00 | 7,593.00 | 7,151 | \$ | 41,070.54 |
| E04509 | Unalakleet | 242,601.15 | 193,324.07 | 333,226.98 | 296,070.00 | 265,190.00 | 350,440.00 | 284,699 | \$ | 1,965,551.20 |
| E01511 | Unga | 207,995.63 | 152,070.70 | 112,840.85 | 102,226.00 | 91,992.00 | 87,099.00 | 75,798 | \$ | 830,022.18 |
| E03518 | Venetie | 67,500.69 | 152,614.32 | 614,594.19 | 538,420.00 | 486,056.00 | 466,302.00 | 370,479 | \$ | 2,695,966.20 |
| E03519 | Wainwright | 198,557.27 | 168,277.19 | 139,866.11 | 135,077.00 | 127,389.00 | 76,889.00 | 83,284 | \$ | 929,339.57 |

Alaska Tribal Shares of National Tribal Transportation \$

| | | | | | | | | | | |
|--------|----------------|------------|-----------|------------|------------|------------|------------|---------|----|-----------------------|
| E04521 | Wales | 98,847.70 | 82,130.29 | 484,517.05 | 413,935.00 | 365,357.00 | 343,480.00 | 272,353 | \$ | 2,060,620.04 |
| E04525 | White Mountain | 124,828.14 | 93,676.82 | 685,664.86 | 581,002.00 | 511,228.00 | 476,999.00 | 375,281 | \$ | 2,848,679.82 |
| E09532 | Wrangell | 62,972.41 | 65,936.92 | 305,817.03 | 783,313.00 | 689,753.00 | 656,519.00 | 727,919 | \$ | 3,292,230.36 |
| E09533 | Yakutat | 76,103.60 | 70,811.80 | 239,059.57 | 247,986.00 | 221,381.00 | 192,729.00 | 177,643 | \$ | 1,225,713.97 |
| | | | | | | | | | \$ | 299,212,421.94 |

SE Alaska Tribal Transportation Funding

SE Alaska Tribes Share of National Tribal Transportation Funds

| Tribe | 2008 | 2009 | 2010 | 2011 | 2012 | Total FY08-FY12 Sum |
|---|------------|------------|------------|-----------|-----------|---------------------|
| Angoon Community Association | 225,887 | 239,425 | 221,232 | 208,896 | 180,133 | \$ 1,075,573.74 |
| Central Council Tlingit & Haida Indian Tribes | 667,714 | 664,839 | 786,731 | 706,870 | 775,065 | \$ 3,601,218.89 |
| Chilkat Indian Village (Klukwan) | 355,352 | 321,215 | 284,495 | 278,538 | 242,907 | \$ 1,482,506.82 |
| Chilkoot Indian Association (Haines) | 242,505 | 1,501,319 | 1,169,215 | 1,120,201 | 1,005,405 | \$ 5,038,644.83 |
| Craig Community Association | 1,005,412 | 850,559 | 747,289 | 703,389 | 596,875 | \$ 3,903,524.35 |
| Douglas Indian Association | 158,166 | 151,823 | 166,400 | 145,419 | 125,288 | \$ 747,096.12 |
| Hoonah Indian Association | 296,494 | 501,428 | 443,152 | 413,658 | 354,504 | \$ 2,009,236.06 |
| Hydaburg Cooperative Association | 2,040,798 | 1,700,474 | 1,537,553 | 1,432,878 | 1,206,870 | \$ 7,918,573.14 |
| Kake, Organized Village of | 845,183 | 720,159 | 634,555 | 598,655 | 505,291 | \$ 3,303,842.67 |
| Kasaan, Organized Village of | 1,579,859 | 1,304,100 | 1,137,211 | 1,066,109 | 895,349 | \$ 5,982,627.80 |
| Ketchikan Indian Corporation | 739,967 | 671,481 | 614,305 | 624,598 | 592,975 | \$ 3,243,325.99 |
| Klawock Cooperative Association | 1,349,967 | 1,133,101 | 1,005,042 | 962,381 | 813,808 | \$ 5,264,299.24 |
| Petersburg Indian Association | 1,071,866 | 919,884 | 804,968 | 126,184 | 110,992 | \$ 3,033,894.21 |
| Saxman, Organized Village of | 66,975 | 70,596 | 67,427 | 64,535 | 60,495 | \$ 330,028.23 |
| Sitka Tribe of Alaska | 240,978 | 260,956 | 195,258 | 37,488 | 98,860 | \$ 833,539.92 |
| Skagway Village | 34,475 | 40,550 | 38,231 | 77,324 | 68,168 | \$ 258,748.26 |
| Wrangell Cooperative Association | 305,817 | 789,313 | 689,753 | 656,519 | 727,919 | \$ 3,169,320.63 |
| Yakutat Tlingit Tribe | 239,059 | 247,986 | 221,382 | 192,729 | 177,643 | \$ 1,078,798.76 |
| | 11,466,474 | 12,089,208 | 10,764,199 | 9,416,371 | 8,538,547 | \$ 52,274,799.66 |

Tribal Transportation \$ via SAFETEA/MAP-21

Assembled by CCTHITA for Sen Egan/Transportation Committee

9-Jan-13

Denali Commission Transportation Program



Transportation

The Denali Commission's (Commission) Transportation Program was originally created in 2005 as part of the Safe Accountable Flexible Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) legislation and accompanying amendments to the Denali Commission Act of 1998 (as amended).

The program includes two major components, rural roads and waterfront development.

- The roads portion of the program targets the planning, design and construction of basic road improvement needs including projects that connect rural communities to one another and the state highway system, and opportunities to enhance rural economic development. Eligible road project types include, but are not limited to: ATV board roads, local community road and street improvements, and roads and board roads to subsistence use sites.
- The waterfront portion of the program addresses planning, design and construction of port, harbor and other rural waterfront needs. Eligible project types include, but are not limited to: regional ports, barge landings and docking facilities.

SAFETEA-LU expired in 2009 and operated under a continuing resolution from June of 2009 through June of 2012.

In late June of 2012, Congress passed a two-year transportation bill, titled MAP-21, that did not include authorization or funding for the Commission's transportation program.

Commission staff will continue to administer the program in coordination with the members of the Transportation Advisory Committee (TAC). The TAC is the body who rates and ranks project submissions, recommends projects to the Federal Co-Chair, and advises the Commission on rural surface transportation needs in Alaska.

The Commission works closely with the following recipients and program partners:

- U.S. Federal Highway Administration (FHWA), Western Federal Lands Highway Division(WFLHD) and Alaska Division
- Alaska Department of Transportation and Public Facilities (ADOT&PF)
- U.S. Army Corps of Engineers (USACE), Alaska Division
- Regional, local and tribal governments, and regional tribal nonprofits

Contacts:

Tessa Delong - Direct: 907-271-1624 - tdelong@denali.gov,

Adison Wetzel - Direct: 907-271-1640 - awetzel@denali.gov



SAFETEA-LU

Safe Accountable Flexible Efficient Transportation Equity Act:
A Legacy for Users

[Legislation](#) | [Summary Information](#) | [Fact Sheets](#) | [Funding Tables](#) | [Cross Reference](#)

Fact Sheets on Highway Provisions

DENALI ACCESS SYSTEM PROGRAM

| Year | 2005 | 2006 | 2007 | 2008 | 2009 |
|---------------|------|--------|--------|--------|--------|
| Authorization | \$0 | \$15 M | \$15 M | \$15 M | \$15 M |

Program Purpose

The new Denali Access System Program will fund planning, design, engineering, and construction of road and other surface transportation infrastructure identified for the Denali access system.

Statutory References

SAFETEA-LU Section(s): 1960

Other: Denali Commission Act of 1998 (42 USC 3121 note)

Funding/Formula

Funded by contract authority, to remain available until expended. Contract authority is not subject for transfer and is subject to the overall Federal-aid obligation limitation. [1960]

The Secretary is to provide the funds directly to the Denali Commission to carry out the program in consultation with the Denali Access System Program Advisory Committee. In distributing funds for projects, the Commission is to consult the list of transportation priorities developed by the Advisory Committee.

In addition to the funds authorized for the Denali Access System, the State of Alaska may transfer up to 15 percent of its Surface Transportation Program apportionment to the Denali Access System program.

Eligible Use of Funds

Eligible uses are planning, design, engineering, and construction of road and other surface transportation infrastructure identified for the Denali access system. Funding for a construction project under this program may include additional amount equal to not more than 10 percent of the total cost of construction to be retained for future maintenance of the project. Funds so retained may not be used for any other purpose.

Program Features

The Commission is the lead agency for purposes of accepting Federal funds and for purposes of carrying out the program.

The Denali Access Program Advisory Committee is established to consist of 9 members, including the chairman of the Denali Commission; four members representing existing regional native corporation, native nonprofit entities, or tribal governments, of which one of the four must be a civil engineer; and four members representing rural Alaska regions or villages, of which one of the four must be a civil engineer. The Advisory Committee's responsibilities are to:

- advise the Commission on surface transportation needs of Alaska Native villages and rural communities, including projects for the construction of essential access routes within remote Alaska Native villages and rural communities and for the construction of roads and facilities necessary to connect isolated rural communities to a road system
- advise the Commission on considerations for coordinating transportation planning among the Alaska Native villages, Alaska rural villages, the State, and other government entities
- establish an annual list of transportation project priorities and funding recommendations for Alaska Native villages and rural communities
- facilitate the Commission's work when a transportation project involves more than one region.

Construction of projects under this section shall, to the maximum extent practicable, encourage the use of employees and businesses that are residents of Alaska.

Technology and design standards for a Denali Access System project shall be determined by the Commission as appropriate given the location and the functionality of the project.

Federal Share

The Federal share is 80 percent, subject to the sliding scale adjustment. Denali funds may be used as the non-Federal share of the costs of projects funded under 23 USC.

[FHWA Home](#) | [Feedback](#)



United States Department of Transportation - **Federal Highway Administration**



Search

Search for a Project

[View All Projects](#)

Recipient Project #:

Recipient:

Project Title:

Themes:

Community:

Current Milestone:

111 Projects Found

| Project | Recipient Organization | Communitie s | Project Title | Current Milestone |
|--------------------|---|---------------------|--|----------------------------|
| <u>01003 77403</u> | Department of Transportation | Emmonak, Statewide | Dust Control Research | Project Close-out Complete |
| <u>01025</u> | Bristol Bay Native Association | Ekuk | Clarks Point Road Reconnaissance | Project Close-out Complete |
| <u>01026</u> | Bristol Bay Native Association | Pilot Point | Dago Creek Road Realignment | Project Close-out Complete |
| <u>01032</u> | Association of Village Council Presidents | Statewide | Regional Roads Planning & Reconnaissance | Project Close-out Complete |
| <u>01035</u> | Sand Point, City of | Sand Point | School Loop Road Rehabilitation | Project Close-out Complete |
| <u>01036</u> | Department of Transportation | Elfin Cove | Boardwalk Design and Construction | Project Close-out Complete |
| <u>01037</u> | Department of Transportation | Pelican | Board Roads Reconstruction Phase III | Project Close-out Complete |
| <u>01040</u> | Nenana Native Council (Association) | Nenana | 9th and K Streets Reconstrucion Phase II | Cancelled Complete |
| <u>01043</u> | Western Federal Land | Akhiok, | 2008 Various | Project Close- |

| | | | | |
|--------------|--|--|--|--------------------------------|
| | Highway Division (WFLHD) | Chefornak, Hooper Bay, Kwigillingok | Reconnaissance Engineering Efforts | out Complete |
| <u>01114</u> | Dept of Commerce, Community & Econ. Dev. | Koyukuk, Manley Hot Springs, McGrath, Minto, Nenana, Nikolai, Takotna, Tanacross | DCCED Ortho Mapping Project | Project Close- out Complete |
| <u>01130</u> | Kawerak, Inc. | Gambell | Gambell Evacuation Road Reconstruction | Cancelled Complete |
| <u>01133</u> | Association of Village Council Presidents | Bethel, Oscarville | Oscarville to Bethel Road Reconnaissance | Planning/Desig n |
| <u>01135</u> | Association of Village Council Presidents | Pilot Station | Pilot Station Community Streets Rehabilitation | Construction |
| <u>01136</u> | Kawerak, Inc. | Brevig Mission | Brevig Mission Community Streets Construction | Construction Complete |
| <u>01137</u> | Association of Village Council Presidents | Nunam Iqua | Nunam Iqua Boardwalk Reconstruction | Project Close- out Complete |
| <u>01148</u> | Craig Community Association | Craig | Craig Community Streets Construction | Project Close- out Complete |
| <u>01157</u> | Alaska Village Electric Cooperative | Hooper Bay | Hooper Bay Wind Turbine Access Road Construction | Project Close- out Complete |
| <u>01164</u> | Department of Transportation | Takotna | Takotna Gold Creek Bridge Replacement | Project Close- out Complete |
| <u>01165</u> | Department of Transportation | Cold Bay | Cold Bay Road Improvements | Cancelled Complete |
| <u>01167</u> | Native Village of Selawik | Selawik | Selawik - Boardwalk Construction | Project Close- out Complete |
| <u>01240</u> | Igiugig Village Council | Igiugig | Igiugig High Ridge Court Road - Construction | Project Close- out Complete |
| <u>01243</u> | Native Village of Eyak | Cordova, Eyak | Native Village of Eyak - Whitshed Road Extension Project | Project Close- out Complete |
| <u>01246</u> | Department of | Northway | Northway Access Road | Project Close- |

| | | | | |
|--------------|---|----------------------------|---|----------------------------|
| | Transportation | | Improvements | out Complete |
| <u>01248</u> | Klawock Cooperative Association | Klawock | Klawock Community Streets Paving Project | Project Close-out Complete |
| <u>01249</u> | Association of Village Council Presidents | Tuntutuliak | Tuntutuliak Board Road Reconstruction/Extension Project | Project Close-out Complete |
| <u>01250</u> | Association of Village Council Presidents | Alakanuk | Alakanuk Community Streets Rehabilitation Design Project | Project Close-out Complete |
| <u>01251</u> | Association of Village Council Presidents | Scammon Bay | Scammon Bay Community Streets Project - Design | Project Close-out Complete |
| <u>01256</u> | Department of Transportation | Healy | Healy Community Roads Rehabilitation/Surfacing | Planning/Design Complete |
| <u>01257</u> | Department of Transportation | Pitkas Point, Saint Mary's | St. Mary's and Pitka's Point Road Dust Control | Construction |
| <u>01258</u> | Department of Transportation | Teller | Teller - Airport Road Dust Control | Project Close-out Complete |
| <u>01259</u> | Department of Transportation | Tanacross | Tanacross Road Improvements Design & Construction | Design |
| <u>01260</u> | Dillingham, City of | Dillingham | City of Dillingham - Tower Road Reconstruction | Project Close-out Complete |
| <u>01262</u> | Kawerak, Inc. | Gambell | Gambell Community Streets | Project Close-out Complete |
| <u>01278</u> | Department of Transportation | Wrangell | Wrangell Downtown Reconstruction - Completion of Front Street | Construction |
| <u>01279</u> | Department of Transportation | Elfin Cove | Elfin Cove - Boardwalk Phase II - Design and Construction | Construction Complete |
| <u>01291</u> | Department of Transportation | Port Alexander | Port Alexander Boardwalk Phase II - Design/Construction | Construction |
| <u>01293</u> | Bristol Bay Borough | Naknek | School Bus Route Road Improvements - Design | Project Close-out Complete |

| | | | | |
|-------------------------------|---|---------------------|--|----------------------------|
| <u>01297</u> | Maniilaq Association | Noorvik | Noorvik Native Community Cemetery Road Construction | Project Close-out Complete |
| <u>01304</u> | Bristol Bay Borough | King Salmon, Naknek | King Salmon and Naknek School Bus Route Road Improvements - Construction | Final Design |
| <u>01313</u> | Association of Village Council Presidents | Scammon Bay | Scammon Bay Community Streets Reconstruction | Construction |
| <u>01316</u> | North Slope Borough | Anaktuvuk Pass | Anaktuvuk Pass Contact Bridge Replacement Construction Project | Construction |
| <u>1324</u> | Department of Transportation | Sand Point | Sand Point School Loop Road Rehabilitation | Construction |
| <u>1377</u> | Petersburg, City of | Petersburg | Sing Lee Alley Bridge Design | Planning/Design |
| <u>1378</u> | Petersburg, City of | Petersburg | Birch Street Rehabilitation - Construction | Project Close-out Complete |
| <u>1381</u> | Department of Transportation | Gambell | Gambell Evacuation Road - Construction | Construction |
| <u>1385</u> | Cordova, City of | Cordova | Cordova Dust Control Project - Material Purchase | Project Close-out Complete |
| <u>1410</u> | Department of Transportation | Fort Yukon | Fort Yukon Dust Control and Construction Project | Construction Complete |
| <u>1418</u> | Nenana, City of | Nenana | Nenana 9th Avenue Reconstruction Phase II | Final Design Pending |
| <u>229-06</u> <u>76918</u> | Department of Transportation | Fort Yukon | Dust Control Design & Construction | Project Close-out Complete |
| <u>234-06</u> | Department of Transportation | Selawik | Skin Street/Barge Landing Road Reconstruction | Project Close-out |
| <u>235-06</u> | Department of Transportation | Tanana | Dust Control Design & Construction | Project Close-out Complete |
| <u>236-06</u> | Department of Transportation | Togiak | Togiak Main Road Rehabilitation | Project Close-out Complete |

| | | | | |
|---------------|---|------------|--|--|
| <u>237-06</u> | Department of Transportation | Bethel | Y-K Delta Winter Trail Marking Design, Construction & Installation | Preliminary design including Environmental Doc. |
| <u>239-06</u> | Department of Transportation | Gulkana | Road Chip Seal-Paving | Cancelled Pending |
| <u>246-07</u> | Western Federal Land Highway Division (WFLHD) | Metlakatla | Walden Point Road Funding - WFLHD | Project Close-out Complete |
| <u>247-07</u> | Western Federal Land Highway Division (WFLHD) | King Cove | Street Improvement - WFLHD | Project Close-out Complete |
| <u>256-07</u> | Kawerak, Inc. | Koyuk | Community Streets Reconstruction | Project Close-out Complete |
| <u>257-07</u> | Kawerak, Inc. | Shaktoolik | Evacuation Road Preliminary Engineering | Preliminary Design including Environmental Docs |
| <u>258-07</u> | Kawerak, Inc. | Unalakleet | Community Streets Improvements Design | Preliminary Design including Environmental Docs Complete |
| <u>285-07</u> | Western Federal Land Highway Division (WFLHD) | Kivalina | Kivalina Evacuation Road | Project Close-out Complete |
| <u>291-07</u> | Gulkana Village Council | Gulkana | Road Paving Project | Project Close-out Complete |
| <u>292-07</u> | Cordova, City of | Cordova | Cordova Dust Control with Asphalt Surfacing | Project Close-out Complete |
| <u>299-07</u> | Alaska Energy Authority | Kwethluk | Kwethluk Barge Landing and Jay Hammond Way Road Extension Design | Project Close-out Complete |
| <u>301-07</u> | Northwest Arctic Borough School District | Noatak | School Access Road Design | Project Close-out Complete |
| <u>305-07</u> | Alaska Energy Authority | Kwethluk | Community and Barge Landing Roads Design | Project Close-out Complete |
| <u>306-07</u> | Sitka, City & Borough | Sitka | Sitka Indian River Road | Project Close- |

| | | | | |
|---------------|---|--------------------|---|--|
| <u>309-07</u> | of Gakona Village Council | Gakona | Upgrade and Paving School Road Rehabilitation and Construction | out Complete Construction |
| <u>311-07</u> | Native Village of Stevens Village | Stevens Village | Community Roads Construction | Project Close- out Complete |
| <u>324-07</u> | Department of Transportation | Pelican | Boardwalk Construction Project | Project Close- out Complete |
| <u>343-07</u> | Alaska Village Electric Cooperative | Hooper Bay | Hooper Bay Wind Turbine Access Road Construction | Project Close- out Complete |
| <u>345-07</u> | Kawerak, Inc. | Unalakleet | Community Roads Construction | Project Close- out Complete |
| <u>346-07</u> | Craig Community Association | Craig | Port St. Nicholas Road Reconstruction Project | Construction |
| <u>351-07</u> | Western Federal Land Highway Division (WFLHD) | Kobuk | Snowmobile Bridges | Project Close- out Complete |
| <u>367-08</u> | Chenega IRA Council | Chenega Bay | Chenega Bay Community Roads Construction | Project Close- out Complete |
| <u>375-08</u> | Department of Transportation | Cantwell | Cantwell Dust Control Construction | Project Close- out Complete |
| <u>382-08</u> | North Slope Borough | Anaktuvuk Pass | Bridge Design | Project Close- out Complete |
| <u>387-08</u> | Western Federal Land Highway Division (WFLHD) | Stevens Village | Access Road Reconnaissance Engineering | Project Close- out Complete |
| <u>394-08</u> | Department of Transportation | Port Alexander | Port Alexander: South Cedar Street Boardwalk | Project Close- out Complete |
| <u>396-08</u> | Department of Transportation | Coffman Cove | Coffman Cove Roads Paving Construction | Project Close- out |
| <u>61307</u> | Department of Transportation | Emmonak | Community Road Improvements | Final Design including Right of Way Docs Complete |
| <u>62940</u> | Department of Transportation | Cantwell | Dust Control Design & Construction | Project Close- out |
| <u>62988</u> | Department of Transportation | Circle | Dust Control Design & Construction | Construction Complete |

| | | | | |
|-----------------|---|------------------------------|--|----------------------------|
| <u>63044</u> | Department of Transportation | Eagle, Eagle Village | Dust Control Design & Construction | Project Close-out Complete |
| <u>69099</u> | Department of Transportation | Coffman Cove | Road Paving Design | Project Close-out Complete |
| <u>69211</u> | Department of Transportation | Elfin Cove | Boardwalk Repairs | Project Close-out Complete |
| <u>69216</u> | Department of Transportation | Pelican | Boardwalk Repairs | Project Close-out Complete |
| <u>69218</u> | Department of Transportation | Port Alexander | Boardwalk Repairs | Project Close-out Complete |
| <u>AN07-GJ1</u> | Alaska Native Tribal Health Consortium | Nunam Iqua | Boardwalk Design | Project Close-out Complete |
| <u>AN07-GJ2</u> | Alaska Native Tribal Health Consortium | Pitkas Point | Sanitation Road Construction | Project Close-out Complete |
| <u>RA-501</u> | Western Federal Land Highway Division (WFLHD) | Chignik Lagoon, Chignik Lake | Innovative Readiness Training (IRT) - Chigniks Project | Project Close-out Complete |
| <u>RA-503</u> | Western Federal Land Highway Division (WFLHD) | Gakona | Village School Road Rehabilitation Project | Construction |
| <u>RA-509</u> | Western Federal Land Highway Division (WFLHD) | Atka | Atka Roads Rehabilitation | Project Close-out |
| <u>RA-510</u> | Western Federal Land Highway Division (WFLHD) | Kalskag | Reimbursable Agreement | Project Close-out Complete |
| <u>RA-511</u> | Western Federal Land Highway Division (WFLHD) | Manley Hot Springs | Manley Hot Springs Community Streets Design | Project Close-out Complete |
| <u>RA-512</u> | Western Federal Land Highway Division (WFLHD) | Manokotak | Manokotak Heights Road | Construction |
| <u>RA-513</u> | Western Federal Land Highway Division (WFLHD) | McGrath | McGrath Road Reconstruction | Construction |
| <u>RA-514</u> | Western Federal Land Highway Division (WFLHD) | Atmautluak, Nunapitchuk | Tevyraq Railway Tram Design | Planning/Design |
| <u>RA-515</u> | Western Federal Land Highway Division (WFLHD) | Statewide | Technical Services - Commission Transportation Program | In-Progress |

| | | | | |
|---------------|---|--------------|--|----------------------------|
| <u>RA-521</u> | Western Federal Land Highway Division (WFLHD) | Akhiok | Akhiok Tsunami Shelter Road | Project Close-out Complete |
| <u>RA-522</u> | Western Federal Land Highway Division (WFLHD) | Tununak | Tununak ATV Geo-Tech Trail Design | Construction |
| <u>RA-523</u> | Western Federal Land Highway Division (WFLHD) | Akiak | Akiak Roads Rehabilitation | Construction |
| <u>RA-524</u> | Western Federal Land Highway Division (WFLHD) | Kwigillingok | Kuicuag Slough Subsistence ATV Trail Construction | Project Close-out Complete |
| <u>RA-525</u> | Western Federal Land Highway Division (WFLHD) | Kwethluk | Kwethluk - Airport Roads Improvements and Extensions | Construction |
| <u>RA-526</u> | Western Federal Land Highway Division (WFLHD) | Shaktoolik | Shaktoolik Evacuation Road Design | Cancelled Complete |
| <u>RA-528</u> | Western Federal Land Highway Division (WFLHD) | Nenana | Nenana 9th Avenue Reconstruction Phase II | Project Close-out Complete |
| <u>RA-530</u> | Western Federal Land Highway Division (WFLHD) | Noatak | Noatak-Delong Terminal Winter Access Route Design | Project Close-out Complete |
| <u>RA-531</u> | Western Federal Land Highway Division (WFLHD) | Tununak | Tununak - Community Streets Reconstruction Design | Project Close-out Complete |
| <u>RA-534</u> | Western Federal Land Highway Division (WFLHD) | Tununak | Tununak Community Streets Reconstruction | Construction |
| <u>RA-545</u> | Western Federal Land Highway Division (WFLHD) | Metlakatla | Metlakatla - Walden Point Connector Road - Design & Construction | Project Close-out Complete |
| <u>RA-551</u> | Western Federal Land Highway Division (WFLHD) | Chignik | Chignik Lake Culvert Replacement - Design | In-Progress |
| <u>RA-552</u> | Western Federal Land Highway Division (WFLHD) | Birch Creek | Birch Creek Feasibility Study | In-Progress |

907-271-1414 Fax 907-271-1415 Toll Free 888-480-4321
Contact System Administrator

BIA High Priority (Transportation) Project Program

Office of Federal Lands Highway

Location: [Home](#) > [Programs](#) > [Transportation](#) > [IRR](#) > [High Priority Projects](#)

IRR High Priority Projects (IRR HPP)

The IRR HPP is a special funding pool that can be used for:

- Tribes or governmental subdivision whose annual allocation is insufficient to complete its highest priority project;
- Emergency/disaster on any IRR facility

Project must meet minimum IRR requirements:

- Inventory
- Highest Priority by Tribe
- Dollar Amount must be greater than Tribe's Annual IRR Allocation and less than \$1,000,000.00

HPP Documents

- [FY 2012 High Priority Projects Funding Priority List](#) (Acrobat, 19 KB)
- [HPP Process Flow Charts](#) (Acrobat, 7.37 MB)
- [HPP Scoring Matrix](#) (Acrobat, 15 KB)
- **IRR High Priority Project Application (Non-Emergency/Disaster)**
 - [Non-Emergency/Disaster Application](#) (Acrobat, 72 KB)
 - [Non-Emergency/Disaster Instructions](#) (Acrobat, 83 KB)
 - [Non-Emergency/Disaster Regional Checklist](#) (Acrobat, 50 KB)
- **IRR High Priority Project Application (Emergency/Disaster)**
 - [Emergency/Disaster Application](#) (Acrobat, 70 KB)
 - [Emergency/Disaster Instructions](#) (Acrobat, 94 KB)
 - [Emergency/Disaster Regional Checklist](#) (Acrobat, 44 KB)

Page last updated: December 21, 2012. Last certified: July 2012.
Questions or Comments? [Contact the Web Team](#)

THIS WILL IMPACT AK

VILLAGES / TRIBES

IN MAP21
AUTHORIZATION;
NOT APPROPRIATION

| Tribe | Year | Project Type | Funded Amount | Sum Total By Year |
|-----------------------------------|------|---------------------|---------------|-------------------|
| Village of Port Lions | 2005 | Construction | 1,000,000.00 | |
| Ugaskik Village | 2005 | Construction | 1,000,000.00 | |
| Village of McGrath | 2005 | Construction | 1,000,000.00 | |
| Village of Nuiqsut | 2005 | Construction | 1,000,000.00 | |
| Village of Deering | 2005 | Construction | 1,000,000.00 | |
| Village of Kotlik | 2005 | Construction | 1,000,000.00 | |
| Village of Takitlek | 2005 | Construction | 1,000,000.00 | |
| Village of Koliganek | 2005 | Construction | 1,000,000.00 | |
| Village of Aniak | 2005 | Construction | 1,000,000.00 | |
| Village of Chistochina | 2005 | Construction | 1,000,000.00 | |
| Chenega Bay | 2006 | Construction | 1,000,000.00 | |
| Andreafski(Bethel) | 2006 | Construction | 1,000,000.00 | |
| Buckland | 2006 | Construction | 1,000,000.00 | |
| Village of Gulkana | 2006 | Construction | 1,000,000.00 | |
| Eagle (Fairbanks) | 2006 | Construction | 1,000,000.00 | |
| | | | | 15,000,000.00 |
| Native Village of Stevens | 2007 | Construction | 1,000,000.00 | |
| Agdaagux Tribe of King Cove | 2007 | Bridge Construction | 1,000,000.00 | |
| Native Village of Mekoryuk | 2007 | Construction | 1,000,000.00 | |
| Akiachak Native Community | 2007 | Construction | 607,013.00 | |
| Native Village of Kake | 2007 | Bridge Construction | 1,000,000.00 | |
| Nanana Native Association | 2007 | Construction | 1,000,000.00 | |
| | | | | 5,607,013.00 |
| Emmonak Village | 2008 | Construction | 1,000,000.00 | |
| Native Village of Nightmute | 2008 | PS&E Development | 249,880.00 | |
| Nunakuyak Tribe (Tooksook Bay) | 2008 | Construction | 1,000,000.00 | |
| Native Village of Atka | 2008 | Construction | 1,000,000.00 | |
| Crooked Creek Traditional Council | 2008 | PS&E Development | 490,000.00 | |
| Native Village of Chuathbaluk | 2008 | Construction | 1,000,000.00 | |
| Native Village of Unalakleet | 2008 | Construction | 1,000,000.00 | |
| Adaagux Tribe of King Cove | 2008 | Construction | 200,000.00 | |
| Native Village of Nanwalek | 2008 | Construction | 1,000,000.00 | |
| Native Village of Port Graham | 2008 | Construction | 1,000,000.00 | |

| | | | |
|--------------------------------------|------|-------------------------|---------------|
| Native Village of Nulato | 2008 | Construction | 1,000,000.00 |
| Naitve Village of Port Heiden | 2008 | Construction | 1,000,000.00 |
| Wrangell Cooperative Association | 2008 | PS&E Development | 249,500.00 |
| Pedro Bay Village Council | 2008 | PS&E Bridge Replacement | 320,000.00 |
| Kenaitze Indian Tribe | 2008 | Construction | 1,000,000.00 |
| Manokotak Village Council | 2008 | Construction | 1,000,000.00 |
| Village of Eruk | 2008 | PS&E Development | 300,000.00 |
| Chilkoot Indian Association | 2008 | Construction | 1,000,000.00 |
| Village of Atmautluak | 2008 | Construction | 1,000,000.00 |
| | | | 16,809,380.00 |
| Sitka Tribe of Alaska | 2009 | Construction | 635,291.00 |
| Native Village of Kwethluk | 2009 | Construction | 249,000.00 |
| Native Village of Tuntutuliak | 2009 | Construction | 1,000,000.00 |
| Native Village of Eek | 2009 | PS&E Development | 499,000.00 |
| Chevak Native Village | 2009 | PS&E Development | 499,000.00 |
| Yakutat Tlingit Tribe | 2009 | PS&E Development | 187,500.00 |
| Native Village of Ruby | 2009 | PS&E Development | 270,000.00 |
| Native Village of Kiana | 2009 | PS&E Development | 411,000.00 |
| Native Village of Manley Hot Springs | 2009 | PS&E Development | 200,000.00 |
| Native Village of Kongiganak | 2009 | PS&E Development | 499,000.00 |
| | | | 4,449,791.00 |
| Native Village of Crooked Creek | 2010 | Emergency | 94,000.00 |
| Native Village of Nunam Iqua | 2010 | Construction | 1,000,000.00 |
| Nondalton Village | 2010 | PS&E Development | 75,000.00 |
| Qagan Toyagungin (Sand Point) | 2010 | Construction | 1,000,000.00 |
| Craig Community Association | 2010 | Construction | 1,000,000.00 |
| Naitve Village of Kqinhagak | 2010 | Construction | 750,000.00 |
| Kaktovik Village | 2010 | Construction | 1,000,000.00 |
| Native Village of Point Lay | 2010 | Construction | 1,000,000.00 |
| Native Village of Sammon Bay | 2010 | PS&E Development | 249,000.00 |
| Village of Alakanuk | 2010 | PS&E Development | 249,000.00 |
| Naitve Village of Gambell | 2010 | Construction | 1,000,000.00 |
| South Naknek Village | 2010 | Construction | 1,000,000.00 |
| Curyung Tribal Council | 2010 | Construction | 1,000,000.00 |

| | | | |
|--|------|---------------------|---------------|
| Chignik Lake Village | 2010 | PS&E Development | 242,000.00 |
| Village of Wainwright | 2010 | Construction | 1,000,000.00 |
| Igiugig Village | 2010 | Construction | 1,000,000.00 |
| Native Village of Eyak | 2010 | Construction | 1,000,000.00 |
| Native Village of Fort Yukon | 2010 | Construction | 1,000,000.00 |
| Nome Eskimo Community | 2010 | Construction | 805,000.00 |
| Native Village of Tetlin | 2010 | Construction | 1,000,000.00 |
| Oscarville Traditional Village | 2010 | PS&E Development | 499,000.00 |
| Native Village of Nunapitchuk | 2010 | PS&E Development | 125,000.00 |
| Native Village of Chenega Bay | 2010 | Construction | 1,000,000.00 |
| Native Village of Tatitlek | 2010 | Construction | 1,000,000.00 |
| Organized Village of Saxman | 2010 | PS&E Development | 249,000.00 |
| Native Village of Venetie Tribal Govt. | 2010 | Construction | 1,000,000.00 |
| Noorvik Native Community | 2010 | Construction | 1,000,000.00 |
| Ekwok Village | 2010 | PS&E Development | 688,619.00 |
| Asa'carsarmiut Tribe | 2010 | Construction | 350,000.00 |
| Seldovia Village Tribe | 2010 | PS&E Development | 241,200.00 |
| Native Village of Akutan | 2010 | PS&E Development | 1,000,000.00 |
| Organized Village of Grayling | 2010 | PS&E Development | 227,000.00 |
| Native Village of Ruby | 2010 | PS&E Development | 350,000.00 |
| | | | 23,193,819.00 |
| Native Village of Anaktuvuk Pass | 2011 | Bridge Construction | 1,000,000.00 |
| AVCP Pilot Station | 2011 | Construction | 1,000,000.00 |
| Native Village of Ambler | 2011 | Construction | 984,925.00 |
| Naitve Village of Akhiok | 2011 | Construction | 1,000,000.00 |
| St. George Island | 2011 | Construction | 1,000,000.00 |
| St. Paul Island | 2011 | Construction | 1,000,000.00 |
| Arctic Village | 2011 | PS&E Development | 430,000.00 |
| Pedro Bay Village | 2011 | Construction | 1,000,000.00 |
| Hughes Village | 2011 | Construction | 1,000,000.00 |
| Healy Lake Village | 2011 | Road Construction | 1,000,000.00 |
| Native Village Kobuk | 2011 | Construction | 1,000,000.00 |
| Seldovia Village Tribe | 2011 | Construction | 758,800.00 |
| Lime Village | 2011 | PS&E Development | 400,000.00 |

| | | | | |
|-----------------------------------|------|------------------|--------------|----------------------|
| Telida Village | 2011 | PS&E Development | 310,954.00 | |
| Igiugig Village | 2011 | Construction | 846,286.00 | |
| Sleetmute Traditional Council | 2011 | PS&E Development | 100,000.00 | |
| | | | | 12,830,965.00 |
| Akiak Native Community | 2012 | Construction | 1,000,000.00 | |
| Iqurmiut Traditional Council | 2012 | Construction | 1,000,000.00 | |
| Nondalton Village | 2012 | Construction | 486,800.00 | |
| Village of Chefnak | 2012 | Construction | 1,000,000.00 | |
| Qawalangin | 2012 | Construction | 1,000,000.00 | |
| Eek | 2012 | Construction | 501,000.00 | |
| New Stuyahok | 2012 | Construction | 1,000,000.00 | |
| Tuluksak Village | 2012 | PS&E Development | 275,000.00 | |
| Alakunuk | 2012 | Construction | 751,000.00 | |
| Karluk Tribal Council | 2012 | Construction | 1,000,000.00 | |
| Scammon Bay | 2012 | Construction | 751,000.00 | |
| Native Village of Perryville | 2012 | Construction | 1,000,000.00 | |
| Kwigillingok | 2012 | Construction | 158,500.00 | |
| Native Village of Nunapitchuk | 2012 | Construction | 665,000.00 | |
| Chevak Native Village | 2012 | Construction | 501,000.00 | |
| Levelock Village | 2012 | Construction | 1,000,000.00 | |
| Huslia | 2012 | Construction | 1,000,000.00 | |
| Native Village of Shungnak | 2012 | Construction | 976,883.00 | |
| Kawerak - St. Micheal Village | 2012 | Construction | 1,000,000.00 | |
| Crooked Creek Traditional Council | 2012 | Construction | 510,000.00 | |
| Organized Village of Saxman | 2012 | Construction | 499,000.00 | |
| Tanana Village | 2012 | PS&E Development | 363,000.00 | |
| Atqasuk Village | 2012 | PS&E Development | 425,000.00 | |
| Hoonah Indian Association | 2012 | Construction | 980,916.00 | 17,844,099.00 |
| Total of FY05-FY12: | | | | 95,735,067.00 |