

HB

163

(5)

Date Referred February 10, 1989

HOUSE REFERRALS: FINANCE

Date of Committee Action: 2/28/89

The TRANSPORTATION Committee considered:

HB 163

HOUSE BILL NO. 163 [APPROP: UPGRADE LOFTUS ROAD-FAIRBANKS]

"An Act making a special appropriation to the Department of Transportation and Public Facilities for improvements to Loftus Road in Fairbanks, and providing for an effective date."

RECOMMENDS:

- replacing with _____ the same title
- the attached amendment(s) a new title
- do pass
- do not pass
- no recommendation
- individual recommendations
- additional referral to the _____ Committee

ADOPTS: _____ letter of intent

ATTACHES NEW FISCAL NOTE(S):

- fiscal impact
- zero fiscal note
- zero with analysis

APPROVES PREVIOUS:

- fiscal note(s) published: _____
- zero fiscal notes(s) published: _____

SIGNING DO PASS:

Bette Cato
Brew A. Brown (w/ letter of intent)

SIGNING OTHER THAN DO PASS: (Do Not Pass, No Recommendation, Amend)

Richard Jones - No Rec.
Richard Jones - No Rec.

Bette Cato
Chairman's signature



Official Business

COMMITTEE:

House Transportation Committee

DATE: 2/28/89

SIGN-IN

Subject of meeting:

HJR 11
HB 163

NAME Please include title **ADDRESS** Please use full address. Please include zip. **PHONE** **REPRESENTING** **DO YOU WANT TO TESTIFY?**

NAME Please include title	ADDRESS Please use full address. Please include zip.	PHONE	REPRESENTING	DO YOU WANT TO TESTIFY?
✓ JEFF COTRAN DIRECTOR	PO Box 2 Juneau	907-2751	DOT/F	IF NECESSARY
MEREDITH HARRISON STUDENT	PO BOX 56092 North Pole	180 0625	Fairbanks Alternative	yes
Steven Torok ✓	3200 Hospital Dr. Juneau	586-7619	U.S. EPA	yes
LARRY DIETRICK ✓	PO Box 33013 Juneau AK	465-2640	ADEC	yes
P	✓	379-1	Har. M.	

HJR 11
HB 163
HJR 11

Alaska State Legislature



House of Representatives

Committee on Transportation

Rep. Bette Cato, Chairman

Pouch V
State Capitol
Juneau, Alaska 99811
(907) 465-4858

LETTER OF INTENT TO ACCOMPANY HB 163

It is the intent of the House Transportation Committee that the design services and contract administration services for the expansion of Loftus Road, as well as other projects, be provided by private engineering consultants to the greatest extent possible.

Alaska State Legislature
Representative Niilo Koponen

Pouch V
Juneau, Alaska 99811
(907) 465-4992

House District 21

119 N. Cushman, Suite 207
Fairbanks, Alaska 99701
(907) 456-8172

POSITION PAPER

HB 163

This bill would provide funding for the upgrade and widening of Loftus Road, a project made necessary by the opening next Fall of two new schools, the 600 pupil University Park Elementary School and Howard Luke Alternative Jr./Sr. High.

The greatest concern is for the children who live nearby, and who will walk or bicycle to and from school. There is now no sidewalk or bikepath to provide for the safety of pedestrians. There is, in fact, not even a shoulder. In addition, there are no streetlights along Loftus Road, a significant hazard in itself during the dark winter months.

At present, Loftus Road has two 10ft. wide lanes. It serves primarily to provide access to major arteries for the surrounding residential neighborhood. The two schools, however will become a destination attracting an estimated 1,000 additional vehicles per day, including school busses. This increase will approximately double the current use.

There is strong neighborhood support for the proposed upgrade, particularly among parents with children who will attend the new schools.

Design work has already been completed, having been funded in last year's reappropriation bill. Because of the immediate life safety issue, it is important that construction begin as soon as possible. The passage of HB 163 would provide for such timely action.

Department of Transportation & Public Facilities



POSITION PAPER

BILL NO: HB 163
A special appropriation to the
DOT&PF for improvements to
TITLE: Loftus Road in Fairbanks.

APPROVED: *WR Gerde*
DATE: February 27, 1989

The department supports this legislation.

Scope-of-Work: Loftus Road is to be upgraded between Geist Road and Birch Lake. Improvements will include construction of roadway illumination, shoulder widening on the east side of the road, and curb and sidewalk on the west side.

Engineer's Estimate: \$580,140

Comments: The Department of Transportation and Public Facilities (DOT&PF) currently has the maintenance responsibility for this section of Loftus Road. Two new schools (University Park Elementary and Howard Lake Alternative Jr./Sr. High School) are now under construction along Loftus Road. These schools are scheduled to be opened in the fall, 1989.

Design work is now being completed by DOT&PF and was funded under Chapter 10/88/12/15. Advertising for construction bids can begin in May, 1989 and construction completed by late summer, 1989.

FISCAL NOTE

Revision Date: 2/10/89
Title: An Act making a special appropriation to the Department of Transportation and Public Facilities for improvements to Loftus Road in Fairbanks; and providing for an effective date.

Agency Affected: DOT&PF
BRU:

Sponsor: Koponen, Boyer, M. Davis and Sharp
Requestor: House Transportation

Components:

EXPENDITURES/REVENUES: (THOUSANDS OF DOLLARS)

OPERATING	FY 88	FY 89	FY 90	FY 91	FY 92	FY 93
PERSONAL SERVICES	0	0	0	0	0	0
TRAVEL	0	0	0	0	0	0
CONTRACTURAL	0	0	0	0	0	0
SUPPLIES	0	0	0	0	0	0
EQUIPMENT	0	0	0	0	0	0
LAND & STRUCTURES	0	0	0	0	0	0
GRANTS, CLAIMS	0	0	0	0	0	0
MISCELLANEOUS	0	0	0	0	0	0
TOTAL OPERATING	0	0	3.5	3.5	3.5	3.5

CAPITAL	0	0	0	0	0	0
---------	---	---	---	---	---	---

REVENUE	0	0	0	0	0	0
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FUNDING: (THOUSANDS OF DOLLARS)

GENERAL FUND	0	0	3.5	3.5	3.5	3.5
FEDERAL FUNDS	0	0	0	0	0	0
OTHER*	0	0	0	0	0	0
TOTAL	0	0	3.5	3.5	3.5	3.5

POSITIONS:

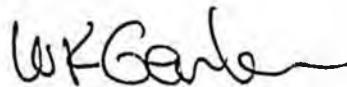
FULL-TIME	0	0	0	0	0	0
PART-TIME	0	0	0	0	0	0
TEMPORARY	0	0	0	0	0	0

ANALYSIS: (Attach a separate page if necessary)

Additional funds are needed because the roadway would be widened and illuminated.

Prepared by: Paul Prusak
Division: Planning

Phone: 474-2433
Date: 2/27/89



Approved by Commissioner: Mark S. Hickey
Agency: Department of Transportation and Public Facilities

Date: 2/27/89

Distribution (by preparer):
Legislative Finance
Legislative Sponsor
Requestor
Office of Management and Budget

7/12

★ Fairbanks North Star Borough

809 Pioneer Road

P.O. Box 1267

Fairbanks, Alaska 99707

907 452-4761

MEMORANDUM

TO: Current Planning
FROM: Todd Boyce, ⁸⁸Transportation Planner
DATE: February 16, 1988
SUBJECT: CU 014-88

Traffic Impact

The proposed elementary school would generate approximately 600 vehicle trips per day. The recently approved alternative school, which also has access onto Loftus Road will add about 400 trips. Staff estimates that about 80% of this traffic will access Loftus from Geist Road. When combined with the existing traffic on Loftus Road (1000) the traffic using Loftus Road between the site and Geist Road would be 1800 vehicles per day.

DOT&PF has indicated that this volume of traffic will not require the signalization of the Loftus Road/Geist Road intersection. If a left turn pocket is required on Geist Road in the future, DOT&PF has indicated that it could be provided on the road surface, by merely restriping.

Pedestrian/Bicycle Access

Geist Road has a bicycle path running along its south side. Loftus Road does not presently have a sidewalk or bikepath, but its south end has a connection to the Parks Highway bikepath.

It would be highly desirable if a pedestrian/bicycle connection could be made between these two existing bikepaths. A sidewalk/bikepath along Loftus Road would be necessary for elementary school age children to walk or bicycle to school.

If the approved funding for this project exceeds the contract amount for the school, a bicycle/pedestrian facility connecting the site to Geist Road should be funded from the remainder. If such funding is not available, FNSB should apply for LSR&T or other state or federal funds to construct this bicycle/pedestrian facility.

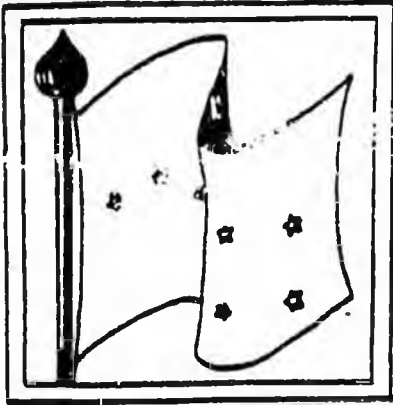
Current Planning
CU 014-88
Page 2

Access

Driveways into the school site should align with existing subdivision road and/or the driveway for the alternative school. This will be necessary to allow for construction of an adequate turning radius for school buses and minimize the hazard of vehicles falling into Deadman's Slough.

DOT&PF driveway permits will also be required for driveways onto Loftus Road.

TB/bhn



LOCATION / DESIGN STUDY REPORT

LOFTUS ROAD



STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
NORTHERN REGION

WINTER 1988-89

LOFTUS ROAD

Project 64308

Location/Design Study Report

WINTER 1988-89

Prepared by:

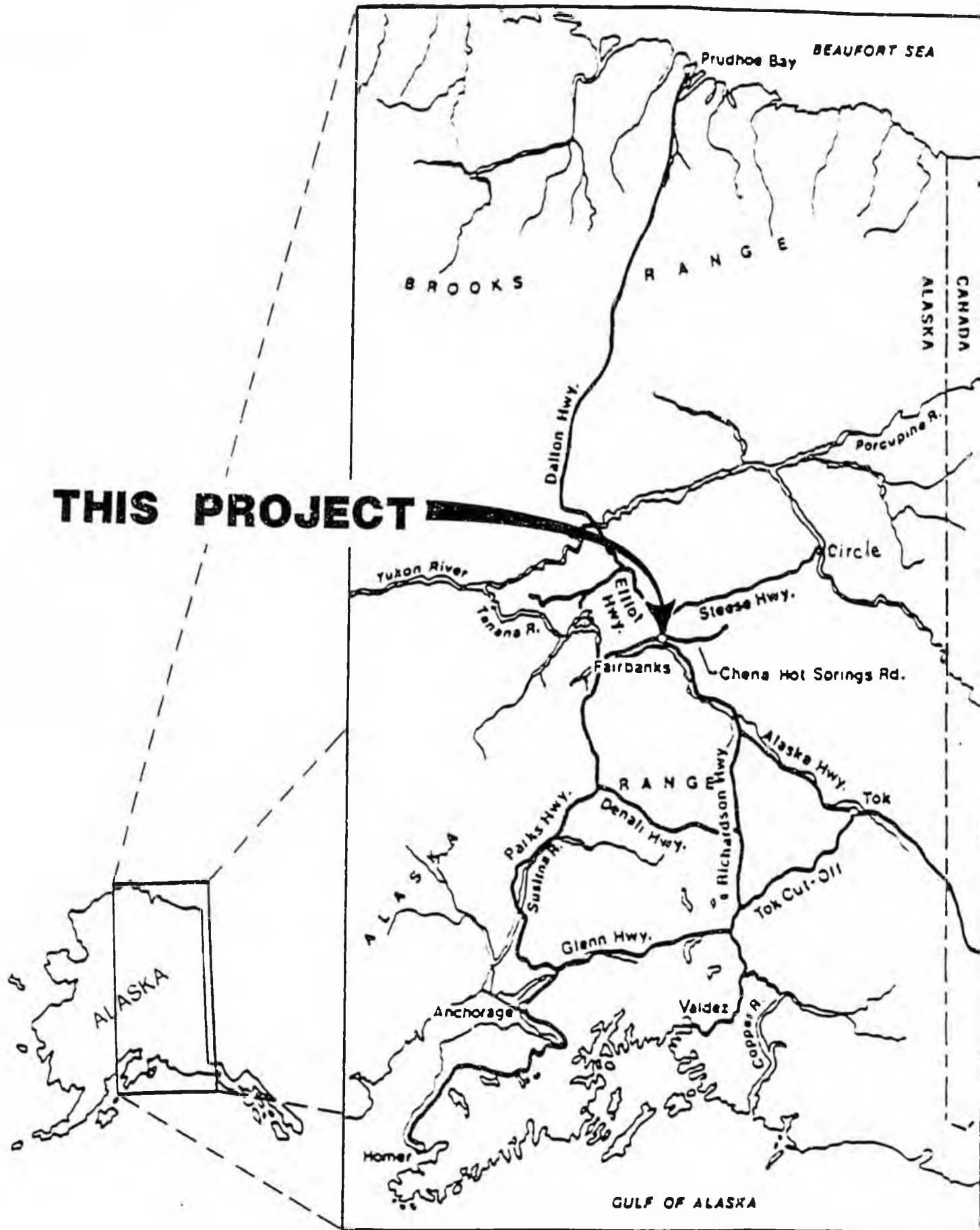
Donald E. Carlson, P.E.



STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
NORTHERN REGION
DESIGN SECTION

NOTICE

The Location/Design Study document reflects the Department's location and design decisions, as of the date of the Northern Region Design & Construction Director's approval to proceed with final plans. Changes frequently occur during the evolution of the design process, so persons who may rely on the information contained in this document should check with the Alaska Department of Transportation and Public Facilities for the most current design. Contact the Design Engineer at 451-2200 for this information.



VICINITY MAP

DESIGN AND LOCATION STUDY REPORT

PROJECT NO. 64308
PROJECT NAME: LOFTUS ROAD

Requested by: Joseph R. Keeney 1/17/89
Project Manager Date

Requested thru: Stephen C. Dink 1/17/89
Design Chief Date

Approved by: E. L. Engle 1-18-89
Director, Design & Construction Date

cc: Project Manager
Design Chief
Project Control Chief
Tech. Services Chief
Right of Way Chief
Utilities Section

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INTRODUCTION

Purpose

The State of Alaska, Department of Transportation and Public Facilities is planning to upgrade approximately .4 miles of Loftus Road from Birch Lane to Geist Road. The purpose of this report is to present an evaluation of the present and projected future usage of this route and to recommend a standard of improvement that will meet anticipated traffic demands. Some of the factors that will be considered in providing the recommendation include: safety, convenience to the traveling public, economic factors, environmental considerations, current AASHTO and State of Alaska highway design standards, and maintenance problems.

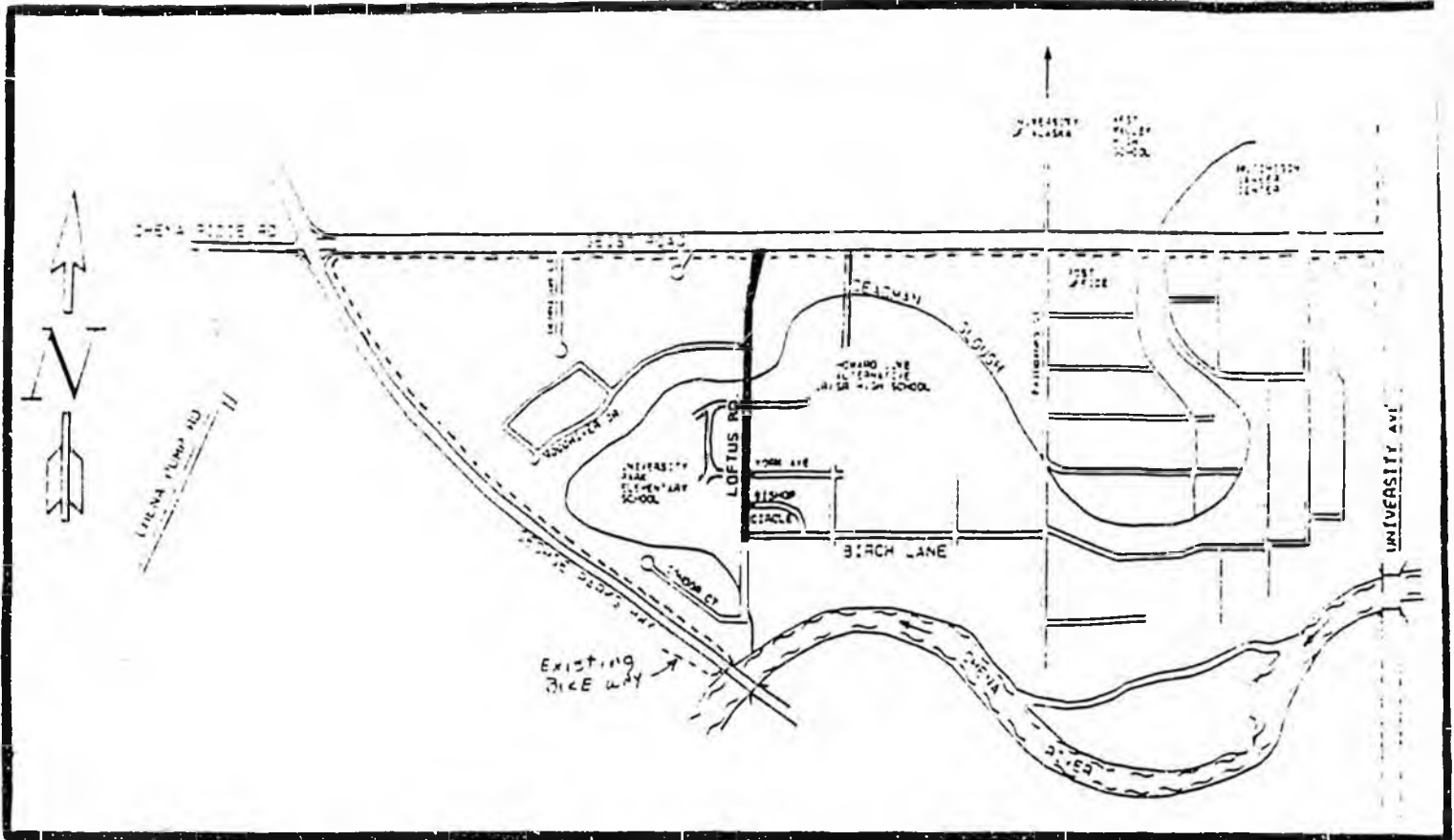
History

Loftus Road was originally an extension of Geist Road, possibly built to access Bunnel Park Subdivision in 1959. In 1970, it was officially renamed Loftus Road. In 1972, the State upgraded Loftus Road by ditching and applying an asphalt and chip surface treatment (A.S.T.). In 1975, the State rebuilt the approach to Loftus Road during the extension of Geist Road to the Parks Highway. In 1981, the State overlaid the existing A.S.T. with 1 1/2" of asphalt concrete pavement, retaining the existing two 10 ft. lane configuration.

Importance

Loftus Road is a two-lane, two-way minor collector with no on-street parking. Current lane widths are 10 feet with no shoulders and 2:1 sideslopes. The primary function of Loftus Road, at this time, is to access the local residential area. From 1964 to 1977 it also provided access to the College Utilities Sewage Treatment Plant, which is no longer in existence. Currently, the new University Park Elementary School and the Howard Luke Alternate Jr./Sr. High School are being built along Loftus Road. Due to the new schools and the residential growth in the area, vehicular and pedestrian traffic will continue to increase. The lack of shoulders for a place to walk or ride a bike along with the lack of illumination makes this route undesirable for pedestrian and bicycle traffic. The narrow lanes constrict vehicular traffic flow, too.

Safe easy access to the new schools and the surrounding area via Loftus Road can be improved by widening the traveled way, by constructing sidewalks and shoulders, and by installing street lights. The Loftus Road Project can provide these improvements with adequate funding. The only feasible alternative to upgrading Loftus Road is the "no build" option. The "no build" option is not recommended due to the new elementary school being built along this route.



LOFTUS ROAD
 Project No. 64308

LOCATION MAP

Public Involvement

The affected public will be involved in developing this project in the following ways: local government approval by the Borough Assembly, which may, at the request of the Planning Commission, include a public hearing; mailing specific impact statements to property owners within the project limits who may be affected by the proposed design.

ROADWAY REQUIREMENTS

Traffic

Traffic projections for the Loftus Road project were generated by DOT&PF Northern Region Planning Section. Projected volumes warrant two lanes with shoulders or curb and gutter. Signalization of the Geist Road intersection is not warranted at this time or within the next 10 years. The posted speed shall remain at 35 mph. An engineering study shall be made during the design phase to determine the need for and location of school speed limit signs and other school zone traffic controls.

A general traffic maintenance plan will be included in the contract plans to provide details on any special signing, detours and/or time constraints required during the construction phase.

Design Criteria

Based on the traffic projections for the year 2010 and current AASHTO standards, the following criteria will govern the design of the proposed project.

Design Speed = 40 mph
Maximum Curvature = 11.5°
Width of Travelled Way = 24 ft.

Detailed design criteria are shown in Appendix A.

Pavement Design

The pavement structure for Loftus Road has been analyzed to a depth of 42 inches using EAL's provided by DOT&PF Planning. Preliminary pavement design indicates 2 inches of asphalt concrete surfacing is appropriate. See Appendix B for the Typical Section. See Appendix C for pavement design charts.

Right-of-Way

Right-of-Way conflicts are not anticipated on this project. Construction permits may be needed for some approaches and in areas where construction is adjacent to the Right-of-Way.

Current private land uses will not be altered in the areas affected by this project.

Utilities

Involvement with existing underground and overhead utilities is not anticipated on this project.

Drainage

The curb and gutter will be drained by inlets with pipes draining into the ditch along the west side of Loftus Road. Existing ditch drainage into Deadman Slough will be retained and improved on both sides of Loftus Road. The Deadman Slough culvert will have to be extended on both ends.

Soils

Loftus Road soils were drilled and tested by the DOT&PF Northern Region Materials Section in October, 1988. The preliminary materials report indicates the existing embankment fill is contaminated with fine-grained soil (silt). The maintenance history and the existing good condition of the roadway surface indicates that the silt contamination has not been a deleterious factor. Subexcavation of silt in contact with the water table will be necessary from Deadman Slough thru the Woodriver Drive intersection. Reconstruction to the desired typical section should provide a firm surface with a design life of 20 years.

Borrow materials required for this project will be contractor supplied.

Illumination

The only street light on Loftus Road at this time is at the Geist Road intersection. The schools currently under construction will have street lights at or near the entrance approaches. Current estimates indicate that 300 elementary students will not be bused to school. The high volume of non-bused students will increase vehicular and pedestrian traffic during the morning and evening hours when it is dark or twilight. Illumination of this route is recommended to provide safe access to the schools and the surrounding neighborhood.

Preliminary design indicates 9 street lights will be required to provide the necessary illumination and they will be installed along the west side behind the new sidewalk.

Bicycle & Pedestrian Facilities

The 6 foot shoulder along the east side and the 9 foot sidewalk along the west side of Loftus Road will provide sufficient capacity for the expected bicycle and pedestrian volumes. Handicapped access ramps will be constructed at all curb returns in the sidewalk areas.

Safety

The new sidewalk, street lights, wider lanes and shoulder will improve the safety of this route.

Maintenance

Since the 1981 overlay, the only maintenance performed was to seal transverse thermal cracks in the pavement. Rutting and alligator cracking is not apparent within the project limits. Reconstruction to the desired typical section will reduce roadway surface maintenance costs during the 20 year design life. The wider design will increase snow removal costs and the street lights will cost about \$1500/year to maintain and operate.

Environmental Impacts

No significant adverse environmental impacts are anticipated as a result of this project. Approximately 2 acres of clearing and grubbing of brush within the right-of-way will be required on this project. Loftus Road is located in Flood Zone B, which is an area protected from a 100 year flood by dikes and levees. The area 100 feet either side of Deadman Slough is in Flood Zone A which can be affected by a 100 year flood.

The State of Alaska DOT&PF Northern Region Environmental Section has determined that the proposed project qualifies for the U.S. Army Corps of Engineers 404 Nationwide Permit. Additional required permits and clearances are a Fairbanks North Star Borough Floodplain Permit, a State Historic Preservation Office clearance and a Department of Fish and Game Title 16 permit. Concerns and requirements arising from these permits and clearances will be addressed during the design of this project.

Preliminary Construction Cost Estimate

The estimated construction cost for this project, based on preliminary design as discussed in this report is \$580,000. Unit costs are based on those recently experienced on similar projects in the Northern Region.

The work required for this project is expected to take less than one construction season to complete. See Appendix D for Pay Item descriptions and preliminary estimated unit costs. See Appendix E for an air photo layout.

APPENDIX A

ALASKA DOT & PF HIGHWAY PRECONSTRUCTION MANUAL
CHAPTER 11 - DESIGN

SEPTEMBER 1988

FIGURE 11-00(01)

PROJECT DESIGN CRITERIA

PROJECT NUMBER: 64308
PROJECT NAME: LOFTUS ROAD
FUNCTIONAL CLASS: MINOR URBAN COLLECTOR, LEVEL TERRAIN

DESIGN YEAR (Usually 5 yr. increment @ least
20 years after construction)... 2010
PRESENT ADT..... 1000
DESIGN YEAR ADT..... 2000
MID DESIGN PERIOD ADT..... N/A
DESIGN HOURLY VOLUME (DHV%)..... 10% (200)
DIRECTIONAL SPLIT (D)..... 45-55
TRUCKS (T%)..... 2%
EQUIVALENT AXLE LOADING (EAL)..... 33,600 (Total thru 2010)
PAVEMENT DESIGN YEAR (Construction Yr. + N): 2010

DESIGN VEHICLE (Usually AASHTO WB-50)..... BUS
DESIGN SPEED..... 40
STOPPING SIGHT DISTANCE..... 325'
PASSING SIGHT DISTANCE..... 1500'
MAXIMUM ALLOWABLE GRADE..... 7%
MINIMUM ALLOWABLE GRADE..... .3% (.5% Desirable)

MAX. ALLOWABLE DEGREE OF CURVATURE..... 11.25
MIN. K-VALUE FOR SAG CURVE..... 70
MIN. K-VALUE FOR CREST CURVE..... 80

NUMBER OF ROADWAYS..... 1
WIDTH OF TRAVELED WAY..... 24'
WIDTH OF RIGHT/OUTSIDE SHOULDER..... 6'
WIDTH OF LEFT/INSIDE SHOULDER..... 0'
TRAVELED WAY SURFACE TREATMENT..... 2" ASPHALT CONCRETE
SHOULDER SURFACE TREATMENT..... 2" ASPHALT CONCRETE
FORESLOPE RATIO: 4:1 (2:1 behind sidewalk)
BACKSLOPE RATIO: 4:1 (as R/W permits)

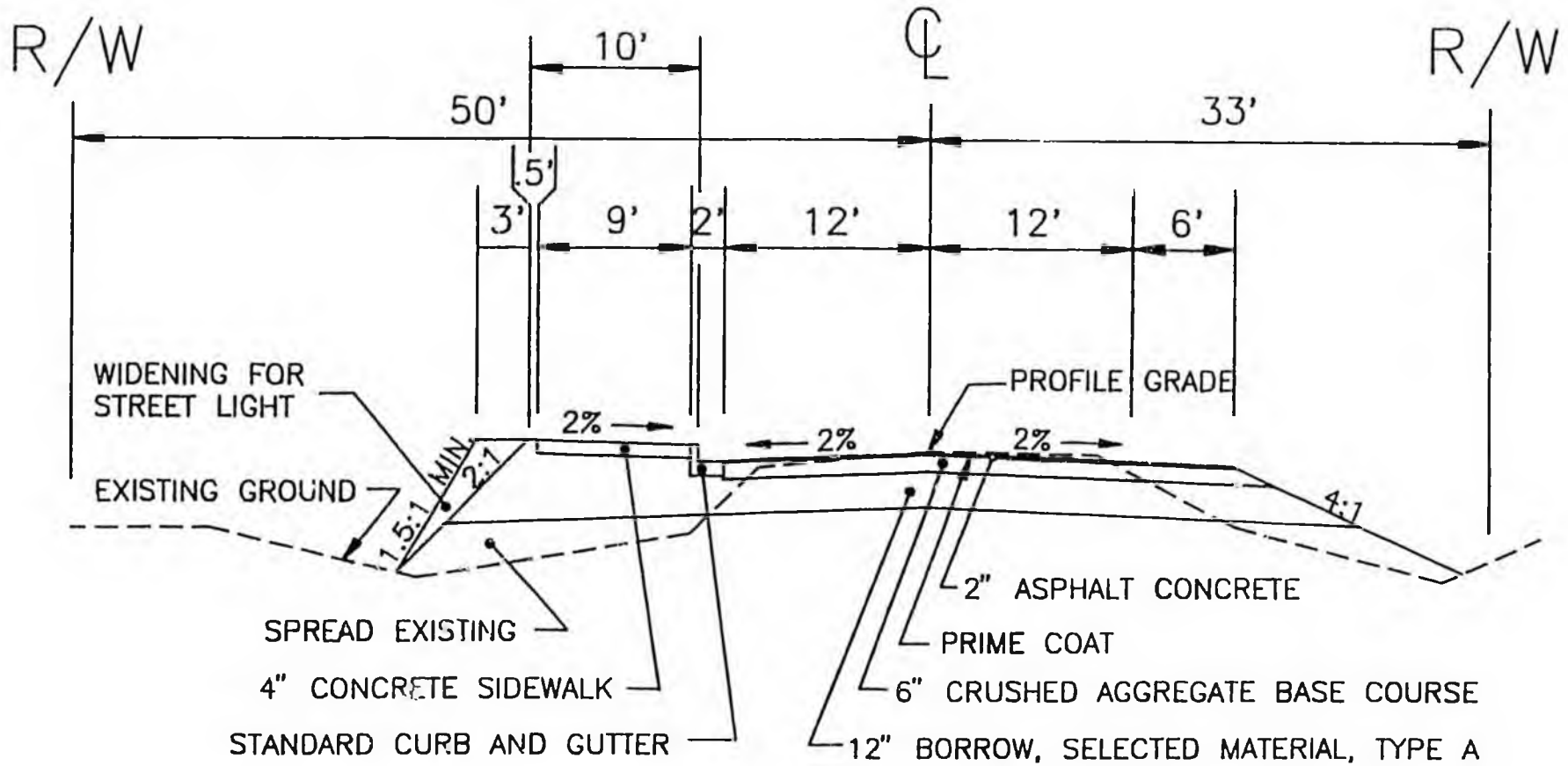
MEDIAN TREATMENT: N/A
ILLUMINATION: 9 NEW LUMINAIRES ON WEST SIDE BEHIND SIDEWALK.
CURB USAGE & TYPE: STANDARD CURB & GUTTER ALONG WEST SIDE.
BICYCLE PROVISIONS: 9' SIDEWALK LEFT & 6' SHOULDER RIGHT
PEDESTRIAN PROVISIONS: 9' SIDEWALK
MISC. CRITERIA: N/A

PROPOSED BY: [Signature] DATE: [Signature]
DESIGN PROJECT MANAGER

ACCEPTED BY: [Signature] DATE: 11/17/89
REGIONAL DESIGN CHIEF

APPENDIX B

LOFTUS ROAD - 64308



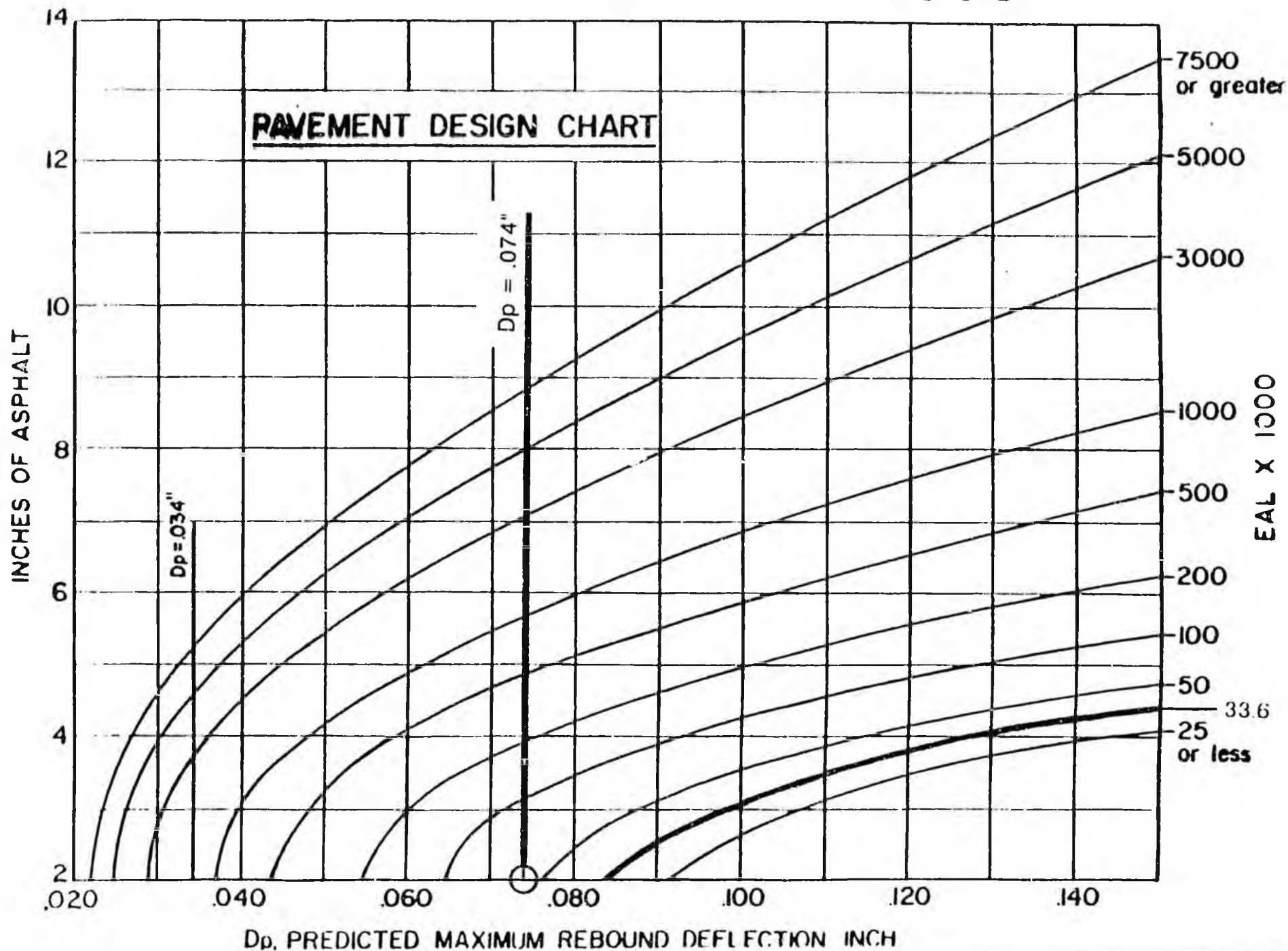
TYPICAL SECTION

APPENDIX B

APPENDIX C

LOFTUS ROAD - 64308

APPENDIX C



LOFTUS ROAD - 64308

Column	①	②	③	④	⑤	⑥	⑦	⑧
Obtained from:	Triad Dimensions	Specifications or Field Data	Fig 3	② - ③	Fig 4	Fig 4	⑥ - ⑤	⑦ × ④ ^{0.8}
Layer Number	Depth Interval (in.)	Fines Content (P ₂₀₀)	Maximum Fines (P _{cr})	Excess Fines	SRF @ Top of Layer	SRF @ Bottom of Layer	(ΔSRF)	(EFF)
1	0-6	6.0	6.0	0.0	.00	.44	.44	0.0
2	6-12	6.0	7.0	0.0	.44	.68	.24	0.0
3	12-18	6.0	9.4	0.0	.68	.81	.13	0.0
4	18-24	80.0	10.5	69.5	.81	.87	.06	1.79
5	24-30	80.0	13.4	66.6	.87	.92	.05	1.44
6	30-42	80.0	27.9	52.1	.92	1.00	.08	1.89

APPENDIX C

Calculations: Predicted Deflection:

If $EFF_1 = 0$; $D_p = 0.34''$
 If $EFF_1 > 0$; $D_p = 0.056'' + 0.0035(EFF_1)$

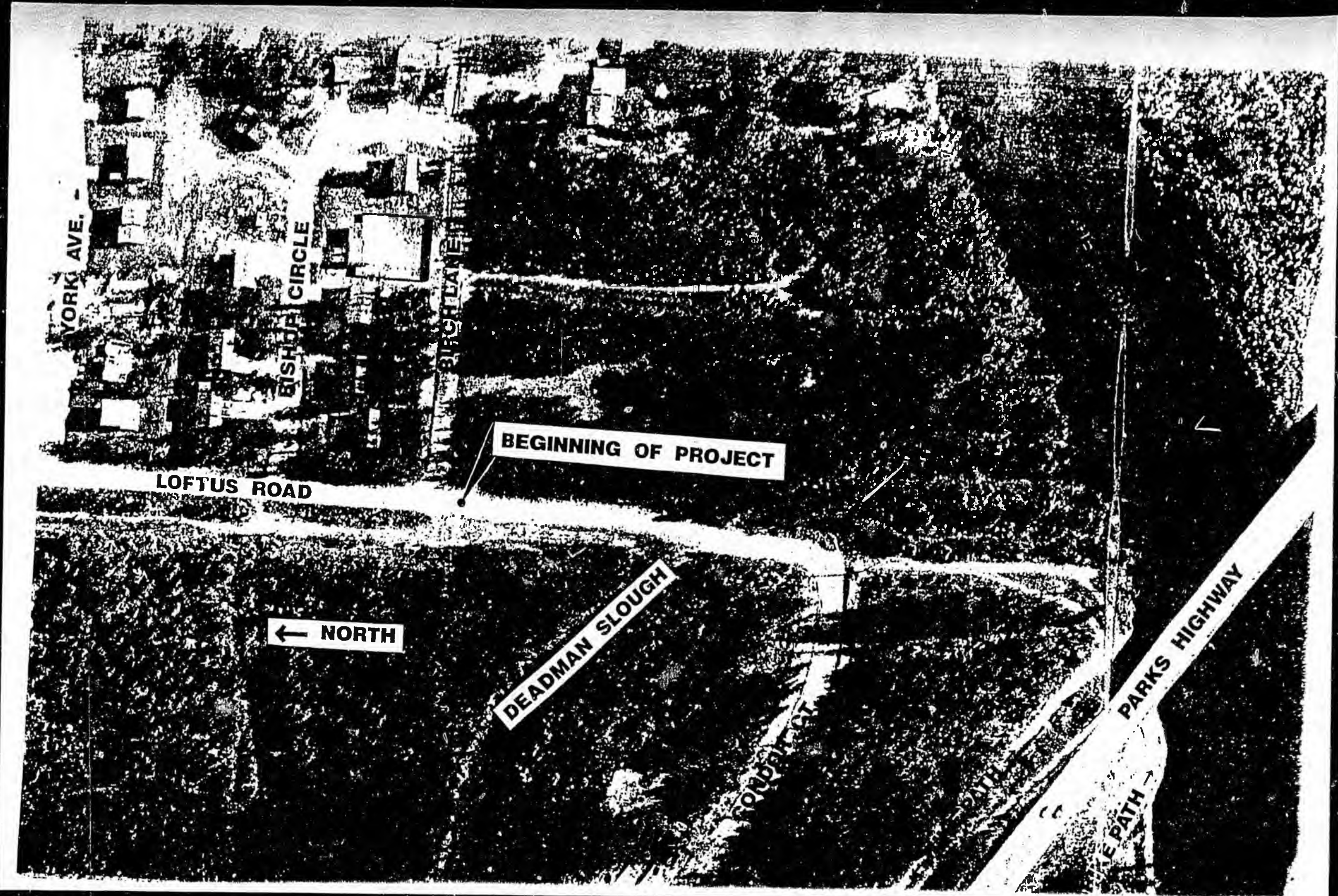
$D_p = 0.074''$

$EFF_1 = \sum EFF =$
 Column 8
 Total = 5.11

Pavement thickness from Fig 6 (Enter with D_p on Horizontal Axis, rise vertically to curve for EAL and then horizontally to read pavement thickness on left axis) = 2.0 inches

APPENDIX D

APPENDIX E



YORK AVE.

BISHOP CIRCLE

BIRCHLANE

BEGINNING OF PROJECT

LOFTUS ROAD

← NORTH

DEADMAN SLOUGH

TRAIL PATH

PARKS HIGHWAY

TRAIL PATH →

GEIST ROAD

BIKE PATH

HOWARD LUKE ALTERNATIVE
JR./SR. HIGH SCHOOL

DEADMAN SLOUGH

END OF PROJECT

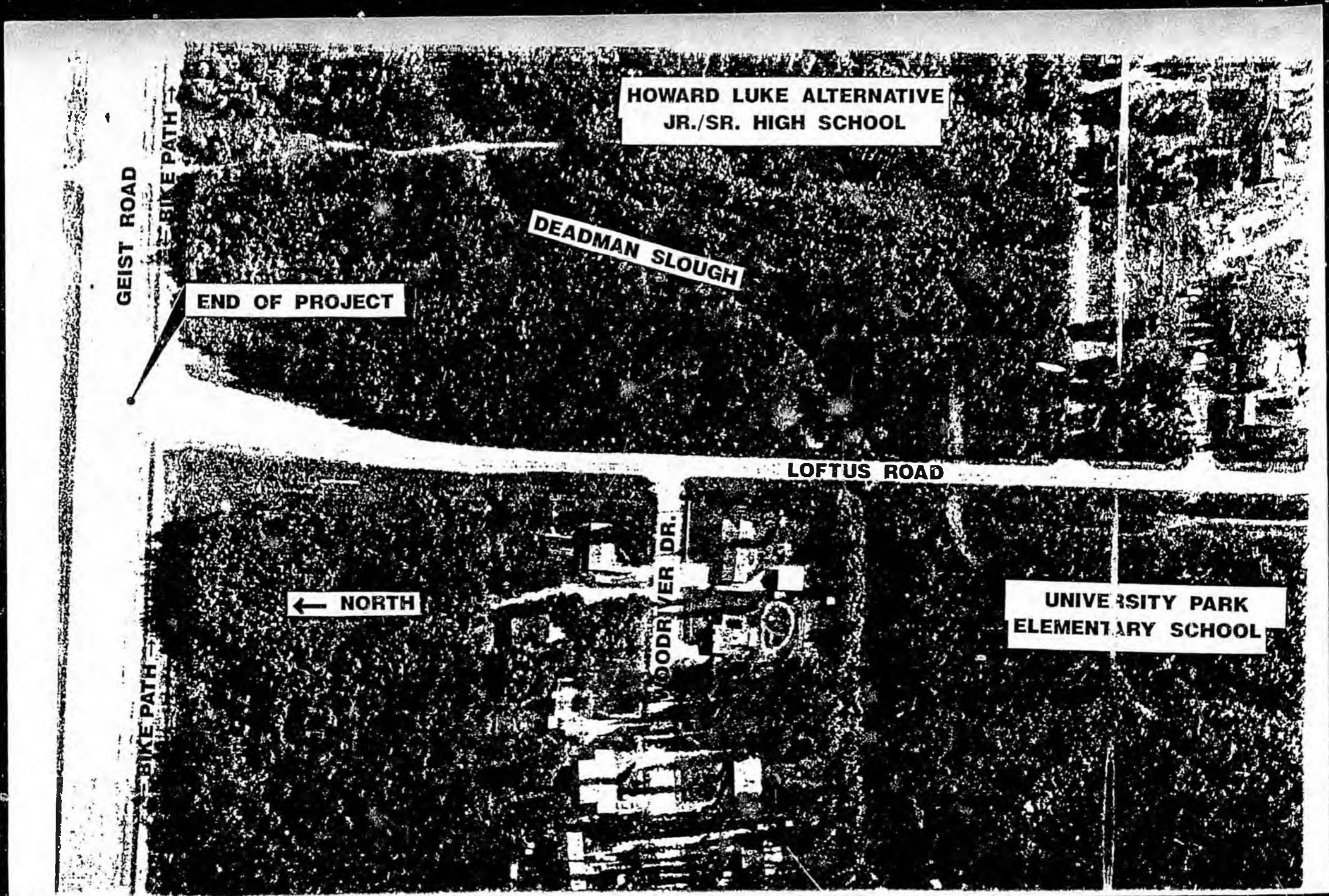
LOFTUS ROAD

← NORTH

WOODRIVER DR.

UNIVERSITY PARK
ELEMENTARY SCHOOL

BIKE PATH



ALASKA STATUTES

Title 37 Public Finance

OCTOBER 1988

Chapter 25. Miscellaneous Provisions.

Section

10. Unexpended balances of one-year appropriations

Section

20. Unexpended balances of appropriation for capital projects

Opinions of attorney general. — The provisions of AS 37.25.010 and 37.25.020 regarding unexpended balances of appropriations are applicable to the University of Alaska. February 28, 1977 Op. Att'y Gen.

There is no constitutional obstacle to making the University of Alaska subject to the provisions contained in this title. February 28, 1977 Op. Att'y Gen.

Sec. 37.25.010. Unexpended balances of one-year appropriations. (a) The unexpended balance of a one-year appropriation authorized in an appropriation bill lapses on June 30 of the fiscal year for which appropriated. However, a valid obligation (encumbrance) existing on June 30 is automatically reappropriated for the fiscal year beginning on the succeeding July 1 if it is recorded with the Department of Administration by August 31 of the succeeding fiscal year.

(b) An indebtedness arising from a prior year for which the appropriation has lapsed shall be paid from the current year's appropriations, if (1) this expenditure does not exceed the balance lapsed; and (2) the original obligation date is not more than two years from the requested date of disbursement.

(c) University receipts received on or before June 30 of a fiscal year in excess of the amount expended for that year may be expended in the succeeding fiscal year if an appropriation of university receipts has been made for the succeeding fiscal year. The amount of university receipts expended in a fiscal year may not exceed the amount of university receipts appropriated for that year.

(d) The University of Alaska shall report the amount of university receipts received in one year and expended in the succeeding fiscal year to the Department of Administration and the Legislative Budget and Audit Committee by September 30 of the succeeding fiscal year.

(e) In this section, "university receipts" has the meaning given in AS 14.40.491. (§ 1 ch 113 SLA 1962; am § 8 ch 143 SLA 1986)

Effect of amendments. — The 1986 amendment added subsections (c) — (e).

Sec. 37.25.020. Unexpended balances of appropriation for capital projects. An appropriation made for a capital project is valid for the life of the project and the unexpended balance shall be carried forward to subsequent fiscal years. Between July 1 and August 31 of each fiscal year, a statement supporting the amount of the unex-

pending balance required to complete the projects for which the initial appropriation was made and the amount that may be lapsed shall be recorded with the Department of Administration. (§ 2 ch 113 SLA 1962)

Opinions of attorney general. — Appropriations that are available for expenditure in a fiscal year should be counted only against the appropriation limit of § 16, art. IX, of the state constitution, for the first fiscal year during which they could be completely expended, even if an unexpended balance is carried forward into the next fiscal year. 1983 Op. Att'y Gen. No. 01.

Sec. 37.25.030. Appropriations for projects of the Alaska energy center. [Repealed, § 65 ch 14 SLA 1987.]

Sec. 37.25.040. [Renumbered as AS 37.05.530.]

Chapter 30. Local Government Bonding.

Section

100. Prohibited bidding on bonds

Secs. 37.30.010 — 37.30.090. Anticipatory borrowing. [Repealed, § 1 ch 118 SLA 1972.]

Sec. 37.30.100. Prohibited bidding on bonds. (a) A person who provides financial programming or marketing assistance to a political subdivision of the state, whether home rule or otherwise, in connection with the issuance or sale of general obligation bonds, revenue bonds, or bond anticipation notes of the political subdivision may not bid on the bonds or notes if offered at public sale, or negotiate for their purchase if sold at private sale.

(b) The sale of general obligation, revenue bonds, or bond anticipation notes of a political subdivision to a person prohibited from bidding on, or negotiating for the sale of bonds or notes under (a) of this section is against public policy and the sale is void.

(c) In this section "person" means an individual, firm, agent, factor, intermediary, partnership, corporation, association, bond house, stockbroker, or bond broker. (§ 2 ch 102 SLA 1974)