

HB

169

STATE OF ALASKA
PRELIMINARY STATEMENT OF FISCAL IMPACT

Bill No: HB 169 Date on Bill: 2/7/83
 Title: Approp. to DOTPF..allocations..local service roads and trails.
 Sponsor: Herrmann, Koponen, Zharoff, Cato and McBride
 Requestor: House Transportation Committee

1. Estimated fiscal impacts on:

a. Expenditures:

(Thousands of Dollars)

	FY 83	FY 84	FY 85	FY 86
Capital	10,000.0			
Operating				
Total	10,000.0			

b. Revenues:

Revenue	FY 83	FY 84	FY 85	FY 86

2. Source of funds to offset fiscal impact of bill:

Sponsor did not identify source.

3. Assumptions:

4. Disclaimer:

This statement has not been reviewed by the OMB in the Office of the Governor. It not represent the policy of the Sheffield Administration or the final estimate of fiscal impact.

Prepared By: Wayne Weeks Phone: 465-4060
 Division: Planning & Programming Date: 2/14/83

Approved by Commissioner: [Signature] Date: 2/19/83
 Department: Transportation and Public Facilities

5. Distribution:

- Original to Legislative Finance
- Copy to OMB
- Copy to Sponsor
- Copy to Requestor

2/8/83

GSSB 438 (L&C), (cont'd)

the board to fill out the term to which the member was appointed, regardless of new sections added by this bill. Provides Act takes effect immediately.

Sections of the Senate-passed version are taken out by House L&C, and they include 1) an amendment to current law that would have allowed for one public member on the board; 2) a registrant requesting renewal of a certificate to give evidence of the registrant's continued competence as a professional architect, engineer, or land surveyor; and 3) section allowing existing board members to serve out their term, regardless of appointment of a public member.

Appropriation
(local service
rds. & trls.)

HOUSE BILL NO. 169, (see pages 150;189, 1983 report). Reported back to the House April 6 by Finance recommending it be replaced with a Finance substitute and as follows: Bettisworth (V-Chair), Grussendorf, Zharoff, Hurlbert, Ward and Fritz recommend it do pass. Martin, Furnace and Duncan had no recommendation. A letter of intent was attached. To Rules. The letter states:

"It is the intent of the House Finance Committee that the Local Service Roads and Trails (LSR&T), program not be discontinued as recommended by the previous Commissioner of the Department of Transportation and Public Facilities, (DOT/PF).

The LSR&T program is vital to local governments and provides very important benefits to communities in the rural and urban areas of Alaska.

The House Finance Committee intends that the Department of Transportation and Public Facilities support continued operation of this vital program and request continued funding for its operations in subsequent fiscal years."

The Finance substitute appropriates \$6.5 million to the Dept. of Transportation and Public Facilities for allocations for local service roads and trails. Provides Act takes effect immediately.

History: Introduced 2/7/83 by Reps. Herrmann, Koponen, Zharoff, Cato and McBride, and appropriated \$10 million to the Dept. of Transportation and Public Facilities for the local service roads and trails program (identical to SB 7). Reported out of Transportation 2/14 with a do pass recommendation and a letter of intent:

It is the intent of the House Transportation Committee that the Local Service Roads and Trails (LSR&T), program not be discontinued as recommended by the previous Commissioner of the Department of Transportation and Public Facilities, (DOT/PF).

The LSR&T program is vital to local governments and provides very important benefits to communities in the rural and urban areas of Alaska.

The House Transportation Committee intends the DOT/PF support continued operation of this vital program and request continued funding for its operations in subsequent fiscal years.

TESTIMONY OF
SENATOR FRANK R. FERGUSON
ON SENATE BILL 7 BEFORE THE
SENATE TRANSPORTATION COMMITTEE

SENATE BILL 7 PROVIDES FOR A SUPPLEMENTAL APPROPRIATION TO THE DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES FOR \$10 MILLION FOR THE LOCAL SERVICE ROADS AND TRAILS (LSR&T) PROGRAM.

THE LSR & T PROGRAM IS A PROGRAM OF VITAL IMPORTANCE TO BOTH URBAN AND RURAL COMMUNITIES. THE PROGRAM OFFERS FUNDING FOR CONSTRUCTION OF LOCAL ROADS AND TRAILS THAT ARE NOT ELIGIBLE TO RECEIVE FEDERAL HIGHWAY FUNDING.

LAST YEAR, THE HAMMOND ADMINISTRATION BEGAN A PHASE-OUT OF THE LSR & T PROGRAM. THE DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES HAS STATED PUBLICLY THE REASONS FOR THE LSR & T PHASE-OUT IS DUE TO THE MUNICIPAL GRANT PROGRAM AND LINE ITEM APPROPRIATIONS. THIS EXPLANATION IS LACKING IN SUBSTANCE.

FIRST, LINE ITEM APPROPRIATIONS AND MUNICIPAL GRANTS ARE, IN

MOST CASES, THE SAME THING. WHEN MAKING A LINE ITEM APPROPRIATION TO A COMMUNITY, THE LEGISLATURE DESIGNATES IT AS A MUNICIPAL GRANT. THE MUNICIPAL GRANT PROGRAM IS NOT A PROGRAM FUNDED BY THE LEGISLATURE BUT RATHER A PROCEDURE ALLOWING THE STATE TO CONTRACT WITH LOCAL COMMUNITIES.

SECOND, APPROPRIATIONS TO LOCAL COMMUNITIES BY THE LEGISLATURE HAVE NEVER BEEN SUFFICIENT TO MEET THE EXISTING TRANSPORTATION NEEDS. THE LSR & T PROGRAM HAS ALWAYS BEEN LOOKED UPON BY THE LEGISLATURE AS A WAY TO FUND PROJECTS THE ADMINISTRATION AND LEGISLATURE WERE NOT ABLE TO CONSIDER DUE TO TIME CONSTRAINTS BUT WERE IN NEED OF CONSTRUCTION OR REPAIR IMMEDIATELY.

THE \$10 MILLION SUPPLEMENTAL WILL ALLOW THE DEPARTMENT TO ENTER INTO AGREEMENTS WITH LOCAL COMMUNITIES FOR PROJECTS THAT HAVE BEEN IDENTIFIED AS IN NEED OF FUNDING. BECAUSE OF THE PHASE-OUT, THE DEPARTMENT HAS BEEN TRYING TO CLOSE OUT PROJECTS AND NOT ENTERING INTO CONTRACTS FOR PROJECTS THEY KNOW NEED FUNDING. THE MEMORANDUM IN YOUR PACKETS FROM

SENATOR MOSS INDICATES THAT APPROXIMATELY \$2,818,000 IS
AVAILABLE FOR FUNDING THROUGH THE END OF THE FISCAL YEAR.
ACTUALLY, THE \$2,818,000 IS COMMITTED BUT NOT UNDER AGREE-
MENT.

IN THE PAST, DOT HAS RECEIVED \$10 MILLION DOLLARS TO COVER A
TWO-YEAR PERIOD FOR THE LSR & T PROGRAM. SINCE THE LSR & T
PROGRAM DID NOT RECEIVE FUNDING THIS FISCAL YEAR, THE \$10
MILLION WILL ALLOW THE DEPARTMENT TO ENTER INTO AGREEMENTS
FOR NEW PROJECTS THIS CONSTRUCTION SEASON, AND CONTINUE
OPERATIONS INTO THE NEXT FISCAL YEAR.

Alaska State Legislature

House of Representatives



Rep. Bette Cato, Chairman

Committee on Transportation

Pouch V
State Capitol
Juneau, Alaska 99811
(907) 465-4858

DATE: 27 MARCH

TO: AL ADAMS, CHAIRMAN *BC*
HOUSE FINANCE COMMITTEE

FROM: Bette Cato, Chairman
House Transportation Committee

RE: HB 169 - appropriations bill for Local Service Roads and Trails

Per our phone conversation this morning, the House Transportation Committee requests that House Bill 169 be moved out of the House Finance Committee with a reduced appropriation of \$6.5 million.

Please find enclosed back-up material addressing HB 169 and recent correspondence from the Department of Transportation regarding LSR&T allocations for FY 1985.

Thanks so much for your time and assistance.

Enclosure

Alaska State Legislature

House of Representatives

Committee on Transportation



Rep. Bette Cato, Chairman

Pouch V
State Capitol
Juneau, Alaska 99811
(907) 465-4858

LETTER OF INTENT TO ACCOMPANY HOUSE BILL 169

It is the intent of the House Transportation Committee that the Local Service Roads and Trails (LSR&T), program not be discontinued as recommended by the previous Commissioner of the Department of Transportation and Public Facilities, (DOT/PF).

The LSR&T program is vital to local governments and provides very important benefits to communities in the rural and urban areas of Alaska.

The House Transportation Committee intends the DOT/PF support continued operation of this vital program and request continued funding for it's operations in subsequent fiscal years.

MEMORANDUM

State of Alaska Department of Transportation & Public Facilities

TO: C.D. Karella
State LSR&T Engineer
Juneau

DATE: November 15, 1982

FILE NO:

TELEPHONE NO:

FROM: D.S. Ronken *DR*
LSR&T Engineer
Fairbanks

SUBJECT: LSR&T Program

In response to our telephone conversation of 11/5/82, I have informed J. Chandler at Nome to supply you with the information you requested regarding the LSR&T Program in the bush communities.

I have been in contact with the local governments and the comments received from them regarding the LSR&T Program are as follows:

Since local roads and streets are always in need of improvement the local governments would like all the funds they can possibly obtain for roadway construction.

The local governments without road powers feel that the DOT/PF has too much control over the funds. They object to some of the conditions stated in Section "C" of the project agreement.

The local governments however, do like the flexibility of the Program, i.e.:

- 1) Various types of construction that can be funded
- 2) Project funding can be changed to cover overrun, providing funds remain in their allocations
- 3) LSR&T Construction Program submitted (priority list) can be changed, if for some reason the local needs change

Some local governments in the Fairbanks area feel that the manner in which funds are allocated based on population by the latest federal census should be changed. With the population in the Fairbanks area increasing every year and the allocation based on the latest census of 1980 which has been questioned by the local government, they feel the present system leaves much to be desired.

In summary, the local governments like the program and would like to see it refunded. The funds obtained through the LSR&T Program are not sufficient to cover the road improvement needs, but all funds that can be obtained to improve the local transportation system will be accepted by the local governments providing the conditions for its acceptance do not change.

MEMORANDUM

State of Alaska
Department of Transportation & Public Facilities

TO: John J. Simpson, Director
Standards and Technical Services
Division

DATE: March 14, 1984

FILE NO: 2519

TELEPHONE NO: 789-6237

FROM: Charles D. Karella
Statewide LSR&T Engineer *CDK*
DC & M Standards

SUBJECT: LSR&T Allocation
F.Y. 1985

The LSR&T Program was allocated \$7 million to re-establish the program. I was assured by the LBRC last spring, that this funding reflected on on-going program and that future funding would not be a problem.

During the House Transportation Committee hearing on SB 332, March 7, 1984, I suggested that it was not necessary to seek other sources of funding beyond the CIP, such as, SB 332. It would be simpler to manage the program at a solid level of funding. I told the Committee that the Governor's office assured me that they would not cut the LSR&T Program from the CIP budget and the Department would continue to have an adequately funded program in the future.

Since we are involved with all local governments and the legislature, I believe in all fairness, that the \$6 million annual funding level is bare bones and to drop below that will only jeopardize the integrity of the program but increase the cost of management per project constructed.

I feel if the DOT&PF does not want to chance the loss of the little rapport it now has with the legislature, we should leave the five million intact or even increase it.

I believe we will not only lose face with the legislature but also with the local governments who have been going in circles since January 1982 because of the following:

1. We began to phase out the program.
2. Then we established the LSR&T Program as a viable continuous program at a funding level of \$7 million per year.
3. Then we reduced the funds to bare bones level of \$6 million per year.
4. Now we have reduced the funding an additional 30%.

The attached comments from the Regions reflect an additional \$6 million that could be used in unorganized boroughs alone. A like amount could easily be utilized for additional local government projects which were requested, but were beyond our funding level of \$6 million per year.

If requested during the hearing tomorrow on HB 558, I will give the Committee a copy of this memo. (With your concurrence of course).

Attachment

mdh

MEMORANDUM

State of Alaska Department of Transportation & Public Facilities

TO: C. D. Karella
Statewide LSR&T Engineer

DATE: March 13, 1984

FILE NO:

TELEPHONE NO: 364-4222

OT
FROM: Ole Bartness
LSR&T Manager
Southeast Region

SUBJECT: Funding LSR&T Projects
FY 85 and Future

In regard to your question regarding the effect on the Southeast Region LSR&T Program with funding in the amount of 3.5 million dollars statewide, I offer the following:

Historically, the Southeast Region has received approximately 10 percent of the statewide appropriation. Based on that percentage, this would amount to \$350,000 for this region. Of this amount, approximately \$150,000 would be for the unorganized borough and could fund three to six projects, depending upon how tight we pull the drawstrings.

At the present time, there is an unencumbered balance of \$100,000 remaining for the unorganized borough in Southeast, and there would have been a zero balance long ago if the home rule cities were still included in the unorganized borough.

In what remains in the unorganized borough of the Southeast Region, some immediate future project priorities have been identified as follows:

<u>Location</u>	<u>Description</u>	<u>Estimated Cost</u>
Metlakatla	Roads & Parking	140,000
Elfin Cove	Boardwalk Repair	50,000
Kupreanof	Trail & Boardwalk	30,000
Angoon	Road to Emergency Helipad	90,000
Clark Bay	Road or Boardwalk	Unknown
Port Protection	Boardwalk/Trail	Unknown
Hyder	Roads	30,000
Edna Bay	Trails/Boardwalks	30,000
Tenakee	Trails, Bridges	Unknown
Hollis	Road Renovation	Unknown

Although there are some unknowns, it would not be out of line to estimate the above projects at a total cost of \$500,000.

Communities we have never heard from before are now contacting us for assistance. This appears to be a result of the State Land Lotteries which designated road and trail easements, but no access or minimal access was provided. In any event, these communities have needs, but we could furnish very little support with a \$3.5 million LSR&T appropriation.

^B:hn

MEMORANDUM

State of Alaska

TO: Charles Karella
Design Construction &
Maintenance
Standards - Juneau

DATE: March 13, 1984
FILE NO:
TELEPHONE NO:

FROM:

Robert L. Haneline *RH*
Central Regional
LSR&T Engineer

SUBJECT:

Project Funding
Requests, LSR&T
Program

This memo will iterate information on project requests presently in abeyance, awaiting a source of funding approval which was relayed to your office by telephone last week.

UNORGANIZED BOROUGH IV (South)

1. Chignik Bay - \$600,000

Construction of 4,500 l.f. of 14 foot-wide gravel roadway and a single lane, single span, 50 foot-long vehicle bridge. Provide access to proposed new HUD houses. Road to serve dual purpose as a protective dike (seawall).

2. Egegik - \$60,000

Construction of 600 l.f. of 16 foot-wide gravel roadway and approximately 3,000 l.f. of four foot-wide gravel path. Provide roadway access to existing HUD houses and replacement of existing boardwalks with a gravel pathway.

3. Eek - \$410,000

Construction of 2,700 l.f. of 14 foot-wide gravel roadway between the village proper and the airplane landing strip and 2,700 l.f. of eight foot-wide boardwalk needed to provide access between the village and the cemetery.

4. Manokotak - \$3,000,000

Construction of eleven miles of 14 foot-wide gravel roadway. The road is needed to provide City access to a proposed new garbage dump site, HUD housing site and the proposed Snake River Port Access Site. Funding needs are beyond the scope of the LSR&T program. This is a community planned development with a high community priority.

UNORGANIZED BOROUGH I

1. Whittier - \$1,000,000

Construction of 1.4 miles of 26' to 36' wide paved streets, including 5,250 l.f. of sidewalk, 10,000 l.f. of curb and gutter and miscellaneous storm drains (\$700,000). The project also is to include the paving of the boat harbor parking area (\$300,000).

This office has also received verbal requests for LSR&T funding involvement from the communities of Mekoryuk and St. Paul. The information received is sketchy, at best. We lack the information needed to properly determine the project scope, cost, or feasibility; only know that the monies needed are anticipated to exceed \$1,000,000 per project.

For what it's worth:

A common sense approach to the functionability of any State funded, intelligently planned program requires that the certainty of future funding availability be known, as the ordinary business of any program centers around a fixed budget. Provisions for limited program adjustments to the seasonal ebb and flow of available funds exists to a small degree in the form of project deletion or construction delay.

To promulgate a five-year LSR&T construction program based on the anticipated availability of six million dollars per year and then to cut back the amount of funds available to the extent being proposed is controversial to say the least.

If nothing else, we have a main purpose at this time of convincing the money appropriating bodies of the importance of maintaining the State's credibility.

MEMORANDUM

State of Alaska
Department of Transportation & Public Facilities

TO: Charles D. Karella
State LSR&T Engineer
Standards & Technical Services
Juneau

DATE: March 8, 1984

FILE NO:

TELEPHONE NO: 443-5266

FROM: Jeffrey S. Chandler *JS*
LSR&T Manager
Northern Region, Western District
Nome

SUBJECT: LSR&T Funding Allocations,
Unorganized Borough/Western

Per your request, here is a brief description of our fiscal outlook for the LSR&T Program in the Western District unorganized borough.

Generally, under present law, the proposed funding level of \$6.0 million for the Statewide annual LSR&T program should be adequate to meet the Western District's 5 Year Plan for the unorganized borough. The major exception is the need for a one time infusion of \$500,000 which would be used as follows :

St. Michael Local Roads	300,000
Western LSR&T Equipment	200,000
Total	<u>\$500,000</u>

The St. Michael project is presently programmed for FY86, however there will be insufficient funding to plan and complete the entire project as requested unless LSR&T or the village can provide the additional funding by 1985.

The Western District LSR&T equipment fleets were substantially reduced during the period we were in phase-out status. We presently have one small fleet in reasonably good shape capable of handling typical LSR&T local hire projects, but one old bulldozer and two old trucks are in need of replacement. This would assure that the fleet will remain operationally effective for the next several years.

Upgrading the existing fleet would result in a level of reliability that would allow us to split it between two projects in seasons when we are administering supplemental grant projects in villages which have a limited assortment of equipment on site. This approach reduces mobilization costs and improves the effective utilization of locally available municipal equipment.

In the event that the present proposed funding level were further increased either Statewide or for the unorganized borough, then we would advance the schedule of projects in our 5 Year Plan accordingly.

Introduced: 2/7/83
Referred: Transportation
and Finance

Funding Information
General Fund \$10,000,000
Other Funds -0-
\$10,000,000

BY HERRMANN, KOPONEN, ZHAROFF,
CATO AND MCBRIDE

1 IN THE HOUSE

2

HOUSE BILL NO. 169

3

IN THE LEGISLATURE OF THE STATE OF ALASKA

4

THIRTEENTH LEGISLATURE - FIRST SESSION

5

A BILL

6

For an Act entitled: "An Act making an appropriation to the Department of

7

Transportation and Public Facilities for allocations

8

for local service roads and trails; and providing for

9

an effective date."

10 BE IT ENACTED BY THE LEGISLATURE OF THE STATE OF ALASKA:

11 * Section 1. The sum of \$10,000,000 is appropriated from the general

12 fund to the Department of Transportation and Public Facilities for

13 allocations for local service roads and trails authorized by AS 19.30.111 -

14 19.30.251.

15 * Sec. 2. This Act takes effect immediately in accordance with AS 01.-

16 10.070(c).

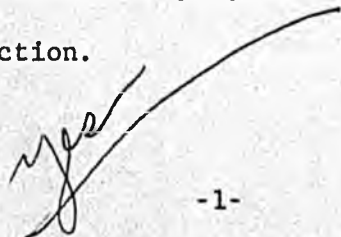
Bette:

This bill is identical to Sen. Ferguson's bill. As this is an appropriation bill, it is not possible to amend the Statute (AS 19.30.111) in it.

The transportation Committee may wish to offer a committee bill after hearing this bill to change the formula to allow a greater proportion of L.S.R. & T. funds to go to the rural areas, (non-Anchorage-Fairbanks-Juneau) The problem would be in gathering the support necessary for passage. Mike Scott in Senator Ferguson's office thought that it would be more possible to accomplish this originating in the House, specifically, the transportation committee. So.. would you like to pursue this or bring it up in committee and discuss possibilities with DOT? Personally, that would be my suggestion. This way the committee would hear the discussion and possibly support such a change in the L.S.R.&T. program.

I await further direction.

Steve



~~HB 169~~ TITLE & SPONSOR SUMMARY

15:43 7/27/83 PAGE 1 OF 2

AMENDED TITLE:

AN ACT MAKING AN APPROPRIATION TO THE DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES FOR ALLOCATIONS FOR
LOCAL SERVICE ROADS AND TRAILS; AND PROVIDING FOR AN EFFECTIVE
DATE

GENERAL DOLLARS: \$10,000,000 (APPROX)

PRIME SPONSOR: HERRMANN.

OTHER DOLLARS: \$0

~~CO-SPONSORS~~ KOPONEN, ZHAROFF, ~~SMITH~~, MCBRIDE.

CURRENT STATUS: 2/14/83 IN (H) FINANCE

HB 169 HOUSE ACTION

15:44 7/27/83 PAGE 2 OF 2

DATE SEQ PAGE

LEGISLATIVE ACTION

02/07/83	01	0193	FIRST READING -- COMMITTEE REPORTS
02/14/83	02	0244	TRAN -- DP07, NR02
02/14/83	03	0244	TRANSP CMTE LETTER OF INTENT
02.14/83	04	0244	F/NOTE HSE SUPPL #8
			FINANCE
			RULES

SB 7 ↔ HB 169
SENATE TRANSPORTATION COMMITTEE, THURS., 2-10-83

MEMBERS PRESENT: SEN. MOSS, KERTULLA, GILLMAN,
FAHRENKAMP, FAIKS, HALFORD

SEN. FERGUSON, ^{PRIME} SPONSOR — FORMAL TESTIMONY

SEN. KERTULLA - BRIEF HISTORY - EARLY 70's a logical system developed as a response to Farm to Market Road program - system based on geography + population; funding goes directly into area for this program
problem comes with maintenance funds
w/ upgrading LSRT should continue based on local input

Gene Wright - Bike Path ^{along side} Richardson Hwy - ^{missing section} in middle Historical Great trail to Cantequina Canyon Support bill

JOHN BATES

Gilman: Reluctance from DOT to take over additional roads for maintenance?

Bates - From start tried to get local gov't to maintain program

Gilman: Some local gov't. haven't the authority to maintain. Example: 2nd Class borough

If bill passes, if borough can't get agreement w/DOT to bring roads into system, can it ~~maintain~~

utilize funds to bring into secondary maintenance system

Roads identified by local govt - reach project agreement -
bring into State maintenance

No Rec. - GILMAN

Do Pass - FAHRENKAMP
FAIRS

LSR&T PROGRAMFACT SHEETFUNDING

FIRST ALLOCATION	1971	\$ 6 MILLION
LAST ALLOCATION	1980	10 MILLION
TOTAL ALLOCATION		50.8 MILLION

PROJECTS (DOLLARS)

COMPLETED OR UNDER AGREEMENT	\$ 50 MILLION
REIMBURSEMENTS REMAINING	8 MILLION

ANNUAL PROGRAM

150 ACTIVE PROJECTS

50 ROAD PROJECTS COMPLETED

50 MILES OF ROAD COMPLETED

2000 MILES OF WINTER TRAIL STAKING

SEVERAL MILES OF BOARDWALK,
FOOTTRAILS & BIKE PATHS,

PROGRAM PHASE OUT

Phase out of the LSR&T Program began Jan 5, 1982

EQUIPMENT FLEETS

Upgrade of the two construction fleets is necessary to retain the programs mobility in remote areas.

The LSR&T Program, initiated in 1971, has been used to fund a variety of projects in almost every community in the Panhandle. Cost has ranged from a drainage project in Haines, costing \$1,300, to major street construction in Ketchikan, costing \$245,000.

Types of projects constructed include footbridges in Kasaan; housing access roads in Yakutat, Hydaburg and Klawock; trails in Metlakatla, Hoonah, Tenakee, Kasaan and Elfin Cove; road construction and reconstruction in just about every town; sidewalks, parking areas, boardwalks, drainage projects, paving, signing, stairways, and a boat launching ramp.

The LSR&T program has been flexible enough to answer the needs of the people in regard to getting them out of the mud, which was the original concept of LSR&T. Costs were kept down and the dollar stretched to the extent possible by using local labor, material and equipment whenever possible. Money was brought into the economy of the towns, jobs were created where none existed, and the people involved developed a sense of pride in their accomplishments.

Some of the projects constructed which have meant a great deal to the communities and the traveling public are listed below:

1. Kasaan Bridges and Boardwalk. Reconstructed to provide access from the harbor to town. Existing structures were completely rotten and the community had no funds.

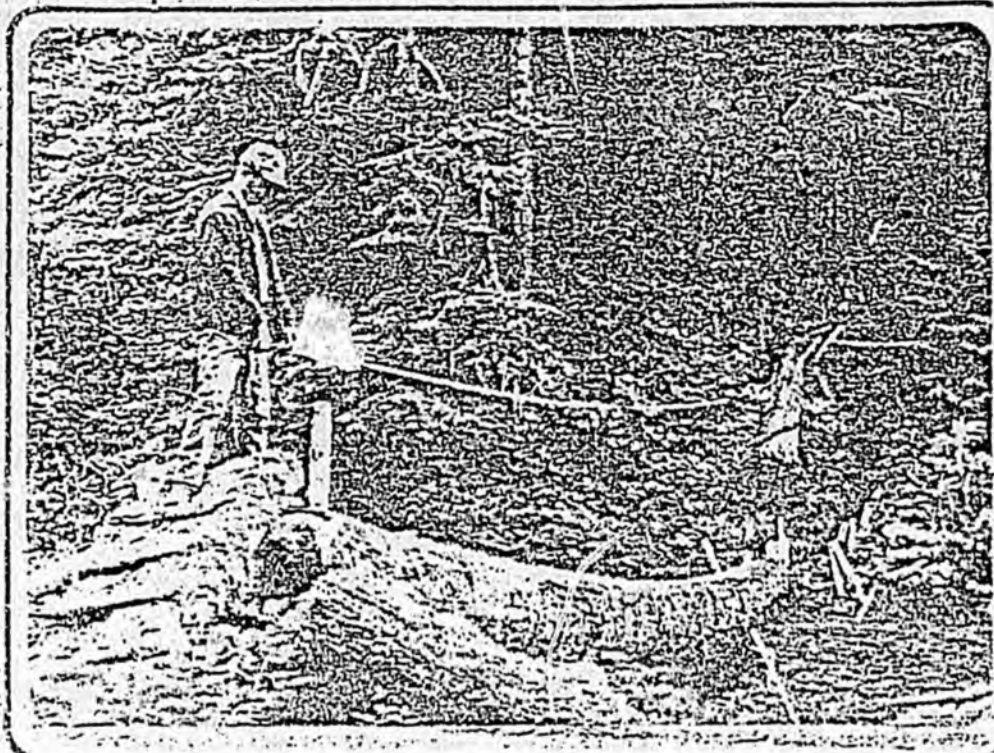
2. Pelican Landfill Access. A road constructed by the city to meet EPA requirements.
3. Kake Totem Park. A road and parking area to the site of the world's tallest totem pole.
4. Hydaburg Townsite Road. A new road to provide access to a new municipal building and housing for senior citizens.
5. Elfin Cove Boardwalk and Trail. Replacement and reconstruction of the boardwalk and trail around the cove, including material for future maintenance.
6. Port Alexander. A boardwalk to replace a washed out trail connecting the inner and outer harbors.
7. Craig Cold Storage Road. A 30' roadway to provide access to a new city shop and site of proposed highway maintenance shop.
8. Haines Trail. A year-round trail for hiking, skiing, horseback riding, snowshoeing, etc.
9. Wrangell - Outer Drive. Construction of roadway embankment, drainage and sidewalks which access new municipal building to downtown.

10. Petersburg. Replaced Birch Street Trestle along Hammar Slough, and constructed several new access roads.

11. Juneau. Sidewalks, drainage, paving of local streets, including Evergreen Bowl Access, Trinity Drive, Memorial Drive and Riverside Drive.

These are only a few of the projects constructed under the program in the past ten years. As can be expected, each project is unique and in the villages sometimes innovation is the key to success.

Attached are excerpts from the April 1976 issue of The New Alaskan which had several interesting LSR&T projects featured.

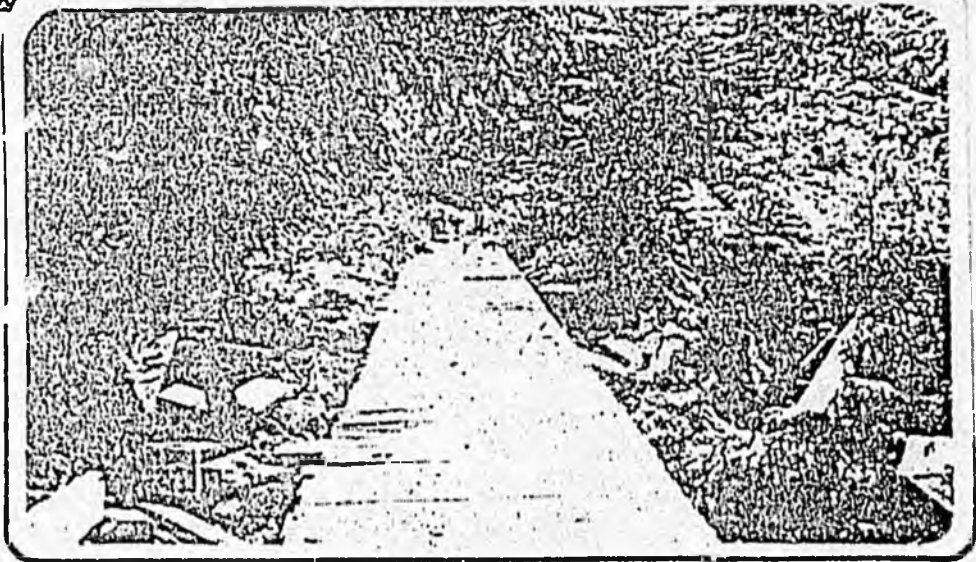


TENAKEE-INDIAN RIVER BRIDGE

The log spanning the Indian River, about one mile east of Tenakee collapsed in the spring of 1975. This crossing provided access to hunting and fishing areas for residents and outdoorsmen from southeast, and also access for home owners at Sunny Cove, Columbia, Superior, Harley Creek and Coffee Cove. Old bridge was placed by Loyal H. Clark in the spring of 1957. The tree came from the east side of Kadashan Bay, about 1/2 mile back from the beach in an area Bill Tonsgard logged. He used a TD-14 International Tractor to yard the log out and towed it across the inlet. The job was done by Mr. Clark alone, and took about 10 days. He rafted the tractor across the inlet and back, which he says was very time consuming.

The Department agreed to assist Tenakee in replacing this bridge, and arrangements were made to begin work directly after the Thanksgiving weekend last fall. Supplies were delivered to the site by Channel Flying of Juneau on November 28, 1975. The following day the temperature dropped sharply and it began to snow. It was decided to delay the project until the weather improved. Plans were made recently to begin work March 1, but once again the weather didn't cooperate.

The replacement bridge will be placed several hundred feet downstream and will consist of 2 logs placed side by side, cabled together and planked to provide a walking surface. The east Tenakee trail will also be improved at the same time. The forman for construction of the bridge will be Thomas Jack, of Hoonah, who resided in Tenakee previously. Don McGee will be forman for the remaining work. All work will be done using local labor. Mayor of Tenakee, Bob Pegues, initiated the project.



Kasaan, Boardwalk

The purpose of this project was to connect the Village of Kasaan to the boat harbor and airplane float, a distance of approximately 1,000 feet. The existing boardwalk was completely rotten, and injuries had been caused due to people falling through the walk.

It was agreed that the lumber would be purchased from Mr. B.G. Morrison who has a small sawmill on the beach at Kasaan, and that the labor would be performed by local residents.

Work was started August 5th and proceeded by fits and starts until November, when a lack of material halted the project. A lack of lumber halted the project several times, as the mill had a habit of eating up its engines and Mr. Morrison was kept busy looking for replacements. The project lacks about 300 feet of being completed. The remaining material is being acquired from Ketchikan, and the project should be completed soon.

A unique aspect of this project is that the village had no monies available, and the project could not be set up on a reimbursement basis. A bank account was established for the project by the Department so weekly payroll could be handled, and materials purchased. The checks require the signature of a village representative and the Local Service Roads and Trails Engineer. This arrangement has been used several times with satisfactory results.

Hoonah — Spasski Trail

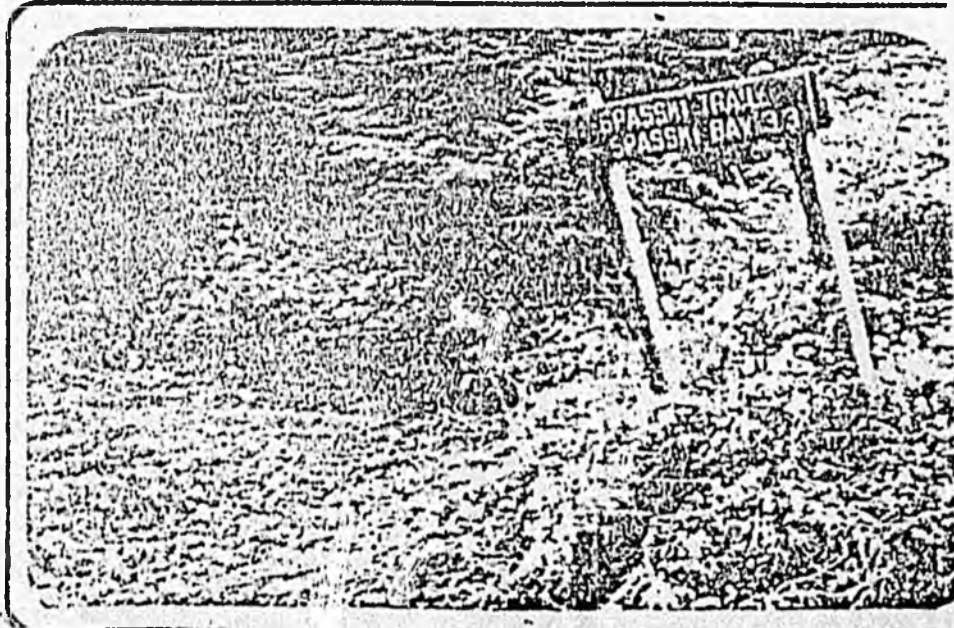
The Spasski Trail was originally upgraded by the Forest Service in 1938 and 1939, and probably maintained by the Forest Service until some time in the 1960's. The trail connects Hoonah to Spasski Bay a distance of approximately 3½ miles. The trail was heavily overgrown and several gullies and muskeg areas required crossings. The trail was usable, but just barely.

It was agreed to upgrade the trail to make it usable for recreational purposes such as hiking, hunting, snowshoeing, etc. Work was begun on September 8, 1975, with Thomas Jack as foreman. The size of the crew was usually about eight men, all local.

One of the cost items in the project was rifle shells, as the brown bear in the area also liked to use the trail. Dogs were usually kept with the crews to help keep the bears away.

The trail was completed to Spasski Bay the first week in October.

Also, as part of the project, it was agreed to construct a stairway down "Jackass Pass" which is a portion of undeveloped roadwork between Hill Street and Hemlock Avenue. This is a very steep area which was utilized by a number of people, including older residents, as a shortcut to town. According to Mayor Miles Murphy, it wasn't really any shorter, but people seemed to use it anyway. The same crew did the work on this project and it was completed in early November.



UNFOUNDED SOURCE HOONAH BULKHEAD
Project G-30182

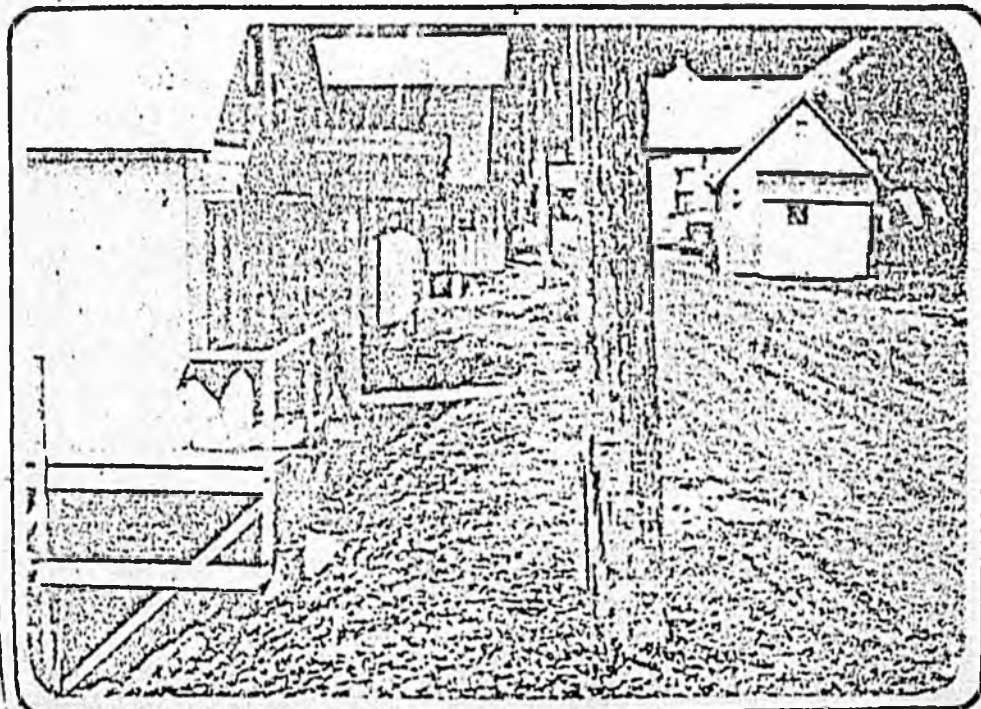
The purpose of this project was twofold. First to reinforce the log bulkhead along Front Street in Hoonah, and secondly to afford a measure of shore protection in areas where there was no bulkhead.

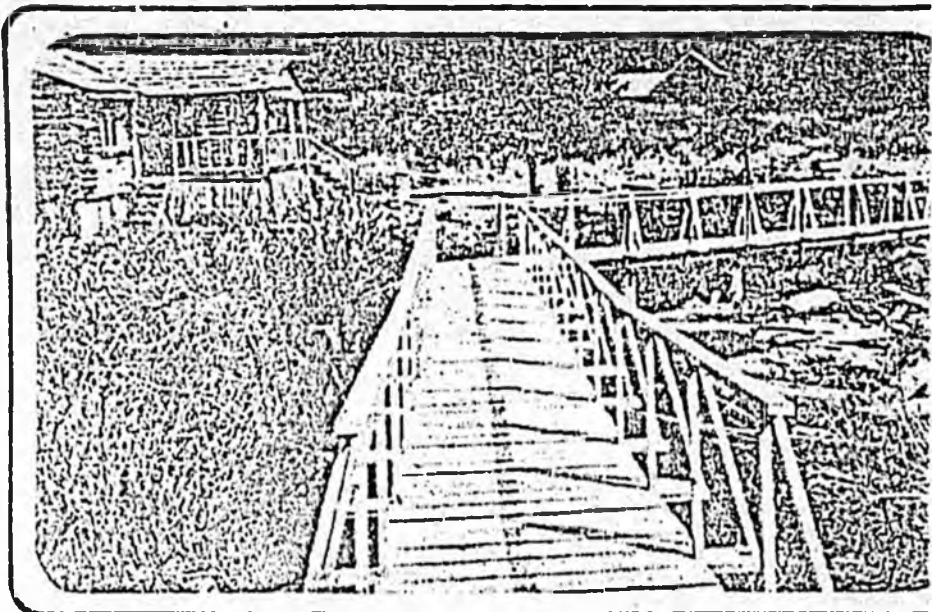
The project was initially designed and advertised by the Department of Highways, but the bids received were considerably over the engineers estimate, and were rejected. It was subsequently agreed between the Department and the City of Hoonah that the City would provide the labor and equipment on a reimbursable basis, and highways would supervise the work.

The work force consisted of Millard Cartee, Charles Bennett and Frank Wright, St. The LSR&T Superintendent was Roger Polley.

Work started in November, 1972, and consisted of building demolition and carpentry work until winter shutdown. Work commenced on the project in April, under supervision of Kelly St. Clair, of Hoonah. He was appointed by Mayor Frank See and approved and hired by the Department.

Rock was obtained from the Bureau of Indian Affairs quarry and the project was completed on May 22, 1973.





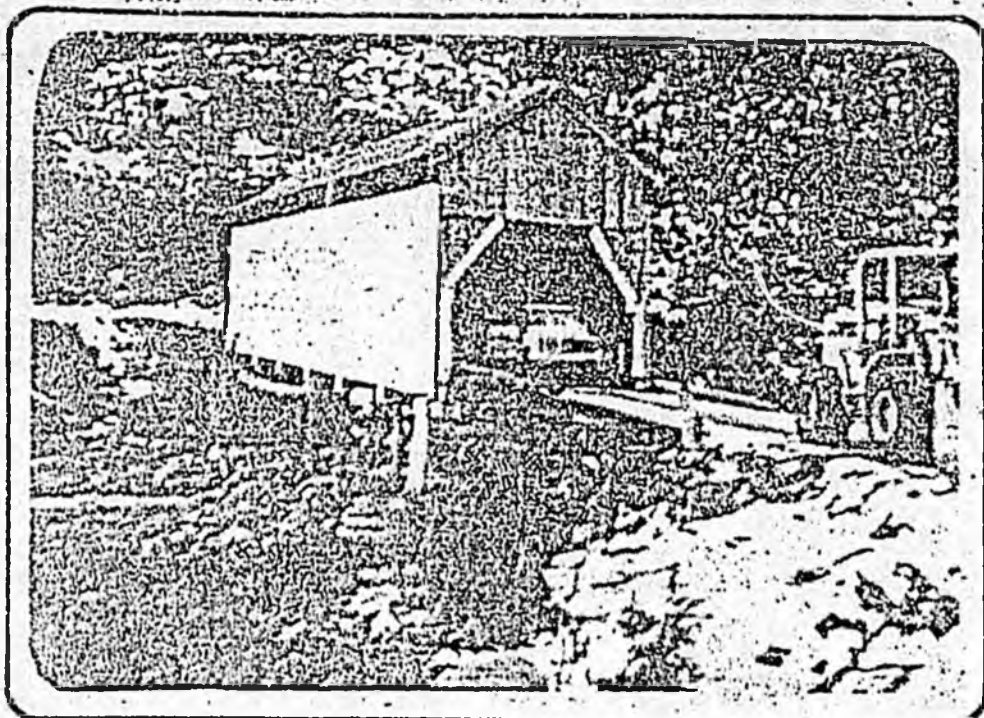
KASAAN VILLIAGE-BRIDGE RECONSTRUCTION

The objective of this project was to reconstruct the Tinkum Creek Bridge, and to redeck the Linkum Creek Bridge. The Tinkum Creek Bridge was constructed under L.S.R. & T. Project C-30019, and was completed on the 29th of September at a cost of \$2,237.23.

The above referenced bridge was destroyed on January 19, 1954 when attacked by drifting logs carried by storm sea waves generated by 70 mile per hour winds during an 18.5 foot high tide.

A field inspection indicated that the bridge would require relocation above the tidal area, to prevent a reoccurring damage. This was approved by the presents of log strewn beaches adjacent to the bridge site. Most of the existing bridge lumber could be salvaged and reused in the new structure. The new bridge was relocated 45 feet up stream and paralleling the preceding site.

The grant of right of way easement and petition was signed on the 23rd of February. Construction began on March 25, with the arrival of Dale Henkins, project supervisor, in Kasaan. Alton McAllister, Dale Peele and Dale Wallace were hired and work began on the project. The lumber was picked up and delivered to Ketchikan Transportation Company, by Ketchikan maintenance forces. The lumber was inadvertently loaded and shipped to Klawock by the Ketchikan Transportation Company. Klawock Maintenance Foreman, Louis Seltzer transported the lumber to Hollis, placing the lumber on the beach. Dale Henkins chartered the seine boat, Glenda Joe, to pick up the lumber in Hollis and delivered it to the Kasaan project. The project was completed on the 1st of April at a cost of \$4,820.00.



HYDER — TEXAS CREEK BRIDGE.

The Texas Creek Bridge, located 7½ miles north of Hyder, is said to be the last remaining covered bridge in Alaska, and is sought by tourists utilizing the Stewart, B.C. road connection.

The bridge was constructed by the Territorial Road Commission in 1928, and provides the only access to the west bank of the creek.

The winters of 1970 and 1971 badly damaged the main support members of the bridge, and without repairs it was doubtful the bridge could hold up under heavy wind and snow loads of another winter.

At a public meeting held in Hyder in September of 1972, it was agreed to make repairs to the bridge utilizing LSR&T funds. The various companies in Hyder having suitable equipment to perform the work were contacted and requested to furnish rental prices on equipment. Mr. Glen Wiikes of G.W. Logging submitted acceptable prices and was awarded the work.

Temporary repairs took eight days to accomplish. The repairs consisted of placing a 60 foot stringer under the damaged chord and installing three bents. These were reinforced with large boulders and river run from the creek, which also cleared the channel area to allow freer flow. Four sets of cross braces were installed inside the bridge, completing the job.

According to a report from Lawrence Marx, Highway Maintenance Foreman in Hyder, this strengthened the bridge to a point where traffic could utilize the bridge once again.

Alaska State Legislature

SENATOR
H. PAPPY MOSS
P.O. BOX 182
DELTA JUNCTION, ALASKA 99737
9071895-4384



WHILE IN JUNEAU
POUCH V
JUNEAU, ALASKA
99811
(907)465-4921

State Senate

MEMORANDUM

Date: February 9, 1983

Subject: Some DOT/PF Correspondence Concerning the Local Service Roads and Trails Program

To: Committee Members
Senate Transportation Committee

From: Senator H. Pappy Moss, Chairman,
Senate Transportation Committee *HPM*

Please find attached three pieces of correspondence included to provide some documentation on the position of DOT/PF regarding the LSRT program.

The first letter, dated November 24, 1982, is included to illustrate the response given by DOT/PF to local communities requesting funding under the LSRT program. The bracketed portion is highlighted as the relevant text concerning DOT/PF's philosophy toward the program.

The second letter, dated April 16, 1982, is included to provide an example of a local community's response to DOT/PF's attitude toward the LSRT program.

The third piece of correspondence is included to show the positive attitude of DOT/PF field managers toward the program.

STATE OF ALASKA

JAY S. HAMMOND, GOVERNOR

DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES
Division of Planning and Programming, Interior/Western Region

University Plaza - West, Suite D
600 University Avenue
Fairbanks, Alaska 99701
(907) 479-4284

November 29, 1982

Robert L. Brean
Executive Director
Upper Tanana Development Corporation
P.O. Box 459
Tok, AK 99780

Dear Mr. Brean:

RE: Tok Homesite Tract Road

We are responding to your letter of November 1, 1982 to Mr. Don Ronken, Regional Local Service Roads and Trails Engineer, regarding an access road to Homesite Tract ASL Survey #77-164.

The Department has not requested funding for continuance of the Local Service Roads and Trails Program in recent budget requests, since the functions of this program have been largely supplanted in recent years by the Municipal Grant Program and by line item appropriations added by the legislature to our Departmental request. Determination of projects to be added as grants or line item insertions is a prerogative of the legislature.

This policy has enabled the Department to concentrate its resources on the urgent needs of the regional transportation system, from reconstruction and rehabilitation of the existing major roads and bridges to upgrading essential rural airport runways and lighting systems.

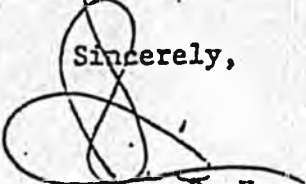
Our statewide transportation needs assessment already includes in excess of four billion dollars worth of needed improvements to the existing system. This tremendous backlog of identified projects coupled with the reduced state revenues now being projected makes it increasingly difficult to include in our budget our priorities for the facility improvements required for inter-community transportation, and certainly precludes us from budgeting for new facilities to service strictly local needs.

While your request is relatively small and for good purposes, there are far too many local needs of this type in our region for us to make an exception to our policies for your particular request.

We would be happy to provide technical assistance, in terms of engineering expertise and cost estimates, but are unable to include an actual funding request in our annual Capital Improvement Program.

Given these situations in relationship to your immediate need, it is suggested that you contact your local legislative contingent to ascertain if they might be willing to include a grant or appropriation for the access road during the coming session. We will be forwarding a copy of your letter to Senator Moss with a cost estimate.

Sincerely,



Robert R. Venusti
Director

cc: Senator H. "Pappy" Moss ✓

fairbanks north star borough

p.o. box 1267 520 fifth ave. fairbanks, alaska 99707 907-452-4761



April 16, 1982

Commissioner Bob Ward
State of Alaska
Department of Transportation
Pouch Z
Juneau AK 99811

Dear Commissioner *Bob* Ward:

I am responding to your March 12 letter in which you expressed your plans to phase out the Local Service Roads and Trails program.

I regret that you feel the need for this action, since the LSR&T Program provides a real and an essential service to people in this area. I do understand the impact of this program on your department and the problems which can arise--and have done so--in this regard. I would ask, however, that we first attempt to safeguard these services to the public and attempt to prevent the negative consequences which an abrupt elimination of the program would generate.

Your letter makes several statements with which I cannot concur, at least in-regard to the comments on local government.

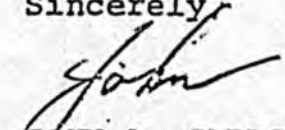
I think we are all aware that the state capital projects funding to local governments, as well as to state agencies, is being drastically reduced this year because of declining revenues to the State of Alaska. The special capital project funds we have received in the last two years have been for one-time activities and were specifically designed not to involve the Borough in expanded program operations.

Consequently, I can assure you that we do not have special funds in reserve which could be called upon to fund phased-out LSR&T activities. I would ask that, in view of the serious consequences which could flow from the elimination of this program, that DOT officials contact

Commissioner Bob Ward
April 16, 1982
Page two

their appropriate Borough counterparts to discuss ways and means by which a mutually beneficial arrangement might be reached, without first taking action that might later result in some harm to the public at large.

Sincerely,



JOHN A. CARLSON
Borough Mayor

/sek

TO Charles D. Karella
State LSR&T Engineer
DOT/PF
Headquarters/Juneau

DATE November 24, 1982

FILE NO

TELEPHONE NO: 443-5266

FROM Jeffrey S. Chandler *JSC*
Acting LSR&T Manager
DOT/PF
Interior Region/Home Residency

SUBJECT: LSR&T Program In Rural Communities

In response to your enquiry regarding the involvement of the Local Service Roads and Trails Program in rural communities of Western Alaska, and the attitudes held by the rural communities towards the Program, I offer the following information.

The LSR&T Program generally has been highly regarded and supported by the bush communities. The underlying reasons for this are as follows:

I. LOCAL PRIORITIES

~~The City Council of each community sets the priorities for local service projects needed in that community.~~

This process insures the Program's responsiveness to local needs. Within the limits of available funding, a community may designate access roads, local streets, bridges, boardwalks, or trail repairs for inclusion under the Program. Since LSR&T is primarily a reimbursement program, the local governing body may start construction as soon as a Project Agreement is approved and materials or equipment available, and is then reimbursed periodically for expenses. For larger heavy construction projects utilizing State equipment or a private contractor, costs are generally paid directly by the State.

II. LOCAL INVOLVEMENT

The local community has direct input into all phases of each project, from planning through construction.

The scope of the project selected is determined by the City Council, within the limits of sound engineering principles. For locally constructed projects, flexibility is preserved through the construction phase, allowing alterations to accommodate local concerns which may not have been anticipated initially. Although LSR&T administers the project, ~~it is basically a community enterprise in which the community shares responsibility.~~ Through cooperative effort the State provides a helping hand rather than a hand-out. A completed project becomes a source of pride for residents of a rural community because they were integrally involved in creating the end product.

III. LOCAL HIRE

~~Locally hired crews are employed to construct most rural LSR&T projects.~~

With labor intensive projects such as boardwalks and trail construction, the crew is hired locally by the local governing body which also appoints a local foreman.

Day to day operations of small scale heavy construction projects involving local equipment are also handled by a local foreman. On larger heavy construction projects involving State equipment, LSR&T may provide a foreman and/or a heavy-duty mechanic, while the construction crew is hired locally. In the event the local labor pool is exhausted, workers are generally sought from neighboring villages. The primary exception to this procedure occurs when a project has been let to contract, where hiring is done by a contractor whose policies generally tend to be more restrictive than those of local managers.

IV. LOCAL TRAINING

Training of local residents in new skills is necessary for most remote LSR&T road projects.

It is unusual to find the required number of qualified equipment operators and truck drivers locally available for the typical local road construction project. Training is therefore provided by an LSR&T representative or designated foreman and mechanic. Emphasis is placed on proper equipment care and usage, not only for safety reasons, but also to avoid unnecessary downtime. Safety awareness is encouraged through the life of a project in order to reduce the chances for injury or accident. This policy of training employees allows a greater proportion of local residents to obtain jobs than is the case with contractors, who generally require skilled workers for available positions. It also results in an added benefit outlasting the life of the project: a pool of trained workers who may be able to later obtain jobs for which they were previously unqualified.

V. LOCAL EQUIPMENT

Local equipment, public or private, is utilized for heavy construction projects whenever available.

For local road projects, LSR&T encourages the use of locally available equipment: from the local governing body, from DOT/PF, or from other public or private sources, in any combination necessary to perform the work. This practice results in substantial cost savings and production increases for the project, by avoiding the extra costs of mobilizing equipment from outside the community. LSR&T also has its own construction fleet, which is generally moved from project to project on an annual basis to supplement local equipment. In the event that adequate equipment is not available either locally or from LSR&T, and when private enterprise is available, then a project is generally put to contract with a contractor who is preferably already mobilized on site. It should be noted here that due to age and the phase-out of the LSR&T Program, most of the LSR&T equipment fleet will have been exceeded by the end of the year.

VI. REGION WIDE PROJECTS

All local communities in the Nome Residency area are eligible for participation in LSR&T's region-wide projects.

Winter Trail Staking and Shelter Cabins are two projects in which most rural communities participate annually. Although a limited amount of funds are available, these are distributed among local communities to help defray the costs of marking trails each winter, and the costs of putting up shelter cabins on the most heavily used trails for the use of the traveling public. A limited number of permanent trail markers are installed each year during Trail Staking. Permanent trail markers could be installed on the 1,400 miles of primary overland

trails in the Nome Residency area within two years, if current funding levels were doubled.

VII. PROFESSIONAL SUPPORT

Technical support services are provided to each project in proportion to the demands of the project and the needs of the community.

Engineering, supervisory, mechanical expertise, and expediting services are available for each LSR&T administered project. Where sufficient local initiative, labor, and expertise is available, periodic visits of the LSR&T Engineer is all that is required, resulting in lower administrative charges to the project. For small scale heavy construction projects, LSR&T provides a highly qualified roving heavy-duty mechanic, who periodically visits each project as needed to assist the local foreman in handling complex emergency mechanical repairs. For larger scale projects, the LSR&T Engineer or designated representative may direct the project and provide experienced foreman and qualified heavy-duty mechanic for the duration of the work. The key to this approach is flexibility: the fewer the resources of a local community, and/or the more complex the nature of the project, the greater the level of direct assistance provided, and vice-versa.

VIII. ADMINISTRATIVE SUPPORT

Most rural communities need some administrative support to keep their projects from bogging down.

This applies to a majority of local development projects involving local administration, whether funded through LSR&T, special legislative appropriations expended through LSR&T, or direct legislative grants to the villages. This need can be met by consulting engineering firms and contractors where funds are sufficient. However, for smaller projects a little common sense advice and direction is frequently all that is required, rather than a costly and lengthy study which doesn't help the local governing body with the day to day problems associated with doing its own construction projects. Although not mandated to do so, LSR&T has been providing technical information, quantity and cost estimates, and alternative recommendations to villages seeking guidance for the application of their direct grants for new construction equipment, new equipment storage buildings, and local transportation-related construction projects. The Nome office has handled seventeen such calls from different villages in the last five months.

IX. SUMMARY

Given the limits of available funding for rural areas, the LSR&T Program has made some substantial contributions in helping local communities to meet their own transportation needs. By and large, remote communities have not requested advanced development projects. Basic projects have been the rule, providing boardwalks, access roads, or streets for the community, frequently for the first time. Additionally, the Program has received strong support from the Bering Straits and KAKA Regional Native Corporations, in the form of streamlined permit procedures, free use materials, and expedited right-of-way reconveyance agreements. Finally, the LSR&T Program has retained the flexibility to fund those projects requested by villages which have not been addressed through the legislative process under line-item appropriations or specific grants. Hopefully this information will assist in the evaluation of the LSR&T Program as it has functioned in the Nome Residency, and contribute to the formation of a policy or program through which rural communities may continue to receive State support in alleviating their transportation problems.



Bristol Bay Borough

BOX 189 • NAKNEK, ALASKA 99633

JIM D. CLARK
MAYOR

TELEPHONE
(907) 246-4224

February 10, 1983

Representative Cato, Chairman
House Transportation Committee
Pouch V
Juneau, AK 99811

Dear Representative Cato:

I am writing in support of HB 169 which provides funds for the continuation of the Local Service Roads and Trails Program.

This program has been of great benefit to our borough. Utilizing local labor and construction equipment already in the area, the program gets more out of the state's money than other grant programs. The personnel at Local Service Roads and Trails are accustomed to working with residents from rural Alaska, understand the conditions which exist there, and work closely to see that all funds are accounted for and used efficiently. Many projects of varying size have been funded in this way, improving conditions in Alaska and contributing to a sense of accomplishment by local residents.

I hope your committee will consider this legislation favorably so the LSR&T program can continue to exist.

Sincerely,

Don Penner
Administrative Assistant

/DP

BRISTOL BAY NATIVE ASSOCIATION

P.O. BOX 189

DILLINGHAM, ALASKA 99576

by Executive Committee

Bristol Bay Native Association
Resolution in Support of the
Department of Transportation and
Public Facilities Local Service -
Roads and Trails (LSR&T) Program

Resolution No. 83 - .03

WHEREAS, the LSR&T program has been very effective in developing village roads in the Bristol Bay Area; and

WHEREAS, the LSR&T program has schedule projects to provide access roads in support of the Bristol Bay Housing Authority (BBHA) Low Income HUD Housing construction in several villages in the Bristol Bay Region; and

WHEREAS, the Bristol Bay Housing Authority has awarded contracts for the construction of houses at Aleknagik, Egegik, Levelock and Port Heiden; and

WHEREAS, the Bristol Bay Housing Authority has scheduled a housing program for 1984 and 1985 at the villages of Iliamna, Igiugig, Kokhanok, Newhalen, and Nondalton and will require access road construction in each village; and

WHEREAS, additional road construction is needed to improve or provide access to townsite lots within village townsites; and

WHEREAS, each village is in need of a Dump Site with access road.

NOW THEREFORE BE IT RESOLVED, that the Department of Transportation and Public Facilities reactivate there very successful Local Service Roads and Trails program which is the very foundation of municipal sewer, water and sanitary waste disposal systems; and

BE IT FURTHER RESOLVED, that all of the projects scheduled herein be incorporated within the reactivated Local Service Roads and Trails program.

SIGNED

William P. Johns
PRESIDENT

CERTIFICATION:

I, the undersigned secretary of said Association, do hereby certify that the Executive Committee is composed of ten (10) members, of whom 8 were present at a meeting this 20 day of January, 1983, and that the foregoing resolution was adopted by the affirmative vote of 8 members.

SIGNED

Ada Aokkedak
SECRETARY

needs answer

TWIN HILLS VILLAGE COUNCIL

GENERAL DELIVERY
TWIN HILLS, ALASKA 99576-2
(907) 972-8001

December 7, 1982

Adelheid Herrmann
P.O. Box 63
Naknek, AK 99633

Dear Adelheid:

On behalf of the Village Council and Community of Twin Hills, I would like to congratulate you to represent our district. With all our prayers and best wishes to your success at work.

I would like to know whether or not the legislator is planning on dissolving the DOT programs for villages that need state assistance in building their roads? I was told that these road construction firm companies are pushing to end state appropriation monies be contracted out to DCT.

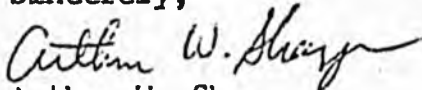
We the Village Council of Twin Hills oppose the idea of having another private owned companies bringing in their own workers and build our roads. I think the program that DOT provides should not be dissolved. Because they hire local residents and at the same time the people who work, get experience in operating heavy equipments.

We also have two(2) new council members and all the council members terms will end October 1984. They are as follow:

Arthur W. Sharp.....President
Roger Wassillie.....Vice President
John W. Sharp.....Sec./Treasure
John C. Mark.....Member
Christopher Sharp, Sr....Member

Thank you and congratulations again.

Sincerely,



Arthur W. Sharp
President

cc: Twin Hills Village Council

City of Akutan

Coastal Resource Service Area

There has been some thought given to forming a Coastal Resource Service Area in the Aleutians west of Unimak Pass. I am enclosing a copy of a letter I recently received from Murray Walsh to Mayor Bill Fisher of Unalaska. In the letter, Mr. Walsh refers to a meeting that was held in Anchorage on November 16, 1982. In addition to the representatives from the Aleutians there were also people there from St. Paul and St. George. A resolution was passed by the participants at that meeting that the ~~Alaska Coastal Management Act~~ needs to be amended ~~to provide for participation by all the communities of the region. We would appreciate it if you could look at the issue and work on the drafting of such legislation or taking other steps necessary.~~ As things stand now, if a district were to be formed in the western Aleutians, at least five of the six seats would go to Unalaska, with the villages having one seat among the three communities, if we were lucky. The present law does not allow for the inclusion of the Pribilofs in an Aleutian district because of the link between the CRSA boundaries and the REAA boundaries. We are not sure if the Pribilofs would even be interested in being included in an Aleutian district, but one of the attractive features of that configuration would be the dispersal of power among more than one large community. It is a relatively complex issue and one that looks as if it will come to a head in the coming months. We would appreciate any help you can give us.

Local Service Roads and Trails

~~A second issue is that of the elimination of the Local Service Roads and Trails program by the Alaska Department of Transportation.~~ This program is relatively small, but it does good things for rural communities. DOT/PF thinks that it will be able to meet the diverse needs of rural communities through its regular program. We in Akutan have dealt with both parts of DOT and truly feel that the LSR & T program more adequately meets the rural need. Our LSR & T project is almost completed so it wouldn't really make that much difference to Akutan, but it is a program that has done good things for us. We would like to see other communities have the same opportunity.

Service Delivery in Rural Alaska

I am also enclosing a copy of a plan I have for coordination of service delivery by state agencies in rural Alaska. I mentioned it to you when we met. I have also sent a copy to Representative Jack McBride but he and I have not discussed it

KOLIGANEK VILLAGE COUNCIL
KOLIGANEK, ALASKA 99576 - VILLAGE TELEPHONE (907) 596-8001

December 28, 1982

Senator Nels Anderson, Jr.
Dillingham, Alaska 99576

Dear Nels:

I am now in receipt of your letter dated December 7th, requesting our list of five capitol project priorities. Before I list them, let me say that it would be appreciated if you would request that Rep. Herman, and Sen Mulcahy, have their offices insure that mail addressed to the village council is addressed as follows, in order to facilitate prompt responses on our part.

Koliganek Village Council
attn: C.T. Seidl, Admin.
Koliganek, Alaska 99576

As to our priorities we held a meeting this afternoon, and developed the following list:

- 1) It is imperative that we have the airport moved away from the village. This is far and above any other priority. You may recall the problem we have been having in the orderly expansion and growth of Koliganek, due to the Airport cutting us off. Representative Herman should also be aware of this situation as I went over it in detail during her campaign. Jack Moore's of BBWA, and Tom Hawkins are both very knowledgable in the mechanics of such a land trade off and indicate that there are precedents for such action; we have already discussed it.
- 2) Additional funding for our LSR&T project. The equipment is here in Koliganek, and we are awaiting spring for the continuation of our project, but could really use more funding to increase the length of our local roads.
- 3) The upgrading of water and sewer. Realizing that funding may not be available for such a large project as the excavating of ground and laying of pipe mains, it might be that two wells could be dug, with insulated pump houses that are equipped with self-draining, freeze proof water outlets. One each placed on either end of the village, so that we can pump cleaner water that is in the river, and the chore would be safer, than treading out on the ice, particularly for the younger children.

LSR & T... this is a road?

Some Facts About The LSR & T Program

Local Service Roads and Trails, as enacted by the legislature of the State of Alaska, is for the purpose of providing for the construction of roads and trails which are not included in the approved federal aid primary or secondary systems which are eligible for federal-state matching funds.

The system is 100% state funded through bond issues approved by the people of Alaska. The State is divided into five allocation districts, by a formula which is based on area and population. The funds are then further allocated to the boroughs and home rule cities. Any funds not allocated to a local government are allocated to the unorganized borough and administered by the Department of Highways.

The program first went into effect in July of 1971 with total funding in the amount of \$5,000,000.00 of which \$629,000.00 was allocated to southeastern. A subsequent bond issue provided an additional \$838,000.00 for southeastern.

Boroughs and home-rule Cities with the capability of engineering and supervising roadway and/or trail construction enter into agreement with the Department of Highways regarding the scope of a project. The Department approves design plans, authorizes advertising and award of contracts, and approves the completed project.

Within the unorganized borough the projects are handled in a variety of ways to realize the most out of the funds and to serve the best public interest. This could be by preparation of plans and advertising by the Department, by use of the local labor force, through a reimbursable agreement with another agency or a combination of the above.

Since this program was initiated in 1971, 42 projects have been completed, 11 projects are under construction, and 7 additional projects are in the planning stages, for southeastern Alaska.

Active projects include a boardwalk between the inner and outer harbors at Port Alexander, clearing of Anchorage Street in Klawock, a complete year around trail facility in Haines, reconstruction of the boardwalk in Elfin Cove, street surfacing in Yakutat and construction of 4th Street in Craig.

A few of the major projects completed within the organized boroughs and home-rule Cities include the Carlanna Lake Road extension in Ketchikan, Outer Drive in Wrangell, and paving and grading of Davis Avenue in Juneau.

In the planning stages are projects in Sitka, Petersburg, Wrangell, Skagway, Angoon, Pelican and others.

In the scheme of road building things, there are roads that are Federal Aid Primary and there are roads that are Federal Aid Secondary. Both types are eligible for federal-state matching funds.

And then there are those roads that aren't funded at all, or at least they weren't until the Alaska State Legislature some years ago came up with approval for a bonding issue on a periodic basis to create wanted roads where no funding sources were available.

The result was LSR&T, otherwise known as Local Service Roads and Trails, and it works in this manner:

—Bond funds are allocated among the five districts under an area/population formula.

—Home rule cities and organized boroughs within the districts are given their pro-rated share.

—Unorganized boroughs have funds earmarked for their use, to be administered by the Department of Highways.

—Home rule cities and organized boroughs select their own projects with Highways acting only as an approval agency.

—Unorganized borough funds are handled in several ways, but the local village has project selection power.

Planned expenditures in 1976 for the LSR&T program total slightly over \$5 million, with nearly a half a million dollars of that amount to be spent in Southeast Alaska. Fifteen projects will be undertaken in Southeast, while the statewide total is 75.

LSR&T is road building in its basic form. There is a minimum of red tape, a maximum of local involvement, and for every dollar spent there is a visible result.

On January 19, 1973, a storm with 70 mile-an-hour winds pushed logs along on an 18.5 foot high tide and destroyed the Tinkum Creek bridge at Kasaan Village. The structure was a bridge in the sense that it crossed water, but not a bridge in the sense of a span carrying cars, trucks or trains. It carried people, and perhaps bikes. But no cars. It was a boardwalk, and certainly not eligible for federal-state matching funds.

Enter LSR&T.

On March 25 three local men were signed up to work, a project supervisor was on hand and needed materials were ordered from Ketchikan to augment the salvaged bridge lumber from the old structure. As the initial stages were underway onsite, the Ketchikan materials were inadvertently sent to Klawock. The Highway-Department Maintenance Foreman there took the timbers to Hollis and placed them on the beach. The Kasaan project supervisor chartered a boat, got the lumber off the beach, and delivered it to Kasaan. On April 4,

the new Tinkum Creek Bridge was finished...and relocated some 45 feet upstream where it would be safer from big storms.

Total cost.... \$4,820.00.

In the spring of 1975, a huge log serving as a bridge across Indian River near Tenakee collapsed. It had been placed there in 1957 by Loyal Clark during a ten-day period that required the use of a raft, of an International tractor and of much musclepower. The log bridge was used by residents and outdoorsmen throughout the Indian river-Sunny Cove - Columbia Superior - Harley Creek - Coffee Cove area. It was sorely needed.

LSR&T again to the rescue.

Immediately following the Thanksgiving weekend in 1975, a

crew was hired and supplies were brought in by a Juneau flight service. Unhappily, the weather changed and snow delayed the project. It was rescheduled for March 1, 1976, but the winter's accumulation of snow forced another postponement.

But the work will be done under the terms of LSR&T, and when it is completed, Tenakee residents will have a double-log bridge, cabled together, and a planked walking surface. The trail which the bridge serves will also be improved.

Kasaan came in for another unique project that started in 1975 when an old boardwalk, connecting the village to the boat harbor and airplane float finally gave up the fight and surrendered to rot. When it came to the point that residents were falling through the rotting wood and being injured, a call for LSR&T was made.

Local labor was signed up, and lumber was to be supplied by the local sawmill. Work was started on August 5, but because the small mill kept chewing up engines...and the owner/operator was forced to scurry about Southeast in search of replacements...the project moved in fits and starts rather than

leaps and bounds. When winter shut things down, about 300 feet of boardwalk remained to be completed after spring startup.

This project was unique in that Kasaan had no money available, so no reimbursable services agreement could be entered into. To solve this problem, the Department of Highways set up a bank account so the weekly payroll could be handled and materials purchased. Checks were countersigned by a village representative and by the LSR&T engineer. This arrangement has been used satisfactorily several times during the life of the LSR&T program.

Strange things do happen on LSR&T projects. One Hoonah job invoice included the cost of rifle shells...certainly no ordinary item for a highway project. But in this case, the project was the clearing of a trail and the rifle shells were needed to keep the bears at bay! And even the dogs sometimes get into the act, as was the case with that same Hoonah job. They, like the rifle shells, were used to help convince the bears that this was to be a people trail, not a bear trail.

Local labor, local material and even local dogs, all are a part of LSR&T. Money is brought into the closed loop of a village economy...jobs are created where none exist...and roads are built where they are most needed, and least affordable. LSR&T is basic people-to-people roadwork.

In defining "road", Webster says nothing about cars, trucks and trains. Those terms have been added in our minds through usage. Webster merely says that a road is "a public way for traveling upon". If your village is 1,500 feet of muck and mire away from the village boat harbor, a boardwalk is just as surely a road as is the finest stretch of eight lane superhighway in the world... and a planked log spanning a 20 foot wide stream can be just as important as is the Golden Gate Bridge.

And that's what LSR&T is all about.

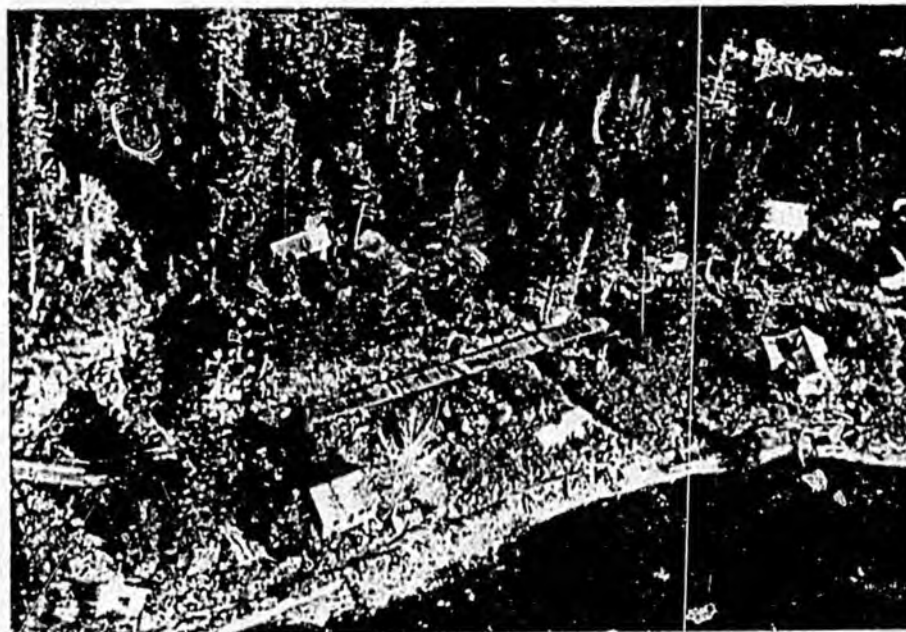
Port Alexander Boardwalk
Project G-30399

Port Alexander is a small fishing community located on the southern tip of Baranof Island. Facilities for docking are located on what is known as the Inner Harbor and the Outer Harbor, located about 1/4 mile apart. The trail between harbors is along the edge of the inlet connecting the harbors. Due to storms, tidal action and other factors, the bulkhead along the trail, and the trail itself are almost obliterated in some areas.

It was requested that LSR & T fund a trail and/or boardwalk between harbors to provide public access. As the Division of Harbors and Waters was active in the area, and helped initiate the project, it was agreed they would supervise the project and a reimburseable agreement was prepared in November, 1973 in the amount of \$34,000.00

Work commenced in the spring of 1974 under the supervision of Richard Cleaver, a local resident. Due to unavailability of gravel, the trail portion was converted to a boardwalk and an additional \$10,000.00 authorized for the project.

Completion was in June, 1976.



Alaska State Legislature

House of Representatives

Committee on Transportation



Rep. Bette Cato, Chairman

Lacker
2 attachments

Pouch V
State Capitol
Juneau, Alaska 99811
(907) 465-4858

TRANSPORTATION PHONE: Phone 465-4858/4859

Meets: Capitol 112, M-F, 8:30 a.m.

REVISION

Wednesday, February 9, 1983

HCR 2: ...travel by senior citizens aboard vessels of
State Marine Highway

Commissioner Casey/DOT/PF Overview

FEB 8 1983

2/14

COMMITTEE REPORT

HOUSE

FURTHER: FINANCE

Return

Date: 2/14/83

2/7/83

Mr. Speaker:

The Committee on TRANSPORTATION has had HB 169

An act making an appropriation to the Department of Transportation and Public Facilities for allocations for local service roads and trails; and providing for an effective date.

under consideration and reports it back as follows:

- do pass *1/1/83* do not pass
- do pass with attached amendments(s)
- replace with CS for _____ same title new title
- and recommends _____
- AND attaches a "Letter of Intent" New Fiscal Note
- reports it back without recommendation Zero Fiscal Note Attached
- referred to the _____ Committee

**MEMBERS SIGNING
DO PASS**

**MEMBERS HAVING
OTHER RECOMMENDATIONS:**

[Handwritten signatures]

[Handwritten signature]

[Handwritten signature]

CHAIRMAN

(b) The commissioner shall, upon request of a local government, transfer funds allocated under AS 19.30.131 to any local government which has assumed local road powers for project cost amounts and contract award amounts submitted to the commissioner.

(c) Nothing in AS 19.30.111 — 19.30.241 prohibits a local government from participating financially in the acquisition and construction of a local service road or trail. (§ 2 ch 84 SLA 1971; am Executive Order No. 39, § 11 (1977); am §§ 2, 3 ch 158 SLA 1980; am § 4 ch 38 SLA 1981)

Effect of amendments. — The 1977 amendment substituted a reference to the Department of Transportation and Public Facilities for a reference to the Department of Highways in the first sentence of subsection (a).

The 1980 amendment deleted the former second sentence in subsection (a), which

read: "A local government which has assumed road powers before May 20, 1971, may not be required to hold an election as provided by this section", and rewrote subsection (b).

The 1981 amendment, effective July 1, 1981, added "acquisition and" preceding "construction" in subsection (c).

Sec. 19.30.161. Design standards, rights-of-way and widths. Design standards, rights-of-way and widths for each local service road and trail project shall have the prior approval of the commissioner, unless the project is under the supervision of a local government that has assumed local road powers. If the project is under the supervision of a local government that has assumed road powers, design standards, rights-of-way and widths shall be established by the local government. (§ 2 ch 84 SLA 1971; am § 4 ch 158 SLA 1980)

Effect of amendments. — The 1980 amendment inserted "rights-of-way and widths" at the beginning of the section, and added the material at the end of the

section following, "approval of the commissioner" and beginning, "unless the project is."

Sec. 19.30.171. Acquisition and conveyance of rights-of-way. (a) The local government shall acquire all rights-of-way required for local service road and trail construction. However, if the local government is unable to acquire the necessary right-of-way, the commissioner is authorized to acquire it in accordance with AS 19.05.080 — 19.05.120. The commissioner may convey to the local government, by appropriate instrument executed in the name of the state, any land or interest in land which has been acquired by the state for the construction of local service roads or trails.

(b) The costs incurred by the commissioner in acquiring this land or interest in land includes all costs and any fees incidental to acquisition. All costs incurred in connection with the acquisition of the land or interest in land for local governments that have assumed local road powers shall be charged to the project allocation for which the land or interest in land is acquired. Right-of-way acquisition costs incurred by the commissioner for local governments without local road powers

under AS 19.30.151(a) shall be charged to the project allocation for the local government for which the land or interest in land is acquired.

(c) When rights-of-way for local service roads are acquired over land or interest in land owned by the state, the commissioner may make such arrangements with the department having jurisdiction over the land as may be necessary to give the local government adequate rights-of-way, and any such department is directed to transfer the jurisdiction of the land to the commissioner. (§ 2 ch 84 SLA 1971; am § 17 ch 71 SLA 1972; am § 5 ch 158 SLA 1980; am § 5 ch 38 SLA 1981)

Effect of amendments. — The 1980 amendment deleted "including relocation assistance and payments in accordance with AS 34.60" following "to acquisition" at the end of the first sentence of subsection (b), deleted "by the commissioner" following "All costs incurred" near the beginning of the second sentence of subsection (b), inserted "for local governments

that have assumed local road powers" near the middle of the second sentence of subsection (b), and added the present third sentence of subsection (b).

The 1981 amendment, effective July 1, 1981, substituted "charged to the project allocation" for "paid by the local government" in the second sentence of subsection (b).

Sec. 19.30.181. Project agreements. A local government which has assumed local road powers may enter into a formal project agreement with the commissioner for the acquisition and construction of a specific project. (§ 2 ch 84 SLA 1971; am § 6 ch 158 SLA 1980; am § 6 ch 38 SLA 1981)

Effect of amendments. — The 1980 amendment rewrote the section. The 1981 amendment, effective July 1,

1981, added "acquisition and" preceding "construction of."

Sec. 19.30.191. Letting of contracts. (a) Repealed by § 15 ch 158 SLA 1980.

(b) Contracts entered into by a local government that has assumed local road powers for the construction of each project shall be awarded only on the basis of the lowest responsible bid submitted by a bidder meeting established criteria of responsibility.

(c) All contracts by a local government that has not assumed local road powers must have the prior concurrence of the commissioner. The commissioner may delegate the power to approve contracts to a local government that has assumed local road powers for the acquisition and construction of local service roads and trails.

(d) Except as provided in (b) and (c) of this section and in AS 44.33.300, construction of each project shall be administered by the department in accordance with guidelines established by the commissioner by regulation. (§ 2 ch 84 SLA 1971; am § 4 ch 277 SLA 1976; am §§ 7 — 9, 15 ch 158 SLA 1980; §§ 7, 8 ch 38 SLA 1981)

portation and Public Facilities if constructed outside local government units which have zoning ordinances. If these roads are constructed within the boundaries of local government units which have zoning ordinances, the width shall conform to subdivision control ordinances of the local government units. Contracts for the work shall be awarded to the lowest responsible bidder qualified to contract with the state. (§ 1 ch 122 SLA 1961; am § 3 ch 7 SLA 1964; am Executive Order No. 39, § 11 (1977))

Effect of amendments. — The 1977 amendment substituted the reference to the Department of Transportation and Public Facilities in the third sentence for a reference to the Department of Highways.

Article 3. Local Service Roads and Trails.

Table with 2 columns: Section and Section. Rows include: 111. Purpose, 131. Allocation of funds, 141. Acquisition and construction programs, 151. Local government participation, 161. Design standards, rights-of-way and widths, 171. Acquisition and conveyance of rights-of-way, 181. Project agreements, 191. Letting of contracts, 201. Construction, 211. Maintenance, 221. Availability of allocated sums, 231. [Repealed], 233. Reports, 241. Definitions.

Sec. 19.30.111. Purpose. It is the purpose of AS 19.30.111 — 19.30.251 to provide for the acquisition and construction of local service roads and trails which are not included in the approved federal-aid primary highway systems eligible for federal-state matching funds. (§ 2 ch 84 SLA 1971; am § 1 ch 38 SLA 1981)

Effect of amendments. — The 1981 amendment, effective July 1, 1981, added "acquisition and" preceding "construction of local service roads" and deleted "or secondary" preceding "highway systems."

Sec. 19.30.131. Allocation of funds. (a) During each fiscal year the commissioner shall allocate sums appropriated or otherwise designated for expenditure upon local service roads for that fiscal year among the five allocation districts in the following manner: one-half in the ratio which the area of each allocation district bears to the total area of the state and one-half in the ratio which the population of each allocation district bears to the total population of the state as shown by the latest available federal census.

(b) The commissioner shall also further allocate the sums in each allocation district to the boroughs within each allocation district in the following manner: one-half in the ratio which the area of each organized borough (excluding salt water areas) within that district bears to the total area of the allocation district and one-half in the ratio

tract bears to the total population of the allocation district as shown by the latest available federal census.

(c) The commissioner shall also further allocate portions of the sum allocated to any borough, either organized or unorganized, and to any home rule or first class city within the borough in the following manner:

(1) one-half in the ratio which the area of each home rule or first class city bears to the total area of the borough excluding salt water areas; and

(2) one-half in the ratio which the population of each home rule or first class city bears to the total population of the borough as shown by the latest available federal census.

(d) The sums not allocated within each district to a local government under (b) and (c) of this section shall be allocated to the unorganized borough and administered by the department. (§ 2 ch 84 SLA 1971; am § 2 ch 38 SLA 1981)

Effect of amendments. — The 1981 amendment, effective July 1, 1981, added "or first class" following "home rule" in three places in subsection (c). Also in subsection (c), the amendment added the paragraph designations (1) and (2).

Sec. 19.30.141. Acquisition and construction programs. Before October 1 of each fiscal year each local government eligible for allocation of funds under AS 19.30.131 shall submit to the commissioner for approval a five-year plan for the acquisition and construction of local service roads and trails. Before December 1 of each fiscal year the commissioner shall submit to the governor a five-year plan for the acquisition and construction of local service roads and trails, including the approved local government programs. An organized borough shall include in its five-year plan local service road acquisition and construction programs for all cities other than home rule and first class cities within the boundaries of the borough. The commissioner shall include in his five-year plan local service road and trail acquisition and construction within the unorganized borough. (§ 2 ch 84 SLA 1971; am § 3 ch 38 SLA 1981)

Effect of amendments. — The 1981 amendment, effective July 1, 1981, substituted "five-year plan" for "long-range program" in four places, added "acquisition and" preceding "construction" in four places and added "and first class" preceding "cities within the boundaries" in the next to the last sentence.

Sec. 19.30.151. Local government participation. (a) Upon application by a local government, local service roads and trails constructed under the provisions of AS 19.30.111 — 19.30.241 and located within the jurisdictional boundaries of a local government, as defined in AS 19.30.241(3), may be transferred to that local government upon

MEMORANDUM

State of Alaska

TO: Charles Karella
State LSR&T Engineer
Highways Design and Construction

DATE: November 23, 1982

FILE NO: 245C-2519

TELEPHONE NO: 266-1542

FROM: Robert L. Haneline *RPH*
Central Regional
LSR&T Engineer

SUBJECT: LSR&T Program
Justification &
Impacts

In presenting an overview of the existing LSR&T program and its possible future direction the following is offered for consideration.

There is a subtle but potentially significant change underway in the thinking regarding the pending phase-out of the LSR&T program. Many people are increasingly considering as realistic the possibility that the proposed phase-out of the LSR&T program might take place. This was apparent at a number of meetings just completed in Anchorage (State Planners Meeting), Fairbanks (Municipal Leagues Meeting), Kotzebue (Traditional Council Presidents Meeting) and Bethel (Traditional Council Presidents Meeting) where the attitude was one of apprehension and concern.

As the realities of the program possibly being abandoned come closer, it is apparent there is a need to address some of the significant impacts of the program not being available.

It has become increasingly apparent, and properly so, that a lot of the "bush" needs cannot be addressed by the State C.I.P. program. Smaller projects are required to compete with larger projects for available funding resources on a regional basis, severely limiting their chances for funding. The mechanisms of a lot of the smaller communities to supply the necessary expertise are limited due to isolated conditions and communication problems. Most "bush" communities do not have the internal resources to help themselves, and often lack the technical background to effectively determine if they are getting what they pay for. Even if given direct grants, use of the monies may not be maximized due to the lack of knowledge on how to best utilize existing resources to efficiently accomplish set goals. This is nourishing doubt as to the direction being taken and is evident to the obvious appeal the LSR&T Program has to a broad popular support.

As the State has a sizable responsibility to "bush" communities, it seems appropriate that the State give some attention to identifying ways in which this commitment to smaller communities will reap the highest return.

A productive example of effective channeling of State funds into "bush" communities is the LSR&T Program. There are many ways in which the State and "bush" communities, alike, have benefited from the creation of the LSR&T Program, where the attitude toward community improvements is that of participation and self-help rather than that of being subsidized at someone else's expense, something not to be considered as the primary purpose or intent of the program, but only mentioned here as it serves to reflect on another positive factor to be contemplated when evaluating the importance of the LSR&T program.

The LSR&T Program may not ~~be~~ represent the total solution to providing for the needs of the "bush" communities, but it is certainly a step that provides economics of scale where other programs cannot provide the same end results for the dollars available. Admittedly, there are some problems with the program, but no one can deny the great benefits of the program. What the LSR&T Program expresses and provides is a sensitivity to "bush" needs, an opportunity for "bush" communities to recognize and fulfill the responsibilities of their communities and to directly contribute to the construction of their transportation needs. This direct involvement instills a sense of community pride in the work accomplished, which normally results in greater care being exercised in the general treatment of the finished construction product. The LSR&T Program provides the necessary link between the State's investment in terms of monies spent and in terms of meeting "bush" transportation needs that might not otherwise be met.

It is felt the LSR&T Program, being the small State Section that it is, accomplishes positive results for the State for a minimal investment compared to larger programs with higher overhead costs.

The present portfolio of the LSR&T Section of the Department of Transportation and Public Facilities consists of hundreds of projects totaling millions of dollars. These projects vary in scope from simple boardwalk and footbridge projects to massive, multilane, arterial highway systems. This project versatility is a reflection of the flexibility developed by members of the LSR&T staff which, balanced against the "bush" needs, succeeds in effectively accomplishing set goals.

None of this is to suggest the LSR&T Program is beyond reproach. But only to say that a great deal of energy and money have been invested in the development of the LSR&T Program, resulting in the efficient workable program it is today. To abandon a program that fills needs that otherwise may not be met seems contrary to the best interest of the State. It is plain that awareness and open minds are required in the final decision making process.

MEMORANDUM

State of Alaska Department of Transportation & Public Facilities

TO: C.D. Karella
State LSR&T Engineer
Juneau

DATE: November 15, 1982

FILE NO:

TELEPHONE NO:

FROM: D.S. Ronken *DR*
LSR&T Engineer
Fairbanks

SUBJECT: LSR&T Program

In response to our telephone conversation of 11/5/82, I have informed J. Chandler at Nome to supply you with the information you requested regarding the LSR&T Program in the bush communities.

I have been in contact with the local governments and the comments received from them regarding the LSR&T Program are as follows:

Since local roads and streets are always in need of improvement the local governments would like all the funds they can possibly obtain for roadway construction.

The local governments without road powers feel that the DOT/PF has too much control over the funds. They object to some of the conditions stated in Section "C" of the project agreement.

The local governments however, do like the flexibility of the Program, i.e.:

- 1) Various types of construction that can be funded
- 2) Project funding can be changed to cover overrun, providing funds remain in their allocations
- 3) LSR&T Construction Program submitted (priority list) can be changed, if for some reason the local needs change

Some local governments in the Fairbanks area feel that the manner in which funds are allocated based on population by the latest federal census should be changed. With the population in the Fairbanks area increasing every year and the allocation based on the latest census of 1980 which has been questioned by the local government, they feel the present system leaves much to be desired.

In summary, the local governments like the program and would like to see it refunded. The funds obtained through the LSR&T Program are not sufficient to cover the road improvement needs, but all funds that can be obtained to improve the local transportation system will be accepted by the local governments providing the conditions for its acceptance do not change.

TO: Charles D. Karella
State LSR&T Engineer
DOT/PF
Headquarters/Juneau

DATE: November 24, 1982

FILE NO.

TELEPHONE NO: 443-5266

FROM: Jeffrey S. Chandler *JSC*
Acting LSR&T Manager
DOT/PF
Interior Region/Nome Residency

SUBJECT: LSR&T Program In Rural Communities

In response to your enquiry regarding the involvement of the Local Service Roads and Trails Program in rural communities of Western Alaska, and the attitudes held by the rural communities towards the Program, I offer the following information.

The LSR&T Program generally has been highly regarded and supported by the bush communities. The underlying reasons for this are as follows:

I. LOCAL PRIORITIES

The City Council of each community sets the priorities for local service projects needed in that community.

This process insures the Program's responsiveness to local needs. Within the limits of available funding, a community may designate access roads, local streets, bridges, boardwalks, or trail repairs for inclusion under the Program. Since LSR&T is primarily a reimbursement program, the local governing body may start construction as soon as a Project Agreement is approved and materials or equipment available, and is then reimbursed periodically for expenses. For larger heavy construction projects utilizing State equipment or a private contractor, costs are generally paid directly by the State.

II. LOCAL INVOLVEMENT

The local community has direct input into all phases of each project, from planning through construction.

The scope of the project selected is determined by the City Council, within the limits of sound engineering principles. For locally constructed projects, flexibility is preserved through the construction phase, allowing alterations to accommodate local concerns which may not have been anticipated initially. Although LSR&T administers the project, it is basically a community enterprise in which the community shares responsibility. Through cooperative effort the State provides a helping hand rather than a hand-out. A completed project becomes a source of pride for residents of a rural community because they were integrally involved in creating the end product.

III. LOCAL HIRE

Locally hired crews are employed to construct most rural LSR&T projects.

With labor intensive projects such as boardwalks and trail construction, the crew is hired locally by the local governing body which also appoints a local foreman.

Day to day operations of small scale heavy construction projects involving local equipment are also handled by a local foreman. On larger heavy construction projects involving State equipment, LSR&T may provide a foreman and/or a heavy-duty mechanic, while the construction crew is hired locally. In the event the local labor pool is exhausted, workers are generally sought from neighboring villages. The primary exception to this procedure occurs when a project has been let to contract, where hiring is done by a contractor whose policies generally tend to be more restrictive than those of local managers.

IV. LOCAL TRAINING

Training of local residents in new skills is necessary for most remote LSR&T road projects.

It is unusual to find the required number of qualified equipment operators and truck drivers locally available for the typical local road construction project. Training is therefore provided by an LSR&T representative or designated foreman and mechanic. Emphasis is placed on proper equipment care and usage, not only for safety reasons, but also to avoid unnecessary downtime. Safety awareness is encouraged through the life of a project in order to reduce the chances for injury or accident. This policy of training employees allows a greater proportion of local residents to obtain jobs than is the case with contractors, who generally require skilled workers for available positions. It also results in an added benefit outlasting the life of the project: a pool of trained workers who may be able to later obtain jobs for which they were previously unqualified.

V. LOCAL EQUIPMENT

Local equipment, public or private, is utilized for heavy construction projects whenever available.

For local road projects, LSR&T encourages the use of locally available equipment: from the local governing body, from DOT/PF, or from other public or private sources, in any combination necessary to perform the work. This practice results in substantial cost savings and production increases for the project, by avoiding the extra costs of mobilizing equipment from outside the community. LSR&T also has its own construction fleet, which is generally moved from project to project on an annual basis to supplement local equipment. In the event that adequate equipment is not available either locally or from LSR&T, and when private enterprise is available, then a project is generally put to contract with a contractor who is preferably already mobilized on site. It should be noted here that due to age and the phase-out of the LSR&T Program, most of the LSR&T equipment fleet will have been exceeded by the end of the year.

VI. REGION WIDE PROJECTS

All local communities in the Nome Residency area are eligible for participation in LSR&T's region-wide projects.

Winter Trail Staking and Shelter Cabins are two projects in which most rural communities participate annually. Although a limited amount of funds are available, these are distributed among local communities to help defray the costs of marking trails each winter, and the costs of putting up shelter cabins on the most heavily used trails for the use of the traveling public. A limited number of permanent trail markers are installed each year during Trail Staking. Permanent trail markers could be installed on the 1,400 miles of primary overland

trails in the Nome Residency area within two years, if current funding levels were doubled.

VII. PROFESSIONAL SUPPORT

Technical support services are provided to each project in proportion to the demands of the project and the needs of the community.

Engineering, supervisory, mechanical expertise, and expediting services are available for each LSR&T administered project. Where sufficient local initiative, labor, and expertise is available, periodic visits of the LSR&T Engineer is all that is required, resulting in lower administrative charges to the project. For small scale heavy construction projects, LSR&T provides a highly qualified roving heavy-duty mechanic, who periodically visits each project as needed to assist the local foreman in handling complex emergency mechanical repairs. For larger scale projects, the LSR&T Engineer or designated representative may direct the project and provide experienced foreman and qualified heavy-duty mechanic for the duration of the work. The key to this approach is flexibility: the fewer the resources of a local community, and/or the more complex the nature of the project, the greater the level of direct assistance provided, and vice-versa.

VIII. ADMINISTRATIVE SUPPORT

Most rural communities need some administrative support to keep their projects from bogging down.

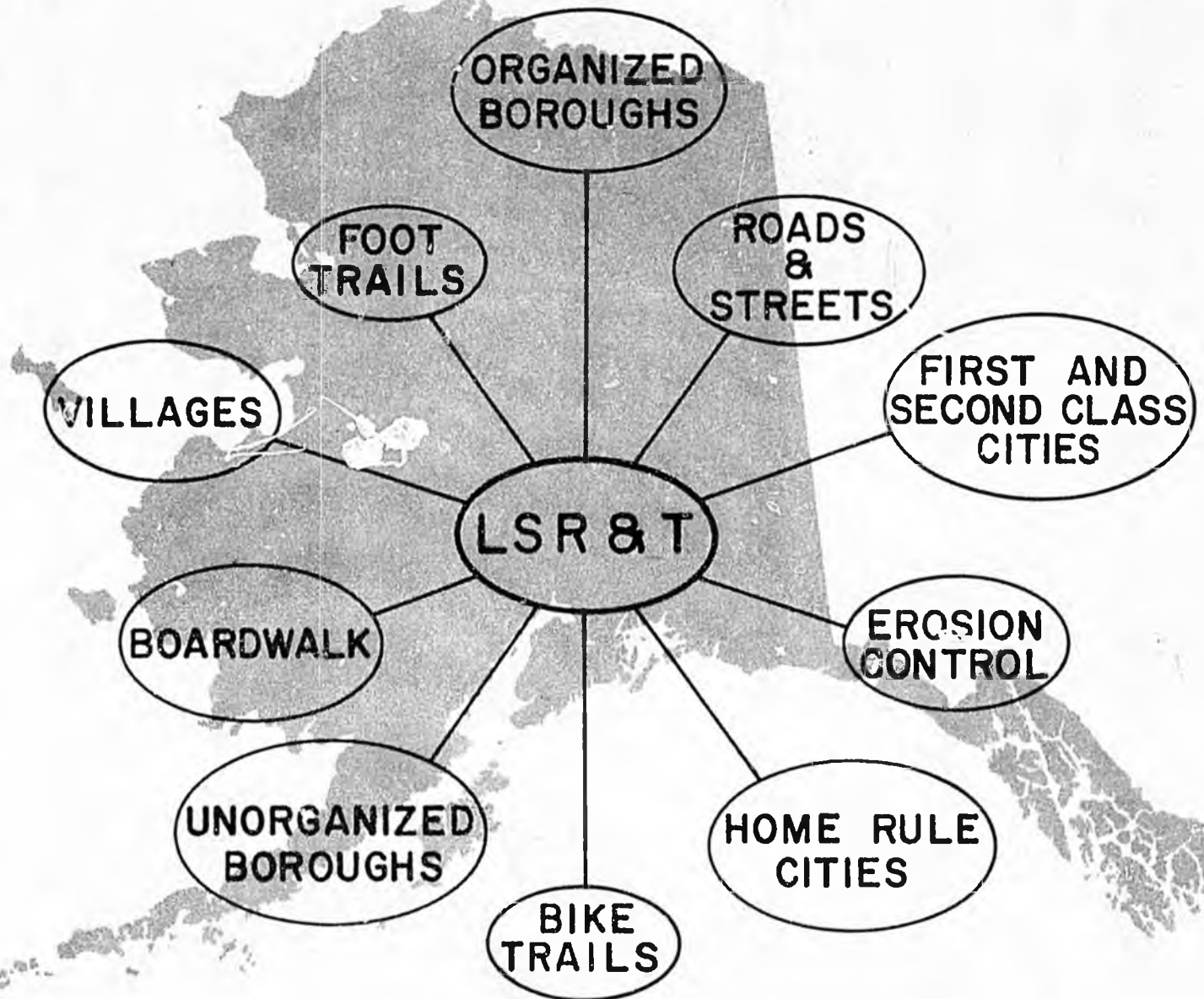
This applies to a majority of local development projects involving local administration, whether funded through LSR&T, special legislative appropriations expended through LSR&T, or direct legislative grants to the villages. This need can be met by consulting engineering firms and contractors where funds are sufficient. However, for smaller projects a little common sense advice and direction is frequently all that is required, rather than a costly and lengthy study which doesn't help the local governing body with the day to day problems associated with doing its own construction projects. Although not mandated to do so, LSR&T has been providing technical information, quantity and cost estimates, and alternative recommendations to villages seeking guidance for the application of their direct grants for new construction equipment, new equipment storage buildings, and local transportation-related construction projects. The Nome office has handled seventeen such calls from different villages in the last five months.

IX. SUMMARY

Given the limits of available funding for rural areas, the LSR&T Program has made some substantial contributions in helping local communities to meet their own transportation needs. By and large, remote communities have not requested advanced development projects. Basic projects have been the rule, providing boardwalks, access roads, or streets for the community, frequently for the first time. Additionally, the Program has received strong support from the Bering Straits and NANA Regional Native Corporations, in the form of streamlined permit procedures, free use materials, and expedited right-of-way reconveyance agreements. Finally, the LSR&T Program has retained the flexibility to fund those projects requested by villages which have not been addressed through the legislative process under line-item appropriations or specific grants. Hopefully this information will assist in the evaluation of the LSR&T Program as it has functioned in the Nome Residency, and contribute to the formation of a policy or program through which rural communities may continue to receive State support in alleviating their transportation problems.

THREE YEAR LOCAL SERVICE ROADS AND TRAILS PROGRAM

1981 1982 1983



STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES

JAY S. HAMMOND
GOVERNOR



STATE OF ALASKA
OFFICE OF THE GOVERNOR
JUNEAU

January 1, 1981

Fellow Alaskans:

This document outlines the 1981, 1982 and 1983 Local Service Roads and Trails Program. Included are project locations, descriptions and estimated costs.

It should be noted this is a planning document of the Department of Transportation and Public Facilities and as with all plans, changing conditions may require modification of the plans in the later years.

All of these projects are in conformance with requests by local governments. Nearly all projects in last year's program for 1980 are complete.

Your extraordinary support of local service roads and trails gives me great pleasure as I submit this, your Three Year Local Service Roads and Trails Program.

Sincerely,

A handwritten signature in black ink, appearing to read "Jay S. Hammond".

Jay S. Hammond
Governor

NARRATIVE

LOCAL SERVICE ROADS & TRAILS PROGRAM

The Local Service Roads and Trails Program provides for construction of local roads and trails which are not included in the federal-aid highway system.

Under the Local Service Roads & Trails Act, funds for home rule cities, municipalities and organized boroughs are allocated by strict formula. These local governments then establish their own project scheduling; therefore, this publication is merely a listing of their individual programs as submitted. Project priorities and scheduling in the unorganized boroughs are based primarily on requests and recommendations of village councils and residents. Personal contact with the villages is established by LSR&T representatives from the Department's regional offices. Village needs are thus determined first-hand, along with approximate costs. Since the cost of requested projects usually exceeds the funds available, the priorities must be established based on need. Local governments often participate in projects costs with their own funds.

Local residents and equipment are utilized wherever practical in the construction of these projects. Not only is this beneficial to the economy of the villages, but it normally reduces the overall cost of the project. A side benefit is the training of local residents who will, in many cases, be maintaining the completed project.

This three years LSR&T Program is based on remaining funds from previous allocations and the 1980 Transportation Facilities bond issue of \$10,000,000 for fiscal years 1981 and 1982.

Funding shown in this program anticipates using all of these funds by the end of calendar year 1982. Therefore, projects shown for calendar year 1983 only can proceed if additional funds are made available.

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STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 1

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1981			
MUNICIPALITY OF ANCHORAGE	Commercial Drive-3rd Ave., to Mt. View Dr.	1.	323,800
	Arctic Blvd. 44th to International	2.	291,400
	Hillside & Girdwood Community Improvements	3.	101,400
	Eagle River - Chugiak Improvements	4.	252,400
	Bike Trails - Anchorage	5.	68,400
	Bike Trails - Girdwood/Hillside	6.	30,300
	Bike Trails - Eagle River/Chugiak	7.	22,400
KENAI-PENINSULA BOROUGH	Longmere Lake Access Road	8.	150,000
	Murwood Drive	9.	50,000
	Bear Creek Road	10.	200,000
	Base Road	11.	75,000
MATANUSKA-SUSITNA BOROUGH	Lucille St. - Phase I	12.	55,000
	Welch Road	13.	29,500
	King Arthur Drive (Houston)	14.	112,000
	Smith Drive	15.	102,000
KODIAK-ISLAND BOROUGH	Lilly Drive	16.	91,300
CITY OF KENAI	Main Street Loop - Phase I	17.	60,200
CITY OF KODIAK	Mill Bay Road	18.	24,100
CITY OF PALMER	Funds Reserved for Calendar Year 1982 & 1983		
CITY OF SEWARD	City Streets	19.	16,900
UNORGANIZED BOROUGH 1			
Chenega	Village Streets	20.	117,500

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 1

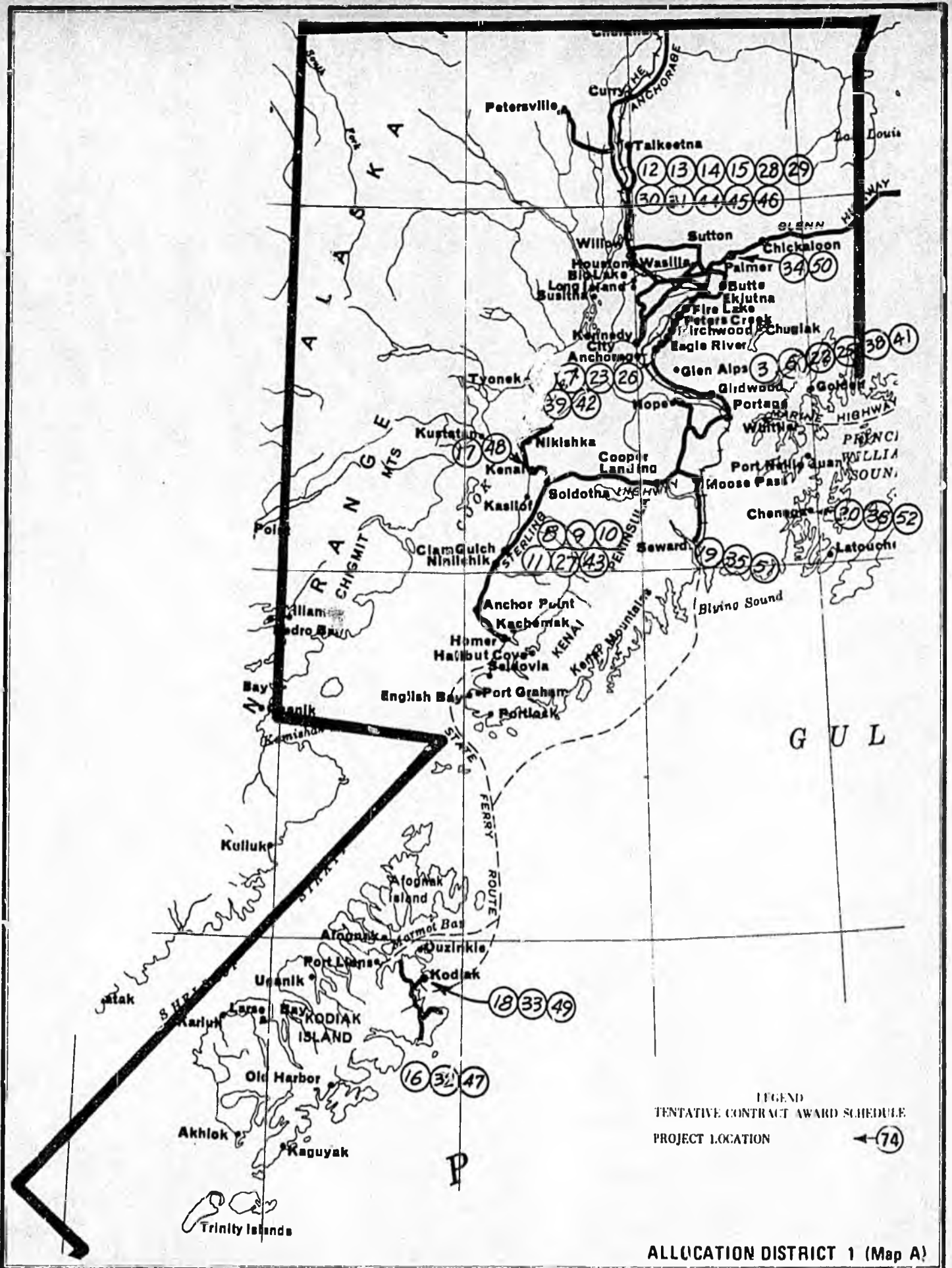
PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1982			
MUNICIPALITY OF ANCHORAGE	36th Avenue-Arctic to Spenard	21.	291,500
	Hillside/Girdwood Community Improvements	22.	129,200
	Eagle River/Chugiak Community Improvements	23.	95,500
	Bike Trails/Anchorage	24.	32,400
	Bike Trails/Girdwood & Hillside	25.	14,400
	Bike Trails/Eagle River & Chugiak	26.	10,600
KENAI-PENINSULA BOROUGH	Feuding Lane	27.	36,000
MATANUSKA-SUSITNA	Palmer West	28.	75,000
	Jupiter Street	29.	20,000
	Bridge-Wilderness East Subdivision	30.	30,000
	Jim Creek Trail	31.	135,400
KODIAK-ISLAND BOROUGH	Womans Bay Drive	32.	91,300
CITY OF KENAI	Funds Utilized in 1981		
CITY OF KODIAK	Ersleine, Wilson & Carolyn Road Improvements	33.	23,100
CITY OF PALMER	North Alaska Street Paving	34.	66,700
CITY OF SEWARD	City Streets	35.	13,500
UNORGANIZED BOROUGH 1			
Chenega	Village Streets	36.	117,500

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

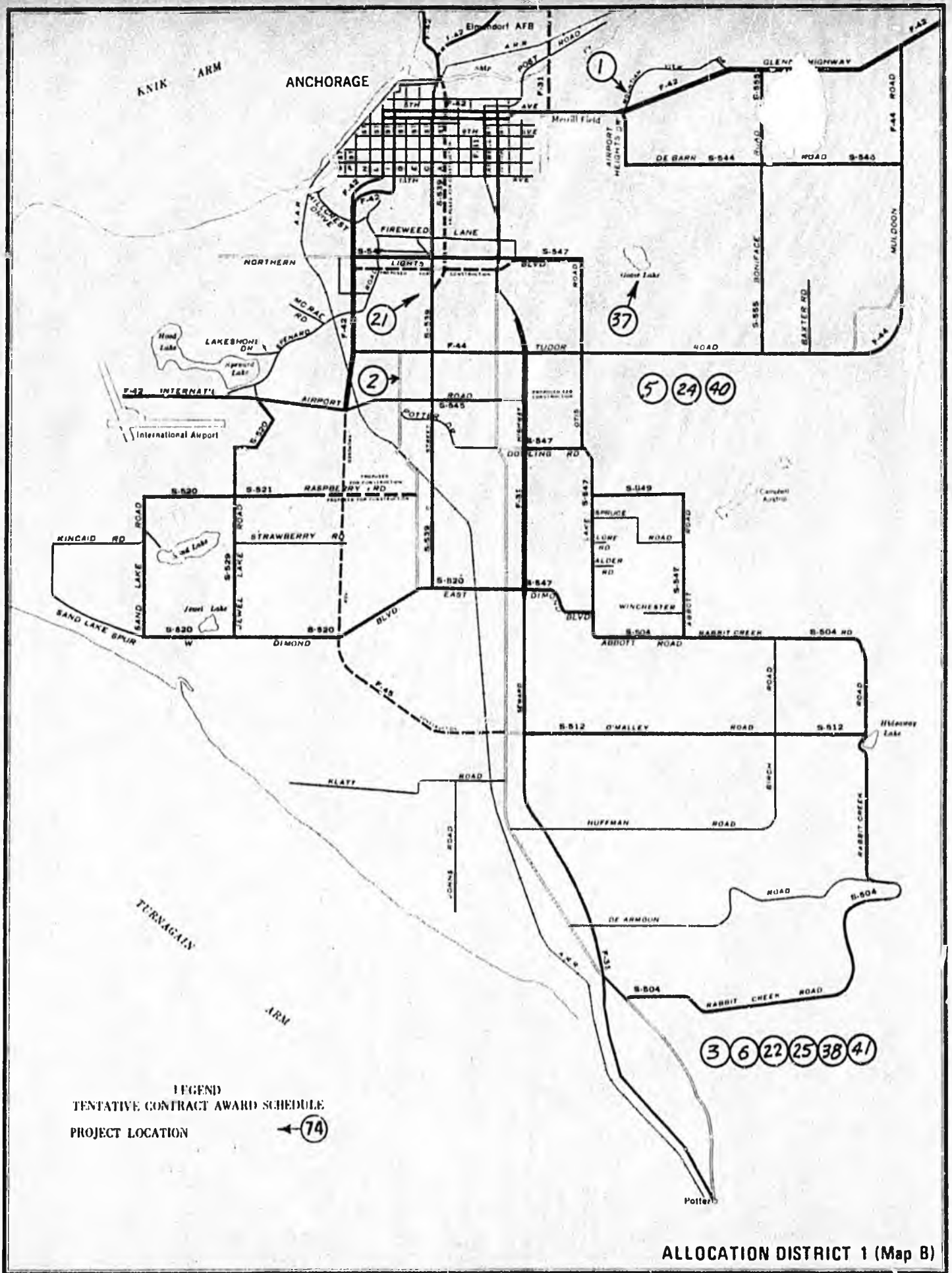
Allocation District 1

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1983*			
MUNICIPALITY OF ANCHORAGE	Providence Drive - University to Northern Lights	37.	291,500
	Hillside/Girdwood Community Improvements	38.	129,200
	Eagle River/Chugiak Community Improvements	39.	95,500
	Bike Trails - Anchorage	40.	32,400
	Bike Trails - Girdwood & Hillside	41.	14,400
	Bike Trails - Eagle River & Chugiak	42.	10,600
KENAI-PENINSULA BOROUGH	Lewis Lane	43.	224,000
MATANUSKA-SUSITNA BOROUGH	Lucille St. - Phase II	44.	220,000
	Phile Spees	45.	23,500
	Various Locations Access Roads & Streets	46.	35,900
KODIAK-ISLAND BOROUGH	Bayview Drive	47.	91,300
CITY OF KENAI	Main Street Loop Phase I	48.	30,100
CITY OF KODIAK	Baranof Avenue - 6th to 10th	49.	23,100
CITY OF PALMER	North Gulkana Street - Paving	50.	105,100
CITY OF SEWARD	City Streets	51.	13,500
UNORGANIZED BOROUGH 1			
Chenega	Village Streets	52.	117,500

* Calendar Year 1983 is unfunded with present allocations.



ALLOCATION DISTRICT 1 (Map A)



STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 2

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1981			
FAIRBANKS NORTH STAR BOROUGH	Service Area Roads	1]	
	Badger-Dennis Intersection	2]	
	Badger Road Bike Path	3]	650,000
	Chena Hot Springs Road Trail	4]	
	Hagelbarger Avenue Extension	5]	
	Holmes Road - Dennis to Badger	6]	
NORTH SLOPE	Point Lay - Community Roads	**7	310,000
CITY OF FAIRBANKS	No Program Submitted		464,400
CITY OF NORTH POLE	no Program Submitted		1,500
UNORGANIZED BOROUGH II			
HEALY/ANDERSON/NENANA	Local Service Roads	8]	
EUREKA	Vehicle Bridge @ Boston Cr.	9]	
BEAVER	Village Streets	10]	
EVANSVILLE	Village Streets	11]	1,370,000
EAGLE	Access Road	12]	
TETLIN	Access Road	13]	

** Refer to Map A Allocation District 4

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 2

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1982			
FAIRBANKS NORTH STAR BOROUGH	Arterial Streets Service Area Roads	14.] 15.]	351,000
NORTH SLOPE	Atkasook - Road to Airport	** 16.	290,000
CITY OF FAIRBANKS	No Program Submitted		125,000
CITY OF NORTH POLE	No Program Submitted		1,400
UNORGANIZED BOROUGH II			
CENTRAL NORTHWAY RAMPART STEVENS VILLAGE	Vehicle Bridge @ Miller Creek Village Streets Mining Access Road Village Streets	17.] 18.] 19.] 20.]	488,600

** Refer to Map A Allocation District 4

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 2

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1983 *			
FAIRBANKS NORTH STAR BOROUGH	Arterial Streets	21.]	351,000
	Service Area Roads	22.]	
NORTH SLOPE	No Program Submitted		289,300
CITY OF FAIRBANKS	No Program Submitted		125,400
CITY OF NORTH POLE	No Program Submitted		1,500
UNORGANIZED BOROUGH II			
DELTA AREA	Community Roads	23.]	502,000
FORT YUKON	City Streets	24.]	
VARIOUS LOCATIONS	Trails & Access Roads	25.]	

* Calendar Year 1983 is unfunded with present allocations.

LEGEND
 TENTATIVE CONTRACT AWARD SCHEDULE
 PROJECT LOCATION

← 74



ALLOCATION DISTRICT 2 (Map A)

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 3

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1981			
CITY & BOROUGH OF SITKA	Price Street Sidewalks	1 } 2 }	100,000
KETCHIKAN GATEWAY BOROUGH	Animal Shelter Access	3	20,000
CITY OF KETCHIKAN	Shoenbar Trail Lighting	4	12,500
	Cook Street Drainage	5	6,000
	4 1/2 Alley Drainage	6	13,100
	Tower Road Drainage	7	12,300
CITY OF WRANGELL	Link Road	8	10,000
	Case Avenue	9	20,000
	Cemetary Access Road	10	2,500
CITY OF PETERSBURG	City Streets or downtown parking	11	22,000
HAINES BOROUGH	Paving local streets	12	22,800
CITY & BOROUGH OF JUNEAU	Davis Avenue	13	40,000
HOONAH	Drainage on Lumbago Street	14 }	
KAKE	Cemetary Access Road	15 }	
PELICAN	Mussel Street - Phase II	16 }	
KLAWOCK	Roadway Drainage	17 }	240,000
MEYERS CHUCK	Service Trails	18 }	
SKAGWAY	Sidewalk	19 }	
ELFIN COVE	Boardwalk	20 }	
VARIOUS LOCATIONS	Road & Trails	21 }	

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 3

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1982			
CITY & BOROUGH OF SITKA	Upgrade Dodge Circle Sidewalks	22] 23]	100,000
KETCHIKAN GATEWAY BOROUGH	Access to subdivisions	24	86,700
CITY OF KETCHIKAN	Funds utilized in Calendar Year 1981		
CITY OF WRANGELL	McKinnon Street	25	22,000
CITY OF PETERSBURG	City Streets or parking	26	22,000
HAINES BOROUGH	Front Street	27	22,800
CITY & BOROUGH OF JUNEAU	Tongass Avenue and Nancy Drive	28	146,000
KUPREANOF	Petersburg Creek to Petersburg Lake Trail	29]	
KAKE	Second Street Bulkhead	30]	
TENAKEE	Service Trail	31]	
CRAIG	Beach Road	32]	240,000
YAKUTAT	Bulkheads on Bayview Drive	33]	
KASAAN	Drainage on Main Road, Phase I	34]	
HYDABURG	Drainage & Surfacing	35]	
VARIOUS LOCATIONS	Roads & Trails	36]	

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 3

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1983*			
CITY & BOROUGH OF SITKA	Sidewalks	37	65,900
KETCHIKAN GATEWAY BOROUGH	No Program Submitted		41,000
CITY OF KETCHIKAN	Denali Avenue Drainage	38	8,900
	Alaska Avenue Drainage	39	13,100
CITY OF WRANGELL	Church St. from Zimovz to Case Avenue	40	42,800
CITY OF PETERSBURG	City Streets or parking	41	21,800
HAINES BOROUGH	Fort Wm. H. Seward Boardwalk	42	22,800
CITY & BOROUGH OF JUNEAU	Radcliff Road	43	93,700
KAKE	Roadway Drainage	44]	
YAKUTAT	Access Road to Monit Bay Heights	45]	
CRAIG	6th Street, Main to Beach	46]	240,000
KASAAN	Drainage on Main Road, Ph. I	47]	
VARIOUS LOCATIONS	Roads & Trails	48]	

* Calendar Year 1983 is unfunded with present allocations.



ALLOCATION DISTRICT 3

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 4

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1981			
BRISTOL BAY BOROUGH	Savonoski Road - Phase III	1	110,200
UNORGANIZED BOROUGH IV			
CENTRAL REGION			
AKOLMUIT	Boardwalk	2]	
ANIAK	Access Road	3]	
ATKA	Streets & Vehicle Bridge	4]	
ATMAUTHLUAK	Boardwalk	5]	
IVANOFF BAY	Boardwalk	6]	
KOLIGANIK	Streets & Vehicle Bridge	7]	695,000
PERRYVILLE	Vehicle Bridge	8]	
SCAMMON BAY	Access Road	9]	
TULUKSAK	Boardwalk	10]	
TUNTU-TULIAK	Boardwalk	11]	
INTERIOR REGION			
KALTAG	Streets	12]	
RUBY	Streets	13]	240,000
WESTERN REGION			
ANVIK	Streets	14]	
EMMONAK	Vehicle Bridge	15]	
GRAYLING	Streets	16]	660,000
HOLY CROSS	Streets	17]	
LITTLE DIOMEDE	Foot Trail	18]	
NOORVIK	Streets	19]	
NULATO-KOYUKUK	Winter Trail Staking	20]	
GALENA			

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 4

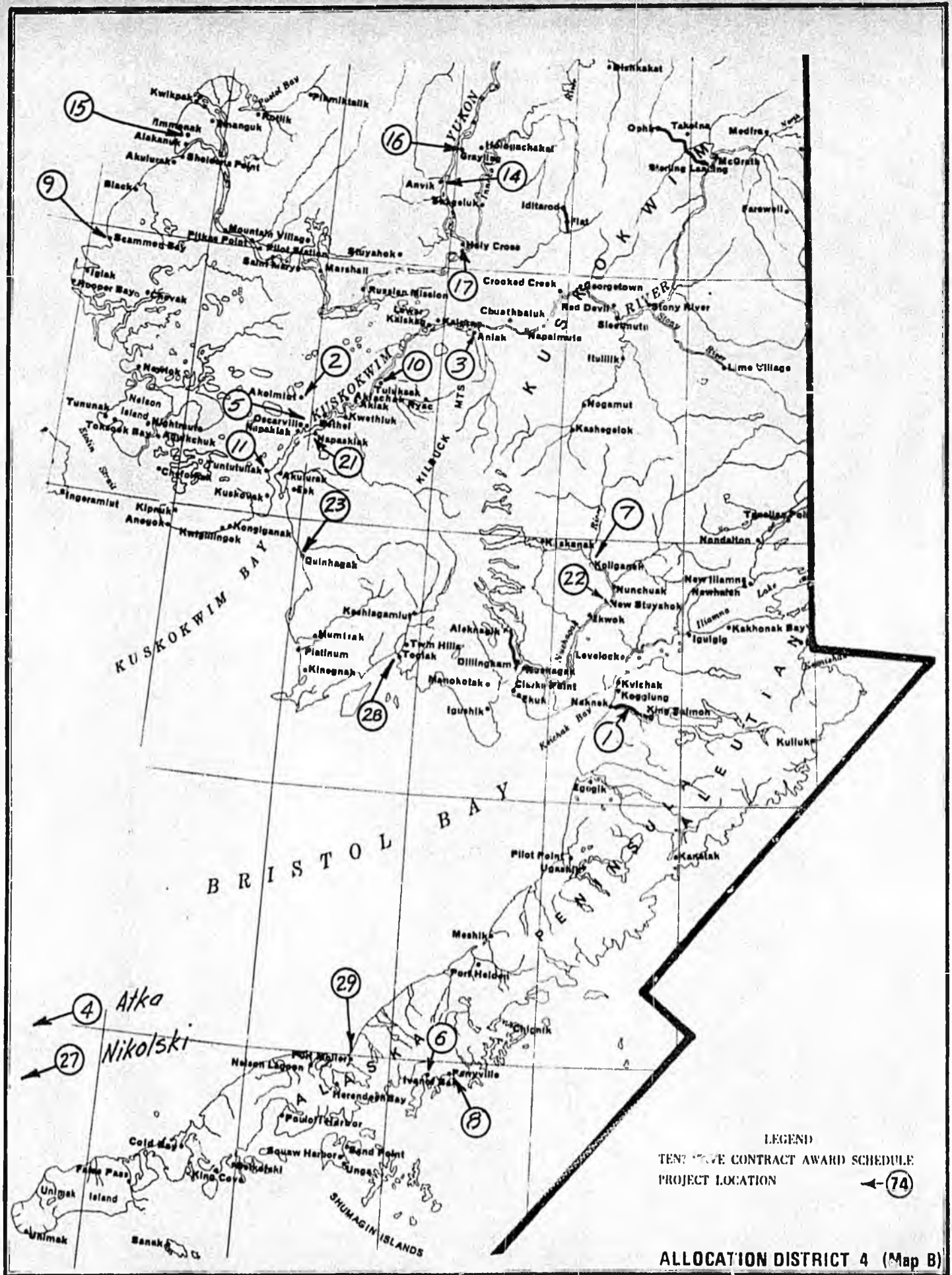
PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1982			
BRISTOL BAY BOROUGH	Funds Utilized in Calendar Year 1981		
CENTRAL REGION			
NAPAKIAK	Boardwalk	21]	
NEW STUYAHOK	City Streets	22]	203,000
QUINHAGAK	Boardwalk	23]	
INTERIOR REGION			
GALENA	Community Roads	24	62,000
WESTERN REGION			
SAVOONGA	Access Road	25]	
NULATO-KOYUK GALENA	Winter Trail Staking	26]	120,000

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

Allocation District 4

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1983*			
BRISTOL BAY BOROUGH	No Program Submitted		22,400
UNORGANIZED BOROUGH IV			
CENTRAL REGION			
NIKOLSKI	Access Road	27]	
TOGIK	City Streets	28]	420,000
USASHIK	Village Streets	29]	
INTERIOR REGION			
HUGHES	City Streets	30]	
HUSLIA	City Streets	31]	111,800
WESTERN REGION			
BREVIG MISSION	Boardwalk	32]	
KIANA	Access Road	33]	
KOTZEBUE	Access Road	34]	395,000
WALES	Access Road	35]	
YUKON-KOBUK	Winter Trail Staking	36]	

* Calendar Year 1983 is unfunded with present allocation.



LEGEND
 TEN? VE CONTRACT AWARD SCHEDULE
 PROJECT LOCATION ← (74)

ALLOCATION DISTRICT 4 (Map B)

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

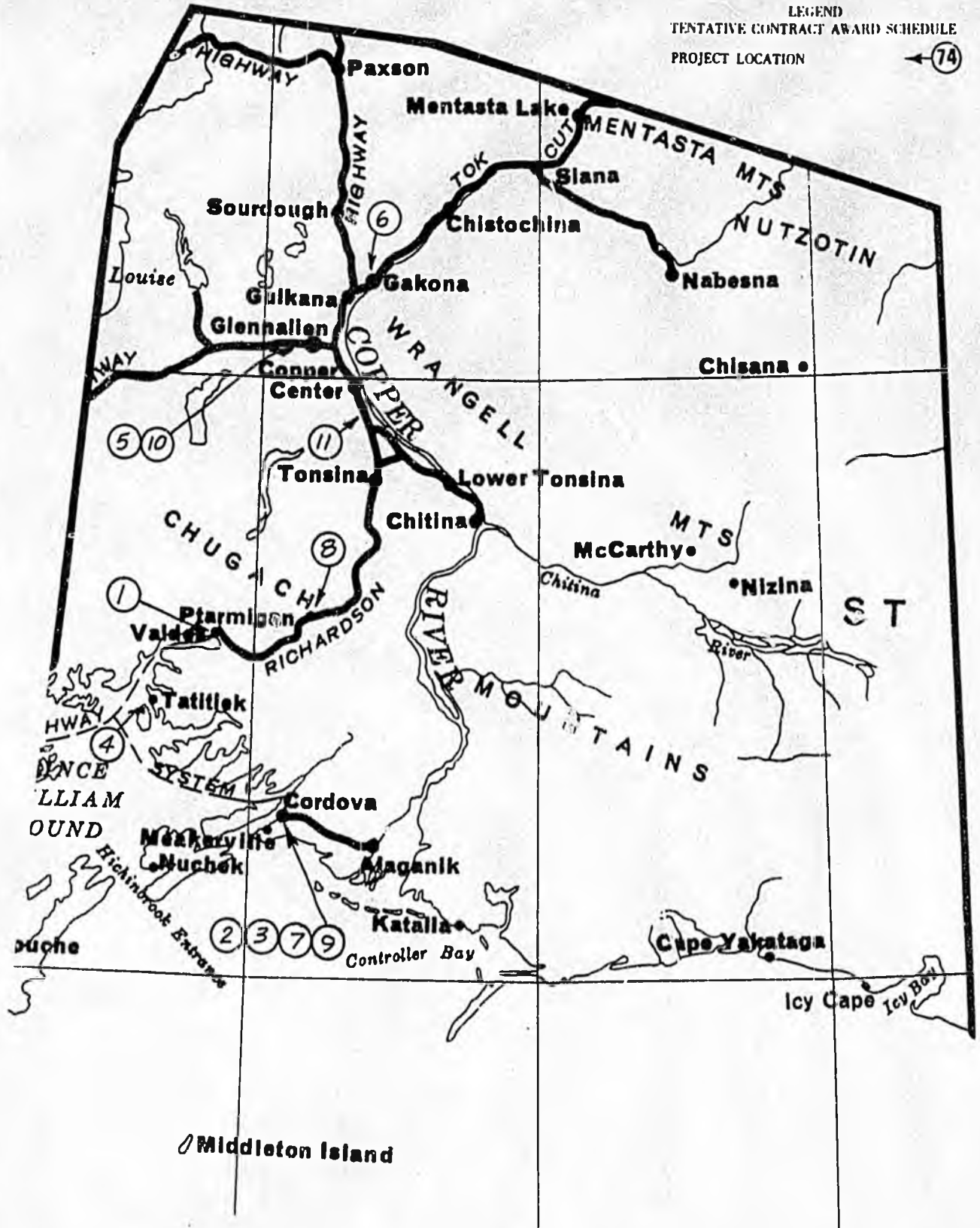
Allocation District 5

PROJECT LOCATION	PROJECT DESCRIPTION	PROJECT NUMBER	LSR&T FUNDS (DOLLARS)
Calendar Year 1981			
CITY OF VALDEZ	West Egan Drive	1.	120,800
CITY OF CORDOVA	First Street Trestle	2.	69,500
	Railroad Avenue	3.	30,500
UNORGANIZED BOROUGH V			
TATITLEK	Airport Access Road	4.]	285,000
GLENNALLEN	School Access Road	5.]	
GAKONA	Foot Bridge	6.]	
Calendar Year 1982			
CITY OF VALDEZ	Fund Utilized in Calendar Year 1981		
CITY OF CORDOVA	Second Avenue Paving, Phase I	7.	30,400
UNORGANIZED BOROUGH V			
Tzaina (Lost Lake)	Lake Access Road	8.	59,800
Calendar Year 1983*			
CITY OF VALDEZ	No Program Submitted		20,900
CITY OF CORDOVA	Second Avenue Paving, Phase II	9.	30,400
UNORGANIZED BOROUGH V			
GLENNALLEN	Village Street - Paving	10]	142,900
TAZLINA	Copperville Subdivision Street Paving	11]	

* Calendar Year 1983 is unfunded with present allocations.

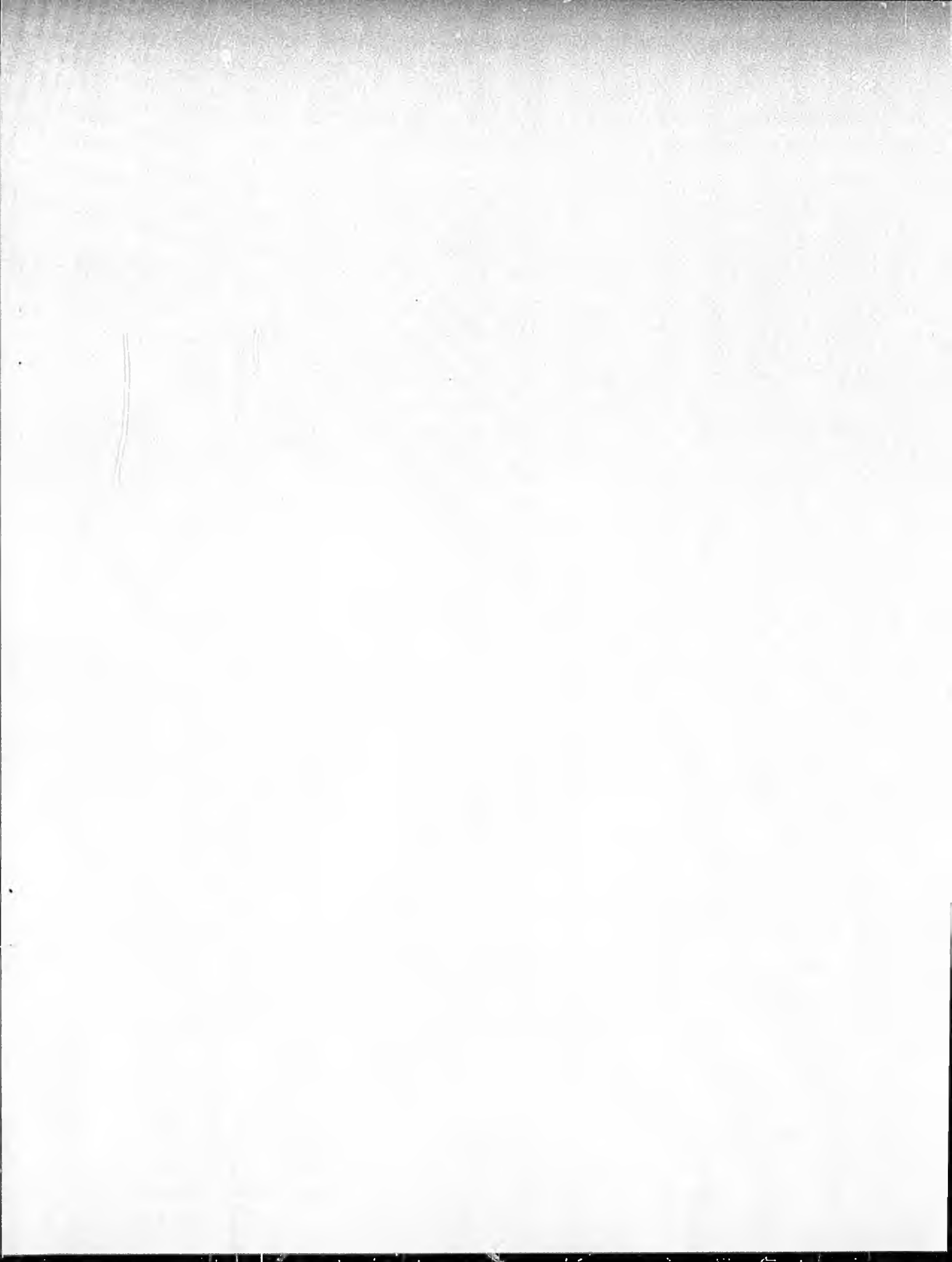
LEGEND
 TENTATIVE CONTRACT AWARD SCHEDULE
 PROJECT LOCATION

74



Middleton Island

ALLOCATION DISTRICT 5



TO: Barbara

FROM: Staff

Subject: HB 169

DOTPF is "supposedly" phasing out the LSR & T program because there is a large share of the monies in this program going directly to grants to communities (see their CIP brown book) and they indicate that they do not need the program for this.

This bill essentially puts \$10 million more into the L S R & T pool and it appears that it is the intent to continue the program.

(Steve (committee staff) suggests that the Legislature should draft a letter of intent or somewhere state their intent with regard to this program; either it should keep going and be funded and not in bits and pieces but wholly or phased out.)

1. planning

2. 10 mil. Zoo Excessed

3. Accountability

B.O.A.

1. Capital Budget

2. Program ?

3. Mithos

4. Bonding

STATE OF ALASKA
PRELIMINARY STATEMENT OF FISCAL IMPACT

Bill No: SB7 Date on Bill: 1/18/83
 Title: An Act making an appropriation to DOTPF for local service roads and trails
 Sponsor: Ferguson, Kerttula, and Moss
 Requestor: _____

1. Estimated fiscal impacts on:

a. Expenditures:

(Thousands of Dollars)

			FY 83	FY 84	FY 85	FY 86		
Capital			10,000.0					
Operating				209.0	218.0			
Total			10,000.0	209.0	218.0			

b. Revenues:

Revenue								
---------	--	--	--	--	--	--	--	--

2. Source of funds to offset fiscal impact of bill:

Not identified by sponsors of bill.

3. Assumptions:

4. Disclaimer:

This statement has not been reviewed by the OMB in the Office of the Governor. It does not represent the policy of the Sheffield Administration or the final estimate of fiscal impact.

Prepared by: Wayne Weeks Phone: 465-4070
 Division: Planning and Programming Date: 2/10/83
 Approved by Commissioner: *W. Weeks* Date: 2/10/83
 Department: Transportation and Public Facilities

Distribution:

- Original to Legislative Finance
- Copy to OMB
- Copy to Sponsor
- Copy to Requestor

2/8/83

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HANDBOOK

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES
LOCAL SERVICE ROADS AND TRAILS

ENGINEER'S HANDBOOK

LOCAL SERVICE ROADS

AND TRAILS PROGRAM

DEPARTMENT OF TRANSPORTATION

AND PUBLIC FACILITIES

STATE OF ALASKA

POUCH Z

JUNEAU, ALASKA

JUNE 1978

NO. _____

INTRODUCTION

This manual became a necessity since the Local Service Roads and Trails program is no longer a pilot program but an integral part of the Alaska Department of Transportation and Public Facilities. This self-sustaining program is designed to accomplish the most with the least amount of State dollars under the rules and regulations of the Alaska State Statutes. Therefore, these guidelines must remain flexible in order that the program can be properly administered.

This manual should be considered a guide for LSR&T Engineers and their subordinates. This guide is not intended to take the place of good engineering judgement nor is it an answer to all situations.

All necessary revisions will be made to this handbook to guarantee the efficiency of the LSR&T Program.

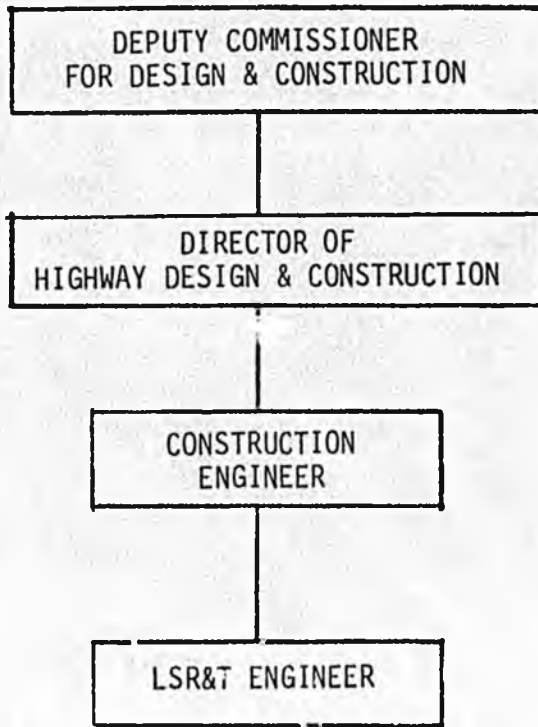
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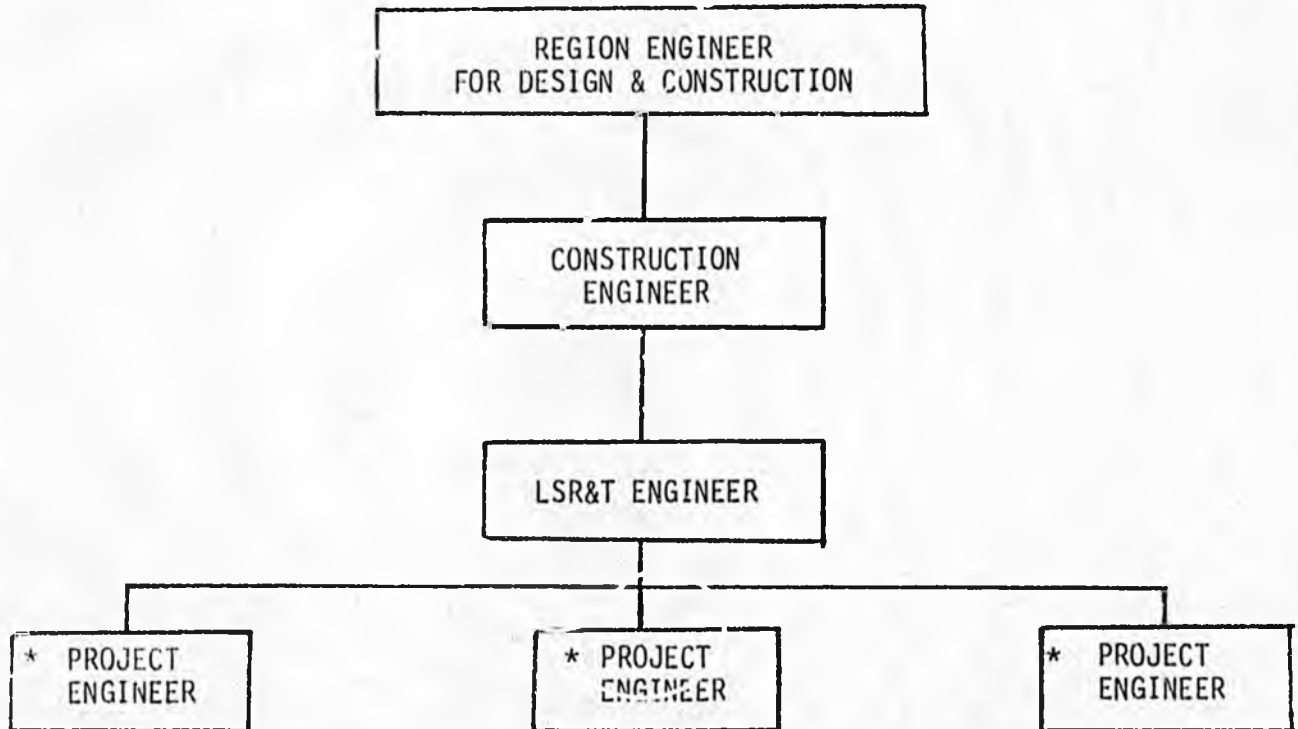
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HEADQUARTERS ORGANIZATION CHART



REGION ORGANIZATION CHART



* Also Roaming Foreman or Appointed Villager

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LOCAL SERVICE ROADS AND TRAILS PROGRAM

Primary Functions:

The Local Service Roads and Trails (LSR&T) program was established by the State Legislature in 1971 to provide for special needs to "bush" communities, rural communities and communities both within and outside of organized boroughs.

Local Service Roads and Trails was designed to aid communities in accomplishing projects that do not have the financial means nor the technical expertise to achieve alone. These projects were to be in the nature of roads, trails, bridges and boardwalks specifically. Special exceptions are considered and granted if justification is sufficient.

The LSR&T Program is wholly State funded. Wherever costs are not prohibitive, generally within organized boroughs, projects are awarded by bid contract. In the remote areas where, because of location and size of the project bid contracts cost are prohibitive, LSR&T maintains its own small fleets of equipment or rents equipment, if available, and directs the operation of the projects.

The scope of LSR&T is basically three-fold. This is particularly true for "bush" projects. It provides:

1. Needed projects to communities that have not the capabilities of accomplishing alone.
2. A source of training for construction work by employing local residents as laborers and equipment operators.
3. Provides supplemental income to communities that are single-income or subsistence oriented.

Project Limitation

Local Service projects are confined to roads which are not part of the federal-aid system and are not, the responsibility of corporate units, or private individuals but are dedicated to public use. These projects cover wide ranging road needs. They include State recreation routes, connections between outlying villages and airports, villages and canneries, villages and port facilities, short routes needed to better integrate subdivision roads and State highways, and improvements to existing roads.

Article 3. Local Service Roads and Trails.

Section	Section
111. Purpose	181. Project agreements
121. Regulations	191. Letting of contracts
127. Allocation districts	201. Construction
131. Allocation of funds	211. Maintenance
141. Construction programs	221. Availability of allocated sums
151. Local government participation	231. Payment for construction
161. Design standards	241. Definitions
171. Acquisition and conveyance of rights-of-way	251. Local government powers

Repeal of former article.—Section Roads." The former article consisted of §§ 19.30.110--19.30.170 and derived from § 5, ch. 129, SLA 1968.

Sec. 19.30.111. Purpose. It is the purpose of §§ 111—251 of this chapter to provide for the construction of local service roads and trails which are not included in the approved federal-aid primary or secondary highway systems eligible for federal-state matching funds. (§ 2 ch 84 SLA 1971)

Sec. 19.30.121. Regulations. The commissioner may make regulations necessary to carry out §§ 111—241 of this chapter. (§ 2 ch 84 SLA 1971)

Sec. 19.30.127. Allocation districts. The state is divided into five allocation districts as follows:

(1) The boundary of allocation district one begins in Kamishak Bay at 59°15' N, 154°00'W; and travels north along longitude line 154°00'W to latitude 63°00'N; thence east along latitude line 63°00'N to a point common with the boundaries of allocations districts two and five; southerly of the Denali Highway at Corkscrew Creek (Milepost 59.1); thence southerly to Prince William Sound at Fairmount Point 60°53'45"N, 147°27'W and inclusive of Kodiak, Afognak, Barren, Trinity and Chirjkof Islands.

(2) The boundary of allocation district two begins at and includes the point at which the Alaska Highway intersects the Alaska-Yukon border; travels northwesterly to the west abutment of the Little Tok River Bridge on Tok Cutoff Highway (Milepost 91.2); thence west to the northerly abutment of the McCallum Creek Bridge on the Richardson Highway (Milestone 202.4); thence west to a point directly north of the Denali Highway at Corkscrew Creek (Milepost 59.1); thence southwesterly to the Glenn Highway at Summit (Milepost 118) and continuing along that line to latitude $63^{\circ}00'N$; thence west along latitude $63^{\circ}00'N$ to longitude $154^{\circ}00'W$; thence north along longitude $154^{\circ}00'W$ to the Arctic Ocean.

(3) The boundary of allocation district three begins in the Gulf of Alaska on longitude line $141^{\circ}00'W$; thence north along longitude $141^{\circ}00'W$ to Boundary Point 137; thence southeasterly along the Canadian-United States border bounding Southeastern Alaska in its entirety.

(4) The boundary of allocation district four begins in the Arctic Ocean at Smith Bay $70^{\circ}54'N$, $154^{\circ}19'W$; on longitude line $154^{\circ}00'W$; travels south along longitude $154^{\circ}00'W$ to latitude $59^{\circ}15'N$ in Karnishak Bay; thence east along latitude $59^{\circ}15'N$ to longitude $152^{\circ}15'W$; thence southwesterly through Shelikof Strait and bounds of the entire Aleutian Chain.

(5) The boundary of allocation district five begins at the Gulf of Alaska on longitude line $141^{\circ}00'W$ and travels northerly along that line to the point (but not including the point) at which Alaska Highway intersects the Alaska-Yukon border; thence northwesterly to the west abutment of the Little Tok River Bridge on the Tok Cutoff Highway (Milepost 91.2); thence west to the northerly abutment of McCallum Creek Bridge on the Richardson Highway (Milepost 202.4); thence west to the point directly north of Denali Highway at Corkscrew Creek (Milepost 59.1); thence southwesterly to the Glenn Highway at Summit (Milepost 118); thence southerly to Prince William Sound at Fairmount Point $60^{\circ}53'45'N$, $141^{\circ}07'W$. (§ 2 ch 84 SLA 1971)

Sec. 19.30.132. Allocation of funds. (a) During each fiscal year the commissioner shall allocate sums appropriated or otherwise designated for expenditure upon local service roads for that fiscal year among the five allocation districts in the following manner: one-half in the ratio which the area of each allocation district bears to the total area of the state and one-half in the ratio which the population of each allocation district bears to the total population of the state as shown by the latest available federal census.

(b) The commissioner shall also further allocate the sums in each allocation district to the boroughs within each allocation dis-

tract in the following manner: one-half in the ratio which the area of each organized borough (excluding salt water areas) within that district bears to the total area of the allocation district and one-half in the ratio which the population of each organized borough area within that district bears to the total population of the allocation district as shown by the latest available federal census.

(c) The commissioner shall also further allocate portions of the sum allocated to any borough, either organized or unorganized, and to any home rule city within the borough in the following manner: one-half in the ratio which the area of each home rule city bears to the total area of the borough (excluding salt water areas) and one-half in the ratio which the population of each home rule city bears to the total population of the borough as shown by the latest available federal census.

(d) The sums not allocated within each district to a local government under (b) and (c) of this section shall be allocated to the unorganized borough and administered by the department. (§ 2 ch 84 SLA 1971)

Sec. 19.30.141. Construction programs. Before October 1 of each fiscal year each local government eligible for allocation of funds under § 131 of this chapter shall submit to the commissioner for approval a long-range program for the construction of local service roads and trails. Before December 1 of each fiscal year the commissioner shall submit to the governor a long-range program for the construction of local service roads and trails, including the approved local government programs. An organized borough shall include in its long-range program local service road construction programs for all cities other than home rule cities within the boundaries of the borough. The commissioner shall include in his long-range program local service road and trail construction within the unorganized borough. (§ 2 ch 84 SLA 1971)

Sec. 19.30.151. Local government participation. (a) Upon application by a local government, local service roads and trails constructed under the provisions of §§ 111—241 of this chapter and located within the jurisdictional boundaries of a local government, as defined in § 241(3) of this chapter, may be transferred to that local government upon approval by the Department of Highways and after a vote of the people in the area. A local government which has assumed road powers before May 20, 1971 may not be required to hold an election as provided by this section.

(b) The commissioner is authorized to transfer funds to any local government which he determines to be qualified to construct local service roads or trails.

(c) Nothing in §§ 111—241 of this chapter prohibits a local government from participating financially in the construction of a local service road or trail. (§ 2 ch 84 SLA 1971)

Sec. 19.30.161. Design standards. Design standards for each local service road and trail project shall have the prior approval of the commissioner. (§ 2 ch 84 SLA 1971)

Sec. 19.30.171. Acquisition and conveyance of rights-of-way.
 (a) The local government shall acquire all rights-of-way required for local service road and trail construction. However, if the local government is unable to acquire the necessary right-of-way, the commissioner is authorized to acquire it in accordance with AS 19.05.080—19.05.120. The commissioner may convey to the local government, by appropriate instrument executed in the name of the state, any land or interest in land which has been acquired by the state for the construction of local service roads or trails.

(b) The costs incurred by the commissioner in acquiring this land or interest in land includes all costs and any fees incidental to acquisition, including relocation assistance and payments in accordance with AS 34.60. All costs incurred by the commissioner in connection with the acquisition of the land or interest in land shall be paid by the local government for which the land or interest in land is acquired.

(c) When rights-of-way for local service roads are acquired over land or interest in land owned by the state, the commissioner may make such arrangements with the department having jurisdiction over the land as may be necessary to give the local government adequate rights-of-way, and any such department is directed to transfer the jurisdiction of the land to the commissioner. (§ 2 ch 84 SLA 1971; am § 17 ch 71 SLA 1972)

Effect of amendment. — The 1972 amendment substituted "AS 34.60" for "ch. 35 of this title" at the end of the first sentence in subsection (b). **Legislative committee report.**—For report on ch. 71, SLA 1972 (HCSSB 383 am H), see 1972 House Journal, p. 898.

Sec. 19.30.181. Project agreements. As soon as practicable after a specific project has been approved by the commissioner, the commissioner shall enter into a formal project agreement with a qualified local government concerning the construction of the project. (§ 2 ch 84 SLA 1971)

Sec. 19.30.191. Letting of contracts. (a) If the construction is to be performed by the local government, or under its supervision, a request for submission of bids shall be made by advertisement unless some other method is approved by the commissioner. The commissioner shall require plans and specifications and methods of bidding which will be effective in securing competition.

(b) Construction of each project subject to the provisions of this section shall be performed by contract awarded by competitive bidding, unless the commissioner finds that, under the circumstances relating to the project, some other method is in the public interest. Contracts for the construction of each project shall be awarded only on the basis of the lowest responsible bid submitted by a bidder meeting established criteria of responsibility. No requirement or obligation may be imposed upon a bidder as a condition precedent to the award of a contract to a bidder unless the requirement or obligation is otherwise lawful and specifically set out in the advertised specifications.

(c) All contracts by a local government awarded by competitive bidding under (b) of this section must comply with the provisions of this section, and have the prior concurrence of the commissioner. (§ 2 ch 84 SLA 1971)

Sec. 19.30.201. Construction. The construction of a local service road or trail undertaken by a local government or under its direct supervision under the provisions of §§ 111—241 of this chapter, shall be performed according to approved design standards and are subject to the inspection and approval of the commissioner. All construction work done and labor performed by or under the direct supervision of the local government shall be in accordance with the laws of that local government and applicable state law. (§ 2 ch 84 SLA 1971)

Sec. 19.30.211. Maintenance. The department shall maintain, or cause to be maintained, any project constructed under the provisions of §§ 111—241 of this chapter, except that upon mutual agreement of the commissioner and the local government the responsibility for maintenance may be transferred to the local government if it is authorized to assume road maintenance powers. (§ 2 ch 84 SLA 1971)

Sec. 19.30.221. Availability of allocated sums. (a) On and after the date that the commissioner has certified the sums allocated to each local government, the sums shall be available for expenditure by the local government under the provisions of this chapter.

(b) These sums shall continue available for expenditure by that local government for a period of five years after the close of the fiscal year for which the sums are authorized and any amounts so obligated remaining unexpended at the end of that period shall lapse and shall be available for other local service road or trail construction within that allocation district in accordance with §§ 111—241 of this chapter. (§ 2 ch 84 SLA 1971)

Sec. 19.30.231. Payment for construction. (a) The commissioner may from time to time as the work progresses, make payments to

a local government for the total cost of completed construction incurred by it on a project.

(b) After completion of a project and approval of the final voucher by the commissioner, a local government is entitled to payment of the unpaid balance for the project from the sums allocated to it.

(c) No payment under this section may be made except for a project covered by a project agreement. No final payment may be made to a local government for its costs of construction of a project until the completion of the construction has been approved by the commissioner following inspections under § 201 of this chapter.

(d) The payments shall be made to the official or depository as may be designated by the local government and authorized under the laws of the local government to receive public funds of the local government. (§ 2 ch 84 SLA 1971)

Sec. 19.30.241. Definitions. In §§ 111—241 of this chapter

(1) "construction" means the building of a new road, street or trail or the improvement of existing roads, streets or trails and includes the necessary preliminary engineering, construction engineering and utility relocation;

(2) "home rule city" means a city defined in AS 29.05.010;

(3) "local government" means an organized borough of any class or a home rule city;

(4) "local service road" means a public road which is not designated as a route on the approved federal-aid highway system;

(5) "project" means an undertaking to construct a particular portion of a local service road or trail, or, if the context so implies, the particular portion of a local service road or trail so constructed;

(6) "trail" means a footpath or way on land or water that is open to public use as a matter of right whether or not a thoroughfare, particularly for dog sleds and mechanized snow vehicles. (§ 2 ch 84 SLA 1971)

Revisor's note (1972). — AS 1972; however, see AS 29.08.010 on-
29.05.010 referred to in paragraph (2) acted by § 2, ch. 118, SLA 1972 for a
has been repealed by § 1, ch. 118, SLA re-definition of "home rule city."

Sec. 19.30.251. Local government powers. Road construction and maintenance authority is granted to local governments, as defined in § 241(3) of this chapter, which do not otherwise have that authority; however, the authority granted is limited to participation in the construction and maintenance of local service roads under §§ 111—241 of this chapter. (§ 2 ch 84 SLA 1971)

Article 3. Local Service Roads and Trails.

Section

- 151. Local government participation
- 191. Letting of contracts
- 241. Definitions

Sec. 19.30.151. Local government participation. (a) Upon application by a local government, local service roads and trails constructed under the provisions of §§ 111 — 241 of this chapter and located within the jurisdictional boundaries of a local government, as defined in § 241 (3) of this chapter, may be transferred to that local government upon approval by the Department of Transportation and Public Facilities and after a vote of the people in the area. A local government which has assumed road powers before May 20, 1971 may not be required to hold an election as provided by this section.
(am Executive Order No. 39, § 11 (1977))

Effect of amendment. — Pursuant to Executive Order No. 39 (1977), a reference to the Department of Transportation and Public Facilities has been substituted for a reference to the Department of Highways in the first sentence of subsection (a). As the rest of the section was not affected by the amendment, it is not set out.

Sec. 19.30.191. Letting of contracts.

(b) Except as provided in AS 44.33.300, construction of each project subject to the provisions of this section shall be performed by contract awarded by competitive bidding, unless the commissioner finds that, under the circumstances relating to the project, some other method is in the public interest. Contracts for the construction of each project shall be awarded only on the basis of the lowest responsible bid submitted by a bidder meeting established criteria of responsibility. No requirement or obligation may be imposed upon a bidder as a condition precedent to the award of a contract to a bidder unless the requirement or obligation is otherwise lawful and specifically set out in the advertised specifications.
(am § 4 ch 277 SLA 1976)

§ 19.30.241

HIGHWAYS AND FERRIES

§ 19.40.060

Effect of amendment. — The 1976 amendment, effective July 1, 1976, added "Except as provided in AS 44.33.300" to the beginning of the first sentence of subsection (b). As the rest of the section was not affected by the amendment, it is not set out.

Sec. 19.30.241. Definitions. In §§ 111 — 241 of this chapter

(2) "home rule city" means a city as defined in AS 29.08.010;
(am § 30 ch 53 SLA 1973)

Effect of amendment. — The 1973 amendment inserted "as" and substituted "29.08.010" for "29.05.010" in paragraph (2). **Legislative committee report.** — For report on ch. 53, SLA 1973 (CSHB 382), see 1973 House Journal, pp. 793, 885.

As the rest of the section was not affected by the amendment, it is not set out.

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DEFINITIONS

Local Government:

A local government is an organized borough of any class, a home rule municipality or a home rule city.

The local government controls its own LSR&T program. Priority of projects is flexible but changes must be approved by the assembly before a project can be approved by the Department.

Local Governing Body:

A local governing body is the classification of authority in a village or city in the unorganized borough.

Right-of-way and gravel rights must be furnished without reimbursement by the State.

Maintenance responsibility is denoted in the project agreement.

ALLOCATION OF FUNDS

The allocation formula is clearly defined in the Article 3. Local Service Roads and Trails, Section 19.30.111 to 19.30.251 of the Alaska Statutes, but a brief description is included herein for explanatory purposes.

The distribution follows a sequential breakdown and is based on area and population factors. Half the funds are allocated on the basis of area and half on the basis of population. Salt water areas are excluded from allocation areas and the latest Federal Census is used for population figures.

The first stage distribution is made to allocation districts, which correspond to the highway districts existing on June 30, 1971.

Each allocation district's distribution is then broken down and allocated to the boroughs within that allocation district, each organized borough figuring for a share and all areas outside organized boroughs combining together for a share. The important distinction here is that the "borough" is considered an area within an allocation district.

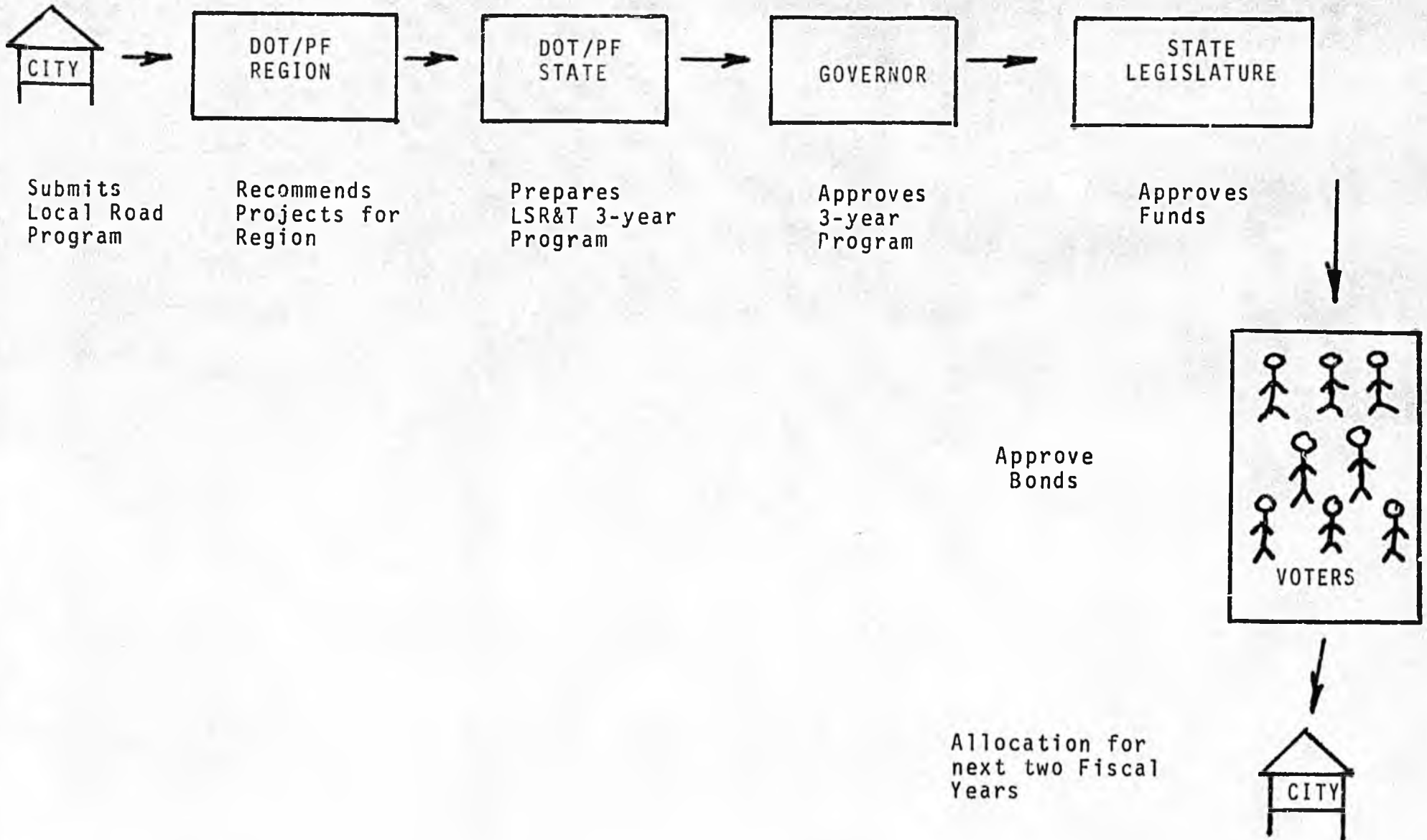
The third and final breakdown allocates to the local government. If a home rule city exists within an organized borough, the borough government and the city government share the "borough" (second stage) allocation in proportion to their area-population factors. If no home rule city exists within the organized borough, the entire allocation is to that borough's government. If a home rule city exists in an unorganized borough, the home rule city's allocation is their area-population proportion of the unorganized borough's allocation.

Using the highway district boundaries existing on June 30, 1971, the allocation is as follows:

<u>Allocation District</u>	<u>Corresponding Highway District</u>	<u>Allocation Factor</u>
I	Central Region	.306486
II	Interior Region	.249049
III	Southeastern Region	.104827
IV	Western Region	.296550
V	Southcentral Region	.043088
		<u>1.000000</u>

ALLOCATION OF LSR&T FUND TO LOCAL GOVERNMENT

2-3



TYPICAL LOCAL GOVERNMENT LSR&T PROJECT

IN AN ORGANIZED BOROUGH OR HOME RULE CITY

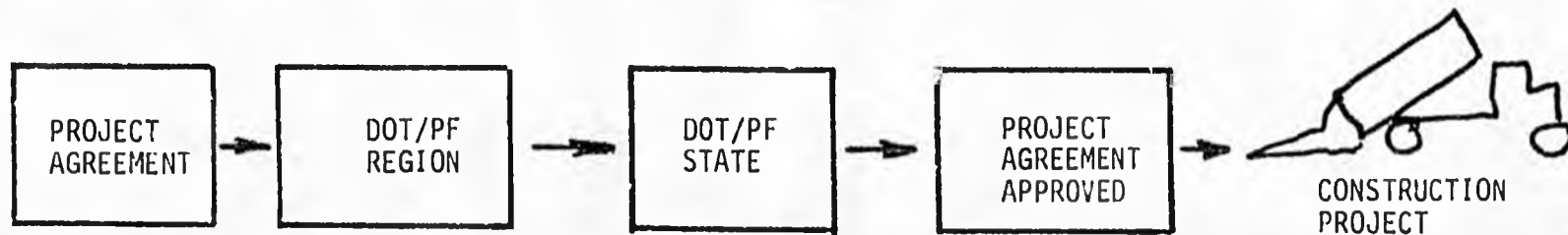


LSR&T
Guy from
Region

Mayor
or his
Representative

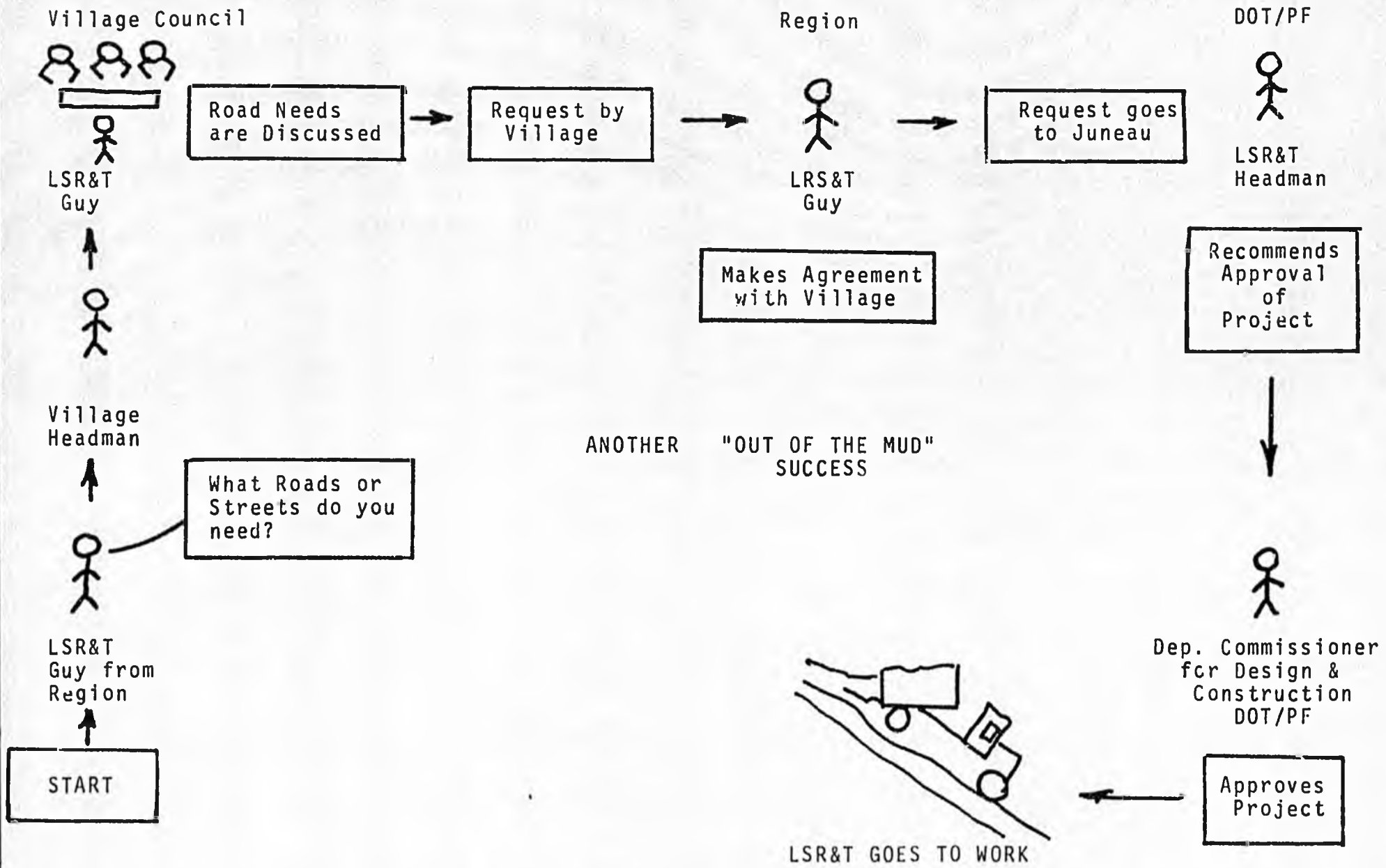
1. Local government submits a construction program to the Region by September 15 of each year.
2. The Regional LSR&T Engineer reviews the program and coordinates with the city in listing projects within funds available and proposed.
3. The local government and the State sign an agreement for reimbursement for each project according to priority.
4. The agreement reflects whether the work is by contract or by local government forces.
5. Maintenance responsibility is included in the agreement.

All phases of the project such as Consultant Engineering, Design Advertising and Letting of a Contract requires approval by DOT/PF. Coordination between the City and the LSR&T man from the Region makes the procedure fast and effecient.



TYPICAL RURAL COMMUNITY LSR&T PROJECT

2-5



APPLICANT GUIDELINES

Nature and Purpose of Program:

To provide State assistance in the development of roads and trails on routes that are not eligible for federal-aid matching funds.

Who May Apply:

Each organized borough and home rule city is eligible for a direct allocation, based on their population and area. All other areas are considered unorganized boroughs and are eligible for projects administered by the Department, subject to priority scheduling within the respective allocation districts.

Authorizing Legislation:

AS 19.30.111-251

Administrative Agency:

Department of Transportation and Public Facilities, State of Alaska.

Type of Assistance:

Financial and Engineering.

How Program is Financed:

100% State. Organized boroughs and home rule cities may supplement their allocation with local funding in any amount. Unorganized borough areas will be encouraged to provide local participation whenever possible. Federal and State grants can be used in conjunction with LSR&T funds.

How To Apply:

Each organized borough and home rule city may receive their allocation by submitting a three-year project program for the construction of such roads to their Regional Highway Engineer by October 1 of each year, indicating the priorities for construction on the program. When a project is approved, a project agreement will be drafted, and development of the project may proceed. No payment can be made for work done prior to execution of the project agreement.

Unorganized borough communities should provide input data for road needs in their areas to their Regional Highway Engineer, who will compile a three-year program and establish priorities for the unorganized borough projects within his jurisdiction. When a project is approved, the Department will proceed with development of the project.

For More Information Contact:

Commissioner
Department of Transportation and Public Facilities
State of Alaska
Pouch Z
Juneau, Alaska 99811

Field Offices:

Central Regional Highway Engineer, Pouch 6900, Anchorage, AK 99502
Interior Regional Highway Engineer, 2301 Peger Road, Fairbanks, AK 99701
Southeastern Regional Engineer, P.O. Box 3-1000, Juneau, AK 99802
Western Regional Highway Engineer, P.O. Box 1048, Nome, AK 99762
Southcentral Regional Highway Engineer, P.O. Box 507, Valdez, AK 99586

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PLANNING

Three Year Program

The allocation districts are the boundaries of the Highway Department established in 1970. The larger of the five districts being the Western District, engulfing nearly half of the land mass of Alaska, is administered by the Western, Central and Interior Regions. This split jurisdiction is strictly an economic measure.

Although the North Slope Borough area is half in the Western Region, to focus administration to one locale, Interior Region has full jurisdiction. Other organized boroughs whose boundaries partially overlap allocation districts, the responsibility lies with the primary region.

A construction program utilizing LSR&T construction equipment, State employees, village labor, equipment rental, when available, is the primary mode of operation in the unorganized boroughs of the Central, Interior and Western Regions. Programming the projects in the unorganized boroughs depends on requests from local governing bodies (both submitted and solicited), available road building materials, right-of-way access, and coordination of other projects in the area.

In the unorganized borough of Southcentral and Southeast, State WCF equipment and maintenance personnel are utilized when a contract for the work is not feasible.

The Local Service Roads and Trails Program is a vital link in the State road network. It is the only State program established for solving local transportation needs. Reflecting a typical construction season, several miles of new road, new and reconstructed boardwalk, year around foot trails, winter trail staking, foot bridges, paved bike paths and vehicle bridges are constructed with LSR&T funds.

These and other special projects are completed each year in answer to what the LSR&T is all about, "Get the people out of the mud". This program does a lot for all the people of Alaska and is presently performing a service the people of Alaska could not do without.

Local governments throughout the State share 52.8% of each LSR&T bond issue. The unorganized boroughs share in the remaining 47.2%. Annual expenditures average this percentage.

Summary

I. Definition

A. Purpose

- a. To get people out of the mud
- b. Flexibility of Program
- c. To aid communities with no revenue
- d. Economy of operation

B. Type of Projects

- a. Local Service Roads
- b. Year around foot trails
- c. Winter trail staking
- d. Bicycle paths
- e. Erosion control
- f. Foot bridges
- g. Boardwalks

C. Type of Funding

- a. LSR&T
- b. Local Government Participation
- c. EDA (Public Works Act and individual grants)

II. Allocation

- A. Two Year Legislation (Amounts vary)
- B. Five Year duration
- C. 50-50 Factor, Area - Population
- D. Operating Budget - Program Support

- a. Bridge Design Headquarters
- b. Road Design and Adv. (Headquarters and Region)
- c. R/W Certification Region
- d. Construction Review Headquarters and Region
- e. Administration Headquarters and Region

E. FY 77-78 Allocation

Organized Boroughs and Home Rule Cities	3,695,721
Unorganized Boroughs	3,304,279
	<u>7,000,000</u>

III. Administration

- A. Allocation District - Regional Control
- B. Organized Boroughs and Home Rule Cities

- a. Construction contract by Local Government
- b. Local Government Labor and Equipment

C. Unorganized Boroughs

- a. Construction Contract by Local Government or State
- b. Village or City Labor and Equipment
- c. State Equipment and Village Labor
- d. State Equipment and State Employees
- e. State Equipment and Village State Employees

IV. Three Year Program

- A. Project Selection

- a. Organized Boroughs and Home Rule Cities
 1. Select their own projects
 2. Able to change program at will
- b. Unorganized Boroughs
 1. Cities and Villages select their own projects.
 2. Region sets priorities within funds available.
 3. Regions may subfill projects with funds available on non-federal routes.

Programming Guidelines

1. Programming of projects in the Matanuska-Susitna Borough and the Kenai Borough are made through the Central Region Office, regardless of the limits of their respective borough boundaries. Programming of projects in the North Slope Borough are made through the Interior Region Office. The administration of construction and maintenance will be performed by the region in which the project is located.
2. Each project (i.e., completed improvement) must provide a usable facility with the expenditure of available funding.
3. Each local service road project submitted for inclusion in the program shall indicate the standards of design for the improvement. This information shall include the following:
 - a. Minimum AASHTO design load for structures.
 - b. A brief narrative description.
 - c. Type and character of traffic.
 - d. Typical section(s) of the proposed improvement(s), and
 - e. Length of project to the nearest tenth of a mile.
4. The program should show priority scheduling for construction, estimated total project construction costs and utilization of allocated funds.
5. The time limit for expending allocated funds is within five years after the close of the year in which the funds were authorized. Local governments may thus broaden the scope of a project and provide for adequate funding by permitting their allocations to accumulate. Program submissions should note this intention, if applicable.
6. Estimated project construction costs must include all costs, including administration, preliminary design engineering and construction engineering.
7. Any portion of the project construction costs may be funded by the local government. For all projects constructed with participated funding by the local government, the determination of the responsibility for maintenance will be made on a project by project basis.

8. The Department may assist the local government in the acquisition of right-of-way, but that all costs for right-of-way, including administrative costs, must be borne by the local government.
9. Roads presently on the State maintenance system may be improved under the Local Service Roads and Trails Program.
10. The highway regions will assist the local governments, if requested, in the preparation of the three-year program, and offer guidance for programming projects within the scope of available funding. All local government programs will be assimilated by their respective highway regions.
11. Preliminary engineering considerations for larger projects shall include studies of possible alternates.
12. A determination will be made on a project by project basis as to whether the local government will administer the construction of the project with reimbursements for costs from available allocated funding, or whether the Department will administer the construction of the project.
13. An executed project agreement is required prior to the expenditure of any sums which are to be included in reimbursement for the costs of the road or trail improvements. All project agreements will stipulate maintenance responsibilities for the completed improvement.
14. Any supplemental information which may assist the local government in the formulation of their three-year Local Service Road and Trail Program will be provided by the Department of Transportation & Public Facilities.
15. The three-year programs, including the priority scheduling, submitted by each local government shall bear a certification of approval by their respective governing body.

Region Outline for Annual LSR&T Program

To better formulate the regions annual three-year program, the ensuing guidelines should be followed:

1. Retain an ongoing program of assimilating information from all local entities throughout the year to eliminate last minute delays.
2. Follow previous program projections insofar as possible and reasonable.
3. Keep the overall allocation within the projected level of funding.
4. Keep the number, size, and type of projects (boardwalks vs. road-work) within the equipment, manpower, and fiscal capabilities.
5. Schedule projects which complement other transportation facility construction.

6. Schedule projects which take advantage of contractors mobilized in remote locations.
7. Spread the work as evenly as possible and avoid concentration of work in one area.
8. Provide a logical and logistical sequence of work to make full use of the equipment.
9. Take advantage of federal programs providing additional transportation funding whenever possible (this increases the effectiveness of the LSR&T Program exponentially).
10. Consider inflation and unforeseen cost increases.
11. Provide timely reaction to local requests.
12. Leave room for flexibility.

Contacting Local Authorities

Local Governments:

Road needs are requested by the State LSR&T Engineer for the Deputy Commissioner of Design and Construction.

Local Governments are notified of their allocation from Bond Issues by the Deputy Commissioner of Design and Construction.

Unorganized Boroughs:

Road needs are solicited by the Region LSR&T Engineer.

The Region should make periodic verbal or written contact with local governments and cities and villages in the unorganized borough pertaining to their road needs. During these contacts, it should be made clear that the services of the Region and State LSR&T Engineers are at their disposal.

This action will assure a prompt transmittal for the annual three-year program.

Project Coordination

In the initial phase of a bush project request, the Region LSR&T Engineer should inquire, at the village level, if other work is being performed or contemplated by other State or Federal agencies. If none is known, the other agencies should be contacted directly.

Assembling information of mutual concern to the LSR&T Program and other State and Federal agencies should be an obligation of the Region LSR&T Engineer.

Annual Report

The annual report of LSR&T activities is prepared each year by the State LSR&T Engineer and submitted to the Director of Transportation Planning by November 15. This report is then incorporated into the Department of Transportation and Public Facilities' annual report.

The LSR&T annual report is based on activities for the calendar year.

The regions are asked to submit certain information that will enhance the program, such as special projects, methods of cost savings, training, etc.

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PROCEDURES

LSR&T Project Coding Procedures

Project numbers are assigned by the State LSR&T Engineer.

An example is as follows:

G	-	2	7	0	0	2	(a)	LSR&T Program
(a)		(b)	(c)		(d)		(b)	Region
							(c)	Allocation Area
							(d)	Project Number in Sequence

When work is required on a project prior to project approval, charges are to be made to region reconnaissance.

See Chapter 5 for reconnaissance numbers.

Road Design

Any LSR&T project that is designated to be designed by the Department will be submitted by the Region LSR&T Engineer using the following procedures:

1. A request for the design work should be sent to Region Road Design by memorandum.
2. All pertinent information relative to the project, including project charge numbers must accompany the request.
3. Proposed advertising date and amount authorized for design and construction.
4. Request an early cost estimate to accomplish the design and construction.

Maintenance and Operations

LSR&T Projects to be maintained by the State.

All LSR&T construction contracts to be administered by the Department or local government, which are wholly or part of another contract, funded with LSR&T money and maintenance to be performed by the Department, must be coordinated with the Region Director for Maintenance and Operations during the design phase.

Construction Projects:

A list of all LSR&T projects to be maintained by the Department and constructed by one of the following methods will be submitted by the Region LSR&T Engineer to the Regional Director for Maintenance and Operations prior to each construction season with a copy to the State LSR&T Engineer:

1. Department of Transportation & Public Facilities Contract
2. Local Government Contract
3. State Equipment and LSR&T Employees
4. State Equipment and Maintenance & Operations Employees
5. State Equipment and Village Employees
6. Local Government Equipment and Employees

The description of each project shall include the project name, location, length and width or surfacing, type of construction and projected beginning and ending dates. Additional information is to be given upon request.

This report is essential in order that a liaison can be established between the local maintenance foreman and the Regional LSR&T Engineer and for better planning of maintenance responsibilities.

Unless agreed to before hand, comments made directly to the local government representative or contractor by the local maintenance foreman are prohibited.

Final Acceptance:

Final acceptance of the project shall not be given without the concurrence of the Regional Director for Maintenance and Operations or his representative.

Final inspection and completion report of all LSR&T projects shall be submitted to the State LSR&T Engineer.

Completion reports for all road projects will be transmitted to the State Maintenance and Operations Engineer by the State LSR&T Engineer.

See Chapter 5 for minimum right-of-way requirements.

See Chapter 6 for completion report requirements.

See Chapter 9 for routine maintenance scheduling.

Recreation Facilities

LSR&T funds are not valid for the construction of a facility whereby the right of entry is not open to the public at all times.

The LSR&T program was established to fund projects that aid the surface transportation of people, equipment and supplies from one point to another over public right-of-way.

As defined in Section 19.30.241 of the Alaska Statutes, the following definitions pertain to the Local Service Roads and Trails Program:

- "(4) 'local service road' means a public road which is not designated as a route on the approved federal-aid highway system;
- (5) 'project' means an undertaking to construct a particular portion of a local service road or trail, or, if the context so implies, the particular portion of a local service road or trail so constructed;
- (6) 'trail' means a footpath or way on land or water that is open to public use as a matter of right whether or not a thoroughfare, particularly for dog sleds and mechanized snow vehicles."

LSR&T funds are not available for construction of recreational facilities such as rifle range pads, race tracks or similar proposals that limit the right of entry to the public.

Roads or trails to recreational facilities and parking areas for such facilities are eligible for funding under the program providing the following criteria is met:

1. Adequate public right-of-way.
2. Sufficient funds available.
3. Approval of local residents.

Reimbursable Service Agreement

Intradepartmental RSA Procedures:

In an effort to reduce administrative and fiscal paper flow, the following procedures will govern requests for services between elements of the Department of Transportation and Public Facilities.

The requesting agency will initiate a memorandum setting forth the type and level of service required of the servicing agency and citing the specific collocation charge code to which charges may be incurred. The memorandum will also reflect the total funds available for the service which will not be exceeded by the servicing agency. The servicing agency will acknowledge receipt and acceptance of the agreement to perform the service by so indicating on a copy of the memorandum and return to the requesting agency. A completed copy of the agreement memorandum will also be provided to the State LSR&T Engineer who will forward the document to Budget Finance.

This then becomes a valid agreement between the two agencies and is authority for the servicing agency to incur charges directly to the collocation charge code indicated. Where it is desired by either the requesting or servicing agency, a separate collocation code may be established.

The servicing agency may elect to use its own project reporting system for collecting costs for reimbursement when personnel services are involved. However, when charging an expense to the requesting office collocation code, the requesting office project ledgers must be used if applicable.

Formal Reimbursable Services Agreements and their associated documents will no longer be required on intradepartmental service requests.

Normal RSA procedures will be followed between other departments of State government and the Department of Transportation and Public Facilities.

Overhead Account

The use of the LSR&T overhead account number N00407 must be closely scrutinized to insure proper practice. All eligible costs must be charged to their respective project numbers. The use of this overhead number is limited to LSR&T assigned personnel. When your people are temporarily assigned to another section for one month or longer, a personnel action must follow.

Federal Grants

It is Legislative Audit's policy to consider all requests for federal grant audits. All requests should be in writing and the following information be included:

1. Number of grants to be audited.
2. Amount of the grant.
3. Whether grant funds were sub-granted to other organizations, and if so, the number of sub-grants and amounts.
4. The grant period and/or periods.
5. Who Legislative Audit should contact to discuss requests, and
6. Since these audits are performed on a reimbursable basis, the amount of funds available for audit.

If after reviewing any request, Legislative Audit is unable to perform the audits requested, they will send you a letter stating that fact. If you have any questions concerning this policy, please contact:

Legislative Auditor
Pouch W
Division of Legislative Audit
Juneau, Alaska 99811

Economic Development Administration (EDA) grants are sometimes utilized to supplement LSR&T funds, in areas where requests for roads exceed funds available.

Negotiations Prior to Bid Award

When a LSR&T construction contract is being advertised for bid, negotiations with the Contractor prior to award is prohibited.

Negotiations with a Contractor by a Department employee may be cause for dismissal.

CHAPTER 5

PRECONSTRUCTION

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PRECONSTRUCTION

Reconnaissance

This phase should determine how the project will be constructed; such as by contract, force account or in-house forces. Sufficient information should be obtained so plans and specifications can be prepared. Under this article a requirement for a meeting with the local community to inform them on the progress of the project and construction timetable may be necessary.

Procedure:

1. Select Typical Section
2. Determine method of construction
 - a) Local government contract
 - b) State contract
 - c) State forces (LSR&T or Maintenance)
 - d) Local government equipment and labor
 - e) Local government labor only
 - f) A combination of two or more
3. Determine if cost of project is within funds available
4. Charges to the region reconnaissance project is permitted only when it is impractical to obtain a project agreement and then only to establish the validity of such a project.

Reconnaissance charge numbers are as follows:

Central Unorganized Borough I	G-00011
Interior Unorganized Borough II	G-00021
Southeastern Unorganized Borough III	G-00031
Western Unorganized Borough IV	G-00041
Southcentral Unorganized Borough V	G-00051

See Chapter 11 for the reconnaissance check list.

Project Agreements

Projects submitted to Headquarters for approval must include the following information:

1. Project Agreement
2. Project description of work including typical section, length of project and a map of the location with scale shown. (See Management Forms 14 and 15).

3. Resolution for Local Entity
4. Funds authorization request
5. Engineering Consultant Agreement, if applicable

For unorganized borough projects to be constructed by LSR&T equipment no transportation of equipment shall be made to the project site until the following has been secured:

1. Right-of-way certification (Refer to this chapter)
2. Material source approved and free use of the material for construction
3. Wage agreement with the local governing body (Approved at the Region level)
4. Historical clearance (Refer to Chapter 5)
5. Structure permit, if applicable

Construction on any LSR&T project is prohibited until all above requirements are met.

See Chapter 11 for agreement forms.

Force Account

PS&E assembly of a force account (in-house) construction project submitted for approval must include the following:

1. Estimated Costs of Construction Engineering
2. Estimated Costs of Equipment and Labor
3. Estimated Costs of Materials
4. Estimated Costs of Closing out Project after Construction is Completed
5. Department of Fish and Game permit, if applicable
6. Coast Guard or Corps of Engineers permit, if applicable

Right-of-way Certification

When right-of-way has been cleared, the Regional Right-of-way Agent will certify in a memorandum that all necessary right-of-way has been acquired for each parcel and list the type of possession gained for each. In the event that the project will be constructed within a platted subdivision or townsite survey within the dedicated right-of-way, a letter of certification from the city will be adequate.

Roads to be maintained by the Department require a minimum of 100 feet right-of-way. The Department may waive this requirement when mitigating circumstances warrant.

Minimum right-of-way width required for subdivision streets must be sufficient to contain the slope limits as designed. Authority to proceed with construction will not be given until the Department is assured that this requirement is met.

Preliminary Engineering

Survey

Local Government

- 1) Determine right-of-way width is sufficient to construct proposed facility
- 2) Line and grade
- 3) Typical Section

LSR&T Forces

- 1) Utilize survey practice as determined by the Region LSR&T Engineer.
- 2) The survey must be complete enough to estimate construction costs and provide enough information to construct the facility proposed.

Structures

Requirements:

1. Stream crossings exceeding 5 cfs refer to U.S. Coast Guard Permit Requirements
2. AASHTO Specifications
3. ADT to determine design loading
4. When utilizing an existing structure at a different location the following data must be reviewed:
 - a) ADT
 - b) intended use
 - c) modification required
5. All structures require the State LSR&T Engineer approval.

Navigation Permits:

During the reconnaissance of a proposed LSR&T project, if spanning of a stream or work is required in a body of water, site information shall be submitted to the State LSR&T Engineer. To verify the need for a permit, the hydraulic unit of the bridge section will review the information and the State LSR&T Engineer will inform the Region LSR&T Engineer of the results.

The Corps of Engineers exercises permit authority for stream encroachments, tidal encroachments, and wet land encroachments under various sections of the River and Harbor Act of 1899, Section 404 of the Federal Water Pollution Control Act Amendments of 1972, and other federal codes. Coordination of permit requirements and processing of applications is handled by the hydraulics unit of the Bridge Design Section. Part of the permit requirements is a "Certification of Reasonable Assurance" from the Alaska Department of Environmental Conservation under Section 401 of the Federal Water Pollution Control Act Amendments of 1972. This process is also handled by the hydraulics unit. The processing of these permits usually requires approximately 120 days.

U.S.C.G. navigation permits are required under Section 9 of the River and Harbor Act of 1899 and other federal codes for all "navigable waters" crossings. The hydraulics unit of the Bridge Design Section coordinates with the Coast Guard to determine if a particular crossing is non-navigable, has "prior approval", or requires a navigation permit. On those crossings where marginal navigability may exist, the hydraulics unit submits a "Request for Declaration of Non-navigability" to the Coast Guard for review by their legal section. If the crossing is declared navigable, the hydraulics unit submits a Navigation Permit Application for processing. Part of the navigation permit requirements is a "Certification of Reasonable Assurance" from the Alaska Department of Environmental Conservation under Section 401 of the Federal Water Pollution Control Act Amendments of 1972. This process is also handled by the hydraulics unit. The processing of these permits usually requires approximately 180 days.

Corps of Engineers U.S. Coast Guard Permit Applications:

In reference to the Corps of Engineers' permits under Section 404 of the amended Federal Water Pollution Control Act of 1972 the following information is to help anticipate the information and time needs in obtaining necessary COE and USCG permits:

Phase III of Section 404, effective July 1, 1977, requires application for permit if there is any placement of dredged or fill material in wetlands or bodies of waters to their headwaters where the flow is 5 cfs or less. The quantity of fill material that the Corps considers is that amount below the ordinary highwater elevation (OHW) or below MHHW for tidal influenced areas.

At the present time the Corps of Engineers is forming a general permit for fills less than 100 c.y. Investigation of the special conditions required for

a general permit has indicated that more paper work and possibly a longer delay in getting a general permit will be required than applying for a regular permit. Except in unusual cases, applications will be made for regular permits.

Section 10 of the Rivers and Harbors Act of 1899 requires permit application for the excavation from or depositing of material below OHW or MHHW (Pacific Coast) in the navigable waters of the U.S. The Coast Guard through its navigation permit process exercises authority under Section 10 for projects that have bridges, culverts, causeways and overhead pipelines. Projects that require a Corps of Engineers permit under Section 10 include but are not limited to wharfs, docks, dolphins, excavation, dredging, filling disposal of dredged material, riprap, retaining walls, breakwaters, wires and cables over navigable waters.

Jurisdictions of these two agencies obviously overlap in some cases. In most cases only one agency will exercise its authority and it is sometimes difficult to determine which agency will be the one to do so. In those cases where jurisdictional authority is in question it is necessary to ask the Coast Guard for a declaration of non-navigability. The U.S. Coast Guard is getting more stringent on the determination of navigable waters. The data required on their standard form for navigability is quite extensive. Determination of navigability has taken up to four months. The time required for processing and issuing a navigation permit is about six months after the application is received by the Coast Guard.

The processing of Corps permits takes approximately 90 to 120 days after they are received by the Corps. The time will be longer if problems occur due to objections by reviewing agencies.

In order to advertise projects as scheduled it is necessary that the following material be received by this office four months in advance for Corps of Army Engineers permits and seven months in advance for Coast Guard permits.

Corps of Engineers Permits

1. Location of project on USGS quad sheets.
2. Location of stream crossings where the flow is over 5 cfs. This could be done by station numbers for small streams.
3. Amount of fill in cubic yards below OHW (MHHW for tidal influence) in the natural channel.
4. Amount of excavation in cubic yards below OHW (MHHW for tidal influence) in the natural channel.
5. Location of disposal site for and method of transporting the excavated material.
6. OHW (MHHW) elevation near culvert and elevation of stream bed at that location.

7. Drainage areas.

Determination of OHW (MHHW) elevation can be as follows:

1. On streams that have USGS gaging records the enclosed definitions of terms by the GOE is helpful. Also is a list and map of USGS gages. Records on any specific stream are available upon request.
2. Preliminary studies indicate that the flow for OHW = 10% of a 50 year flood as a State wide constant with the exception of the Interior Region where studies indicate an 8% conversion factor. It should be noted that these conversion factors should be used with caution. As more data is collected, adjustments will be made for the conversion factors.

U.S. Coast Guard Permits

1. Location of project on USGS quad sheets
2. Location of stream crossings where the flow is over 5 cfs. This could be done by station numbers for small streams.
3. Drainage areas
4. Length of stream and fall per mile
5. History of navigation
6. Pictures of stream crossings if available

Bridge Design:

The assistance available from Bridge Design on LSR&T projects can be greatly improved by more complete and timely communications. This will also avoid inadequate designs which might prove hazardous while keeping within budgeted funds.

When a project involves a bridge a completed DH45 form (applicable portions) should be provided along with the following:

- Construction schedule
- Amount budgeted for structure
- Number and type of vehicles in community
- Anticipated live load during life of structure
- Recommended structure type
- Materials available (if any)
- Equipment available
- Transportation limitations
- Experience of labor for erection (when to be done by local labor)
- Site survey, marked aerial photo or sketch drawn

In case a design is done or partly done by a region, other agencies or individuals, this section will review the work in keeping with the considerations listed above.

EXAMPLE

BRIDGE SITE SURVEY

Form DH 45

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

Project No. S-0481(9) BRIDGE SITE SURVEY Bridge No. 1162
(To Accompany Contour Map)

Name of Stream: Sheep Creek Date of Survey: June 17, 1963

Location: 5 mi. E. of Mantilla Rt. No. FAS-481 Sta. No. 1027+30

Bench Mark TBM#5 Sta 1027+00 Do fish use stream for spawning? Yes

Drainage Area: 96 sq. mi. How Determined: U.S.G.S. Topo. Map

General Description of Drainage Area: 40% Rocky mountainous, 60% rolling brush
covered country

Any Storage Basins Upstream? Yes, Cascade Lake - 5 sq. mi. - 2 mi. upstream

Elev. of High Water 340.0 High Ice 333.5 Date of High Ice 3/2/61

Location of High Water Mark 20 ft. Left Sta. 1027+30.

What caused H.W. Heavy rain in spring with snow melt during warm weather (70°).

To what elevation can water be backed up without local flood damage? 350.0

Slope or gradient of stream: 500 ft. upstream 1.7% 500 ft. downstream 1.6%

In dry stream bed, give water table elev. Stream never dry.

Describe ice and drift conditions Sheet ice - rotu in place. Brush and small trees
(15' max. length) during flooding.

Scour conditions: None existing. Stream may scour at piers.

In general, is stream cutting or filling? Stable

Should structure be skewed? No

Should channel change be made? Not necessary.

Any special treatment for approach fills? Riprap abutment slopes.

Sidewalks and Utilities No sidewalk or utilities in area.

SUBMITTED BY William Jones HEATH

APPROVED BY _____
(District Engineer)

Date _____

Materials

1. Royalties are to be paid by the recipient with no reimbursement from the State except under extreme conditions such as, the competitive bidding procedure.
2. Inspection:
 - Local Government - responsible for inspection and acceptance of materials.
 - DOT/PF Contract - Construction Manual, Section 1.03.09
3. Determine that adequate materials exist to construct the project.

Traffic Safety Responsibilities

I Construction

Road Construction Traffic and Pedestrian Safety Requirements (Type of Protection must fit nature of work)

Short term day light construction operations

- a) Minor traffic and pedestrian interruptions -Notes: 1,6
 - b) Major interruptions (moderate to high volumes) -Notes: 1,2,6
 - c) Lane closures, Flagman operation -Notes: 1,2,5,6
 - d) Street closed during construction period -Notes: 1,2,4
2. Long term day light with overnight protection
 - a) Minor traffic and pedestrian interruptions -Notes: 1,3,6
 - b) Major interruptions (Moderate to high volumes) -Notes: 1,3,5,6
 - c) Lane closures, Flagman operation -Notes: 1,3,5,6
 - d) Road or Street closed overnight -Notes: 1,3,4
 3. Night Construction operation
 - a) Minor traffic and pedestrian interruptions -Notes: 1,3,5,6
 - b) Major interruptions (Moderate to high volumes) -Notes: 1,3,5,6
 - c) Lane closures, Flagman operation -Notes: 1,3,4,5,6

- d) Road or Street closed during night construction

-Notes: 1,3,4,5,6

Note:

Notification of road disruption (closed or restricted) should be given to news media and local Police, Fire and Emergency Agencies.

Worksite Protection Notes

1. One advance warning sign approximately *700 feet before each direction of approach to construction area, such as ROAD CONSTRUCTION AHEAD, ROAD WORK AHEAD, etc. One additional specific warning sign approximately *300 feet before jobsite, such as, ONE LANE ROAD AHEAD, SHOULDER WORK AHEAD, MEN WORKING, SURVEY CREW, etc.
 2. Traffic cones and temporary barricades placed to direct traffic through the work area and to protect worksite.
 3. More permanent barricades and barriers with steady burning or flashing battery or electric lights installed for visibility and delineation.
 4. ROAD CLOSED signs and barricades sufficiently placed to safely close traveled way. A DETOUR plan shall be submitted to the Engineer for approval prior to commencement of closing roadway. DETOUR signs directing motorists to alternate routes around closed roadway.
 5. Flagman required for directing traffic with FLAGMAN AHEAD signs installed 300 to 500 feet in advance of station.
 6. Flagman required to protect worksite area with FLAGMAN AHEAD signs installed 300 to 500 feet in advance of worksite.
- * Verify distance using approach speed data and A.T.M.

II Traffic Design

Permanent Traffic Control Devices Requirements

1. Warning Signs

- a) Curve warnings with speed plates -Notes: 1,2
- b) School and Pedestrian warnings -Notes: 1
- c) Hazards, such as loose gravel, slides, etc. -Notes: 1,5
- d) Intersections, street ends, stop ahead, etc. -Notes: 1,3,5,6

2. Regulatory Signs

- a) Stop, Yield -Notes: 1,4,6
- b) Speed limit -Notes: 1,3,6
- c) One Way, Do Not Enter -Notes: 1,6
- d) Load restrictions and closures -Notes: 1,2,5

3. Guide Signs

- a) Street name signs -Notes: 1,2,6
- b) Directional and mileage signs -Notes: 1,2
- c) Service signs (Hospital, police, etc.) -Notes: 1,2,6,7

4. Delineation and Object Markers

- a) Roadside delineation - hazard areas -Notes: 1,2,8
- b) Bridge end and Object markers -Notes: 1,2,9

Permanent Control Device Notes

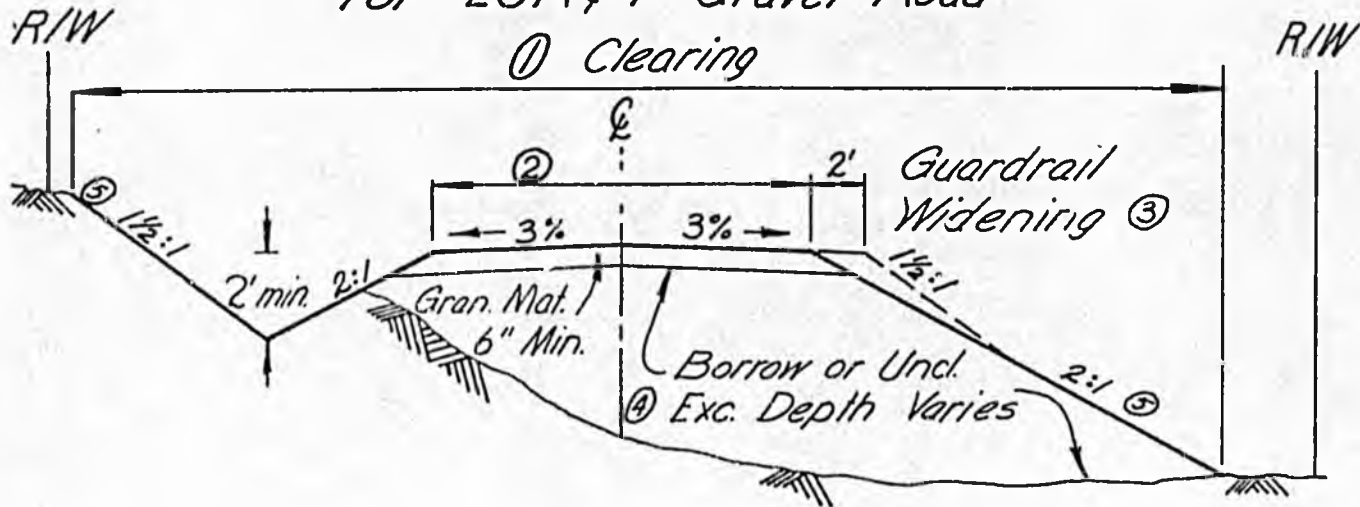
1. Determine location, and type of sign needed during P.S. & E. preparation.
2. Additional field conditions may warrant further protection devices.
3. Has a speed study been completed or is one warranted?
4. Have STOP or YIELD warrants been met?
5. Have construction or permanent conditions warranted restrictive signs?
6. Are existing nonstandard signs being replaced?
7. What services signs needed and if so what type?
8. Areas where road sides and objects need delineation.
9. Bridge ends, guardrail ends and objects requiring type III object markers.

Typical Sections

The Typical Section will be determined by the intended use and should be adequate to provide the desired service.

TYPICAL SECTION

For LSR & T Gravel Road



- ① Minimum Clearing & Slopes kept within existing Right-of-Way.
- ② 14' Min. Single Lane with turnouts @ Inter-visible locations.
20' Min. Two Lane
- ③ Minimum requirements for guardrail:
 - a) Outside of horizontal curves less than 600' radius
 - b) Ponding of water at toe of fill exceeds 3 feet in depth.
 - c) Approaches to structures.
- ④ Overlay embankment durable enough to withstand intended loads.
- ⑤ Slopes will vary depending on material (2:1 F.S. to 5' Min.)

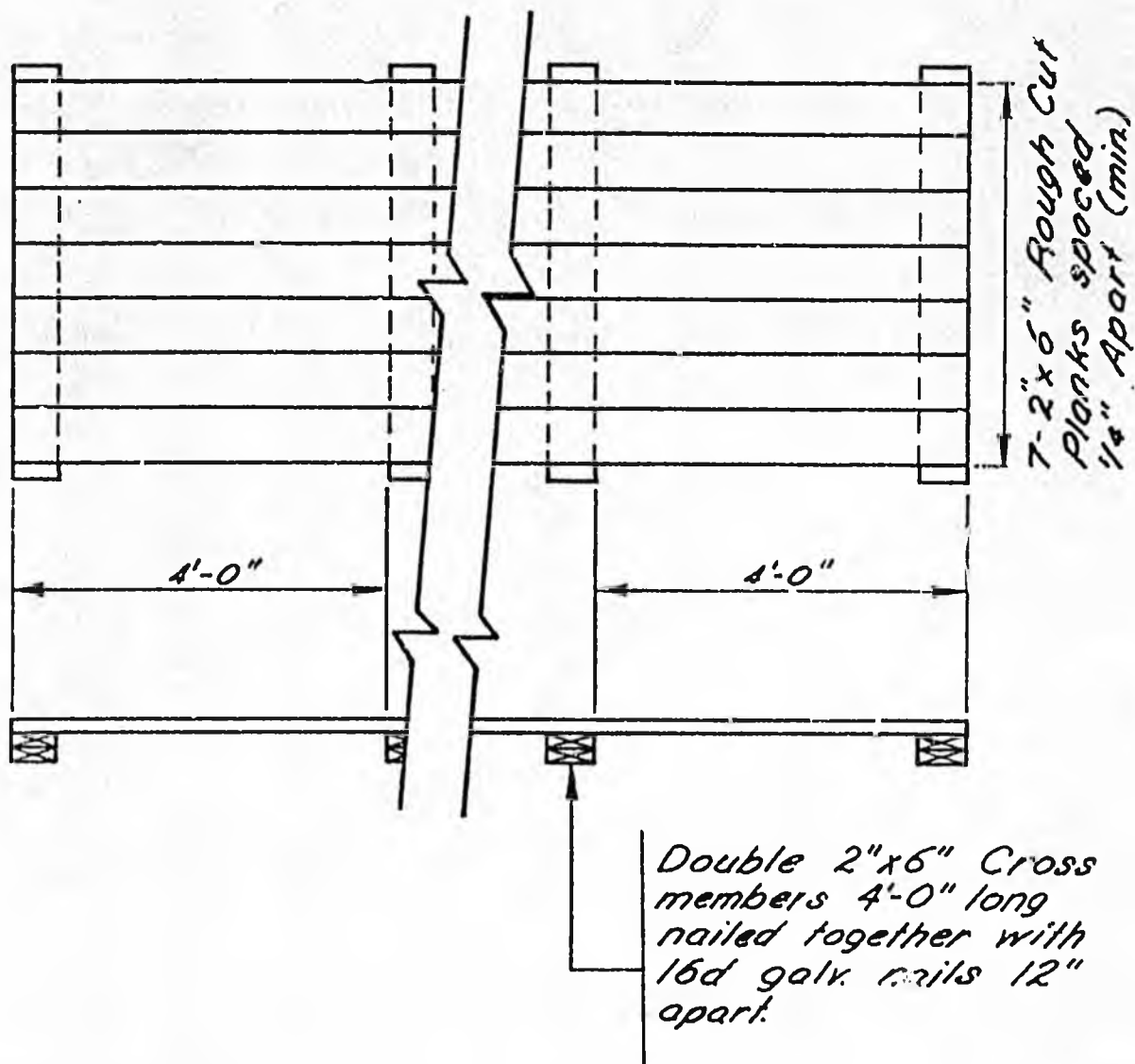
Alignment is not normally a problem since LSR & T goal is to get people out of the mud and connect two points with a vehicle trail. The project should be routed to eliminate safety hazards.

All Local Government projects will be scrutinized as to good engineering practices and AASHTO design of Structures.

Widen 2 feet inside of curve when radius is less than 600 feet.

4/15/77

TYPICAL BOARDWALK LAYOUT

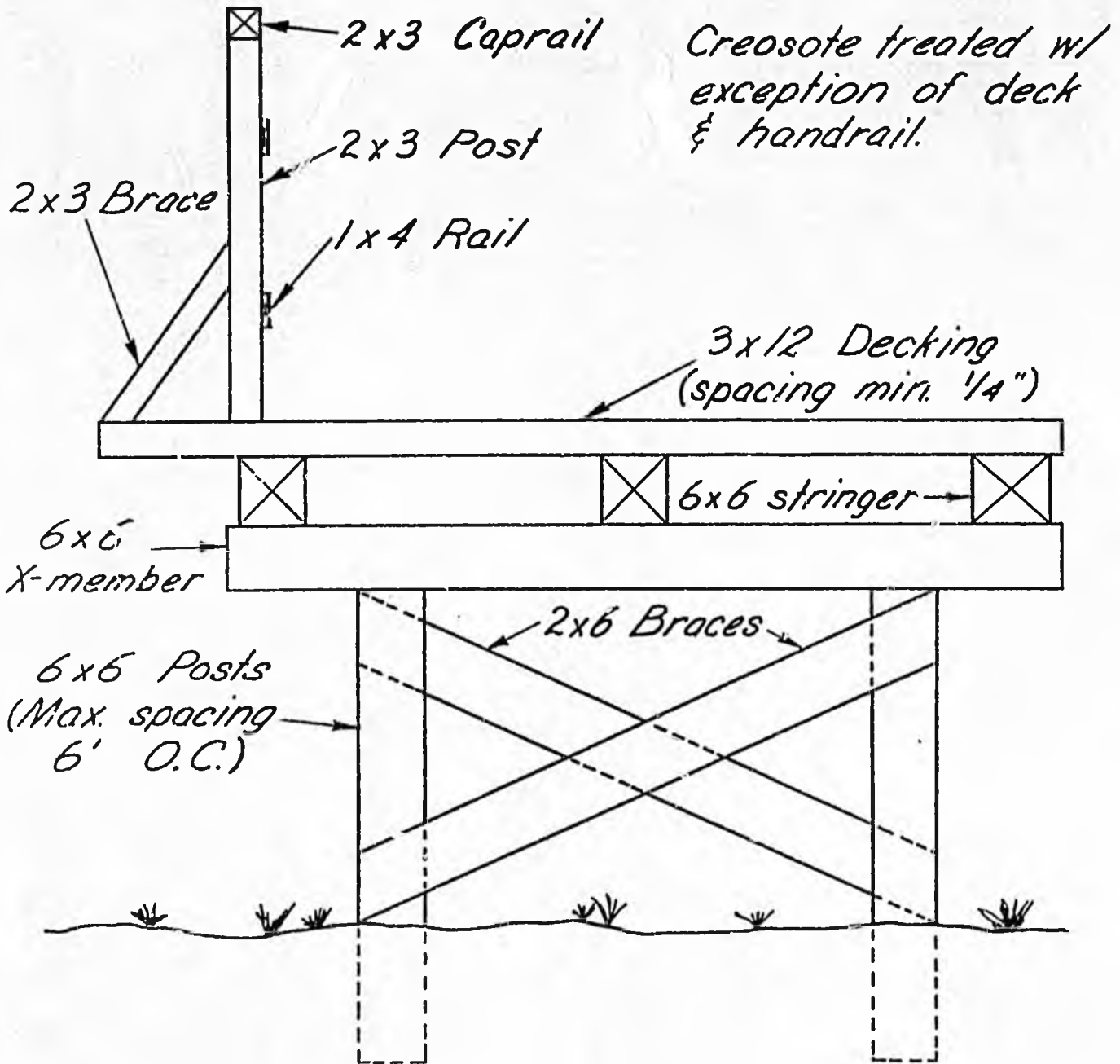


NOTES:

1. Stagger butt end joints at cross members.
2. Minimum plank length should be 8'.
3. Use 3-16d galvanized nails for each plank at each cross member.

TYPICAL BOARDWALK TRESTLE

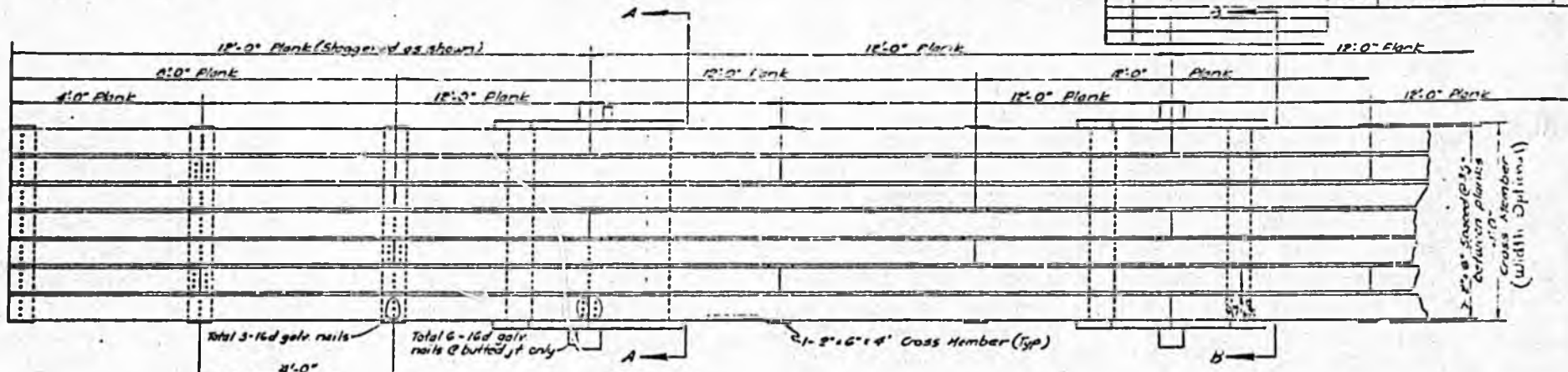
Typical Section



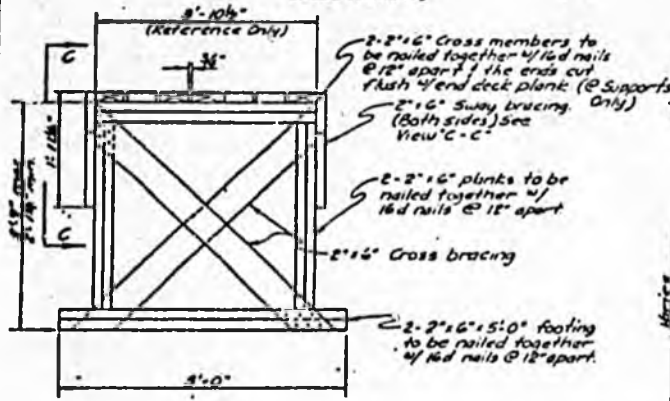
Length- ___ ft.
Width- ___ ft.

No Scale

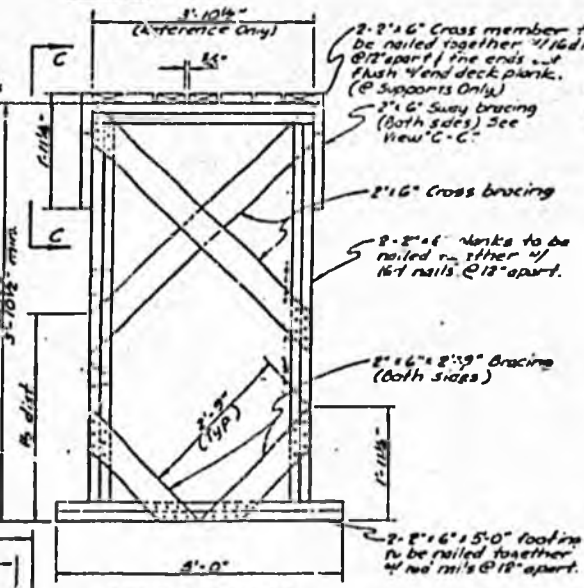
REVISIONS	DATE	BY	APP'D



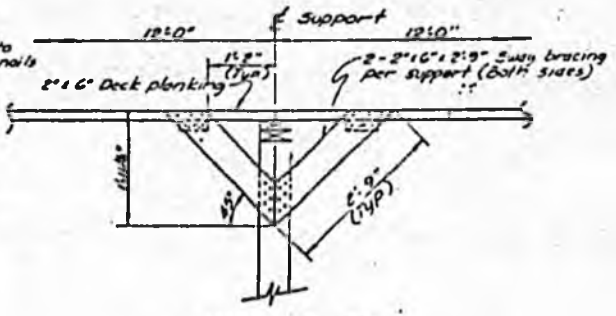
PLAN - FOOT BRIDGE



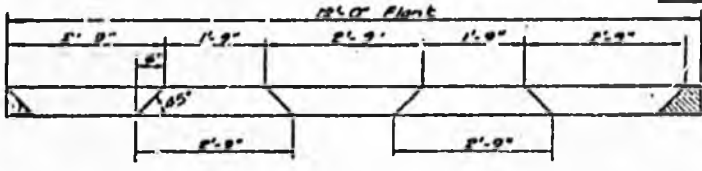
SECTION 'A-A'



SECTION 'A-A'



VIEW 'C-C'



TYP BRACE CUTTING PLAN

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS
VILLAGE FOOT BRIDGE
LAYOUT
6-10116

Utility Relocation

Utility move costs are fundable under the LSR&T Program and must be considered when necessary to construct a proposed project.

Alaska Historic Preservation Act

In compliance with Section 41.35.070 of the Alaska State Statutes, a list of all LSR&T projects scheduled for construction and not previously submitted shall be sent each year by the region directly to:

Director and State Historic Officer
Division of Parks
619 Warehouse Drive, Suite 210
Anchorage, Alaska 99501

The list of projects must include the project name, type of construction and a location sketch with scale shown. This information should be submitted as soon as construction activities are planned in order that adequate time for review is allowed prior to construction.

A copy of this report must be submitted to the State LSR&T Engineer.

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CONSTRUCTION

Management

This will be determined by how the project is to be constructed. Contract or force account projects will require a fulltime project engineer/inspector. If constructed by in-house forces, a local individual may be appointed project foreman and will be under the control of the Regional LSR&T Engineer or roving foreman.

Construction Procedures (Project Engineers)

Projects are administered by two distinctly different contractual methods; by construction contract and by village forces and Department equipment. LSR&T projects, both methods are 100% State funded. It is noteworthy that no Federal Highway Funds are involved on these projects. Federal grants, special allocations and local participation funds are sometimes used in conjunction with LSR&T funds. If other funds are included in your project, a separate set of instructions will accompany your project assembly.

METHOD A (by contract):

When LSR&T projects are under contract, a Project Engineer is assigned and the LSR&T Engineer serves as Assistant Construction Engineer. Some of the instructions given to the Project Engineer include:

1. Type of funding and funds available for project including engineering.
2. Coding on stock requests and time sheets.
3. Materials summary requirements.
4. Approvals for change orders, etc.
5. Records required - use Construction Manual.

Administration of the contract will be in accordance with the Construction Manual.

METHOD B (with local labor and State equipment):

Prior to your assignment to the job site, certain agreements shall have been entered between the Department and the local governing body. These are a "Project Agreement" and a "Reimbursement Agreement" for labor. The "Reimbursement Agreement" will state that the local governing body will hire laborers at a stipulated rate to work under the direct supervision of the Project Engineer. Your ring-bound book will contain:

EXAMPLE:

1. DAILY EXPENDITURE ACCOUNT: This form will record the daily and cumulative costs of the project. We will be given a

figure to enter on the first day's sheet for the "Total Cost Brought Forward" line near the bottom of this form. Make the subsequent daily entries and submit a copy weekly to the Region LSR&T office.

2. WEEKLY LABOR EXPENDITURE ACCOUNT: This form serves a dual purpose. Make the entries daily and total them weekly. The daily totals are entered on the "Daily Report of Contract Costs" and the completed form is the backup for payment to the local governing body for labor.
3. PAYMENT FORM: This is the form that is used to make periodic payments to the local governing body for labor. Complete it and transmit it to the Region LSR&T Engineer with the "Daily Labor Expenditure Account" for backup.
4. EEO REPORT: This form is to be submitted at the end of each quarter.

A complete Project Diary must also be kept in a suitable bound book.

We anticipate that the ring-bound books will serve a dual purpose. First, it is your initial supply of forms; and second, your carbon copy of the completed day-to-day or week-to-week forms, retained in the book, becomes your initial file of project records.

Instruction to Local Foreman:

1. Limits of Authority.
2. Inform him of what is to be constructed and ask for suggestions on how this can be accomplished, since he may be more familiar with the area and local individuals hired to do the actual work.
3. Discuss with him the priorities and sequence of construction.
4. Emphasize safety and the necessity to hold regular safety meetings.
5. Emphasize preventive maintenance for equipment.
6. Establish a routine for servicing equipment at regular intervals.
7. Inform of the requirement of keeping daily load count of material hauled.
8. Emphasize submitting time sheets in timely manner.

As an aid in filling out the remarks column on the daily expenditure account sheet, you will find below a list of the minimal information required:

Contractors Work Schedule

1. Shifts/day
2. Hours/shift
3. Days/week

Culverts

1. Placement (Location)
2. Length

Travel

1. Departure
2. Destination

Construction Status

1. % completed this day
2. % completed to date

Borrow

1. Placement (Location Station to Station)
2. Quantity (Yd3).

Equipment Being Used

1. Type (identify)
2. Approximate number of hours each piece of equipment is being used.

Project Budget

Each project is budgeted (estimated) to include design, construction and administration.

It is the Region LSR&T Engineer's responsibility to submit a cost estimate that is reasonable.

No construction equipment is to be moved to a project site until the R/W certification and royalties for material is secured.

No construction equipment is to be moved from a project site that is not complete without the prior approval of the State LSR&T Engineer.

No project is to be terminated, typical section reduced or length of project shortened without the prior approval of the State LSR&T Engineer.

Reimbursement to a local government exceeding the authorized amount requires prior approval of the local government authority (usually the assembly).

Overruns for State administered projects over 10% require prior approval of the State LSR&T Engineer.

Equipment Costs

Cost of operation exceeds the value to the fleet:

1. Evaluate
 - a) down time
 - b) parts and labor
 - c) availability of parts

2. Reasons

- a) too old
- b) lemon

3. Determine action

- a) keep the equipment on line
- b) replace

Transportation

Evaluate moving costs of construction equipment.

- 1. Air
- 2. Barge
- 3. Truck

Safety

Pursuant to Part 1926 of the U. S. Department of Labor OSHA Act, the following basic safety specification shall apply to all LSR&T projects and to the personnel working on them:

Proper safety equipment shall be provided by the employer for the work conditions. It shall be the employee's responsibility to properly use all equipment or devices when appropriate and conditions warrant.

The Regional LSR&T Engineer or his designee, shall sideline or discard defective machinery and equipment. This equipment will not be used on the project until it has been restored to a safe condition.

All equipment to be used on any LSR&T project shall first be inspected and approved by the Region LSR&T Engineer, or his authorized representative.

In matters concerning the safety of a work site or conditions, the project site supervisor shall obtain the Regional LSR&T Engineer's permission before proceeding with the work.

A basic safety/first-aid plan shall be drawn up and presented at a meeting prior to construction and a plan for the implementation shall be formulated at this time.

Before the start of any work, project personnel shall attend a safety orientation meeting. It is mandatory that safety meetings be held at least once per month. All new or late hires shall be oriented to all existing safety programs and policies and procedures that apply to the project.

Should there be any questions concerning safety by the project representative, contractor or employees, they may contact the Region Safety Officer through the Regional LSR&T Engineer.

All State contracts shall adhere to the safety rules and procedures outlined in Section 1.07 of the Construction Manual.

All personal injuries on the job shall be reported on form ADL 210 Workman's Compensation form, within twenty four hours.

Supply

Purchases.

Purchases of equipment chargeable to an equipment project number in excess of \$500 shall have prior approval from the State LSR&T Engineer. This can be accomplished by phone.

Bills:

Freight bills are to be paid up on receipt of billing. All other bills are to be paid upon receipt of the merchandise.

Bid Waivers:

Planning ahead should eliminate all bid waivers.

Bookkeeping

LSR&T Project Construction by LSR&T Forces:

1. A system must be maintained to insure an audit trail.
2. To eliminate time-consuming review costs, the following procedure is necessary:
 - a) Attach invoices to disbursements
 - b) Show documentation for all materials
 - c) Checks paid out with wage agreements attached
 - d) Signed employment cards for each employee listed (unless they are State employees)
 - e) On LSR&T administered projects, Chapter VIII of the Construction Manual may be used for routine record keeping practices.

DOT/PF Contract LSR&T Projects:

When projects are being constructed by State contract, including force account, the Construction Manual will be followed for record keeping.

When projects are constructed with in-house forces a bookkeeping system must be set up and kept current of all costs charged to the project to provide an accumulative total of all expenditures.

Inspection

Local Government Projects:

Periodic inspections are required (minimum);

- 1) reconnaissance
- 2) during construction
- 3) when construction is completed.

LSR&T Administered Projects:

Same as for Local Government.

Completion Report

All LSR&T completion reports shall include the following:

- 1) Location Map with Scale
- 2) Type of Facility
- 3) Length and Width of Facility
- 4) Final Inspection Report
- 5) Report of Deficiencies Rectified
- 6) Acceptance by Maintenance and Operations when applicable
- 7) Project History
- 8) Certification by Local Government (See Example, Chapter 11, Management Form No. 5).

Regional Construction Review

Purpose:

To establish procedures for review of LSR&T records to assure that such records adequately document costs incurred; that all measurements and computations are in accordance with the project specifications, and that all amounts due on estimates are properly separated into fund categories according to the project estimate, or that reimbursement to a governing body is in accordance with the project agreement and that records are adequately documented.

Procedures:

1. Until otherwise notified by the State LSR&T Engineer, review shall begin at the time the project is completed. The Regional Engineer may require an interim review when he believes such action is warranted. The number of LSR&T projects to be reviewed will be determined by the Regional Engineer. The status report will reflect projects available for review.

2. The Review Engineer, appointed by and under the direct supervision of the Regional Engineer should check the basic documentation of the project as necessary to assure himself that quantities were determined as prescribed by the plans and specifications or that reimbursement to the governing body (municipality, borough, village, etc.) has been made in accordance with the project agreement.

A. Department of Transportation and Public Facilities/Contract project:

Upon project completion, the final estimate and project records shall be reviewed. This review must be accomplished within 30 days after final estimate. The review should determine the following:

- a) Documentation and records support the quantities being paid.
- b) Measurement was made in accordance with plans and specifications, appropriate change documents, and properly documented force account records.
- c) Quantity computations were properly performed and independently checked.
- d) All work orders, change orders and extra work orders are accounted for.
- e) Plans and field records indicate work was performed within the project limits.
- f) Final acceptance has been made.
- g) Liquidated damages have been assessed when due.
- h) Project charges on the final estimate have been separated as per project agreement estimate, and properly distributed.
- i) LSR&T funds have not participated in right-of-way work.
- j) Costs are correctly classified and all participating costs are properly supported in accordance with the project agreement.

B. LSR&T projects performed by a governing body, either by force account, contract, or utilization of local labor. This review must be accomplished within 30 days after the records are submitted to the Review Engineer. Review charges to this type of project cannot exceed one man workday unless prior approval is received from the Region Engineer.

The review should include the following:

- a) Audit trail readily documents and supports quantities being paid and supports labor, equipment and material/ freight costs. The Wage Deductions Agreement, LSR&T Form No. 80 to be utilized when necessary.
 - b) Governing body receives authority to advertise and authority to award a contract, for work to be performed by contract construction.
 - c) LSR&T funds have not participated in costs incurred prior to execution of the project agreement or in costs attributed to right-of-way.
 - d) Change orders, etc. have had LSR&T Engineer's concurrence in accordance with the project agreement.
 - e) Increase in costs over the amount originally authorized are documented and properly authorized.
 - f) Final acceptance has been made.
- C. If the project records fail the above review(s) they should be returned to the Regional Construction Engineer or LSR&T Engineer for corrective action. The Region Construction Section or LSR&T Engineer should document the disposition of all discrepancies. Within thirty days a report of such disposition will be forwarded to the Review Engineer, who will then prepare a final review report for submission to the Regional Engineer, with copies to the State LSR&T Engineer and Construction Engineer, outlining the review procedure, findings, and subsequent disposition of any discrepancies.
- D. Final review reports will be retained by the region for a minimum of three years.

Status Report

The Status Report of projects is to be submitted annually by November 1. This report shall reflect the project status through September 30 on only those projects that were active on January 1, of that year. The State LSR&T Engineer may request interim reports.

Finance

Disbursements:

All requests for disbursement shall include the proper coding and region approval. Payment requests for payroll will receive special handling by the Headquarters' Finance Section.

Bank Accounts:

Joint Account for LSR&T Projects

In the event a village or small community has no money on which it can draw for procurement of material or payment of wages, it may be advantageous to advance the community the funds to initiate a project.

This can be done by including in the agreement a clause pertaining to establishing a joint account for purchase of material and payroll purposes, and a clause to the effect that the account may be closed by the Departmental signatory at any time.

After the agreement is approved, or at the time the agreement is submitted for approval, an advance may be requested and the account opened. Two reliable village employees, preferably the bookkeeper and foreman should be village signers, one of whom must sign with a Departmental signer in order for the check to be valid. If you have a State inspector or Project Engineer on site, they can co-sign and verify the checks against the daily and weekly reports prior to issuing the checks on the site.

If a Departmental co-signer is not on the site, the checks can be mailed in with the reports, material billings, etc., for verification and signing of the checks. Income tax and other deductions may be made prior to issuing the checks, or the employees can sign the wage deduction LSR&T Form No. 80.

Upon receipt of cancelled checks and bank statements (mailed to the Region LSR&T Engineer) the appropriate checks, or copies, should be attached to the weekly reports and material billings or invoices for ease of review.

Coding Structure

OLD CODE/NEW CODE CONVERSION

<u>Old Code</u>	<u>New Code</u>
14-81-7-101	25-81-2-101
-201	-201
-301	-301
-401	-401
-501	-501
-670	-001
-991	-002

NEW COLLOCATION CODES - LOCAL SERVICE ROADS
EFFECTIVE JULY 1, 1977

25-	Department of Transportation
25-81	Highways
25-81-2	Cap. Imp./General Fund
	Local Service Roads
25-81-2-001	Administrator
-002	Cost Bond Sales
-101	Central Region
-201	Interior Region
-301	Southeastern Region
-401	Western Region
-501	Southcentral Region

CHAPTER 7	7-0
EQUIPMENT	7-1
Purchase	7-1
Rental	7-1
Operation	7-1
Repair and Maintenance	7-1
Disposal	7-1
Inventory	7-2

EQUIPMENT

LSR&T Construction Equipment Purchase

To insure the LSR&T construction fleet remains operational, new or used equipment may be purchased to replace existing equipment.

The request for funds to purchase equipment must be accompanied with adequate proof as to necessity, intended use and cost savings. Each request will be adjudicated on its own merit.

Equipment purchases to expand the existing LSR&T construction fleet will not be approved except where a cost savings to purchase rather than rent is justified.

Equipment Rental

A. Private Source

Before approving an equipment rental contract, the equipment to be used should be examined by the Region LSR&T Engineer or his representative (knowledgeable of machinery) as to its operational capabilities. The equipment must meet state safety standards.

For equipment rental agreements see Chapter 11, LSR&T Form 70.

B. WCF Region and Headquarters Levels

WCF equipment should only be used if absolutely necessary.

A check of the rental rates before using is necessary and establishing if rental will be wet or dry. When using WCF equipment (wet rental) it is necessary to make sure that all fuel and repair costs are charged to the vehicle and not the project. Establishing a system for recording daily usage is necessary.

Operation

Proper training of operators before they mount the piece of equipment is essential. Safety of the operator and by-standers must be considered the main factor during this orientation.

Repair and Maintenance

Prior to beginning work on a project instructions should be provided on preventative maintenance procedures and establishing a system for recording the daily equipment usage and a time frame for servicing equipment.

Disposal of LSR&T Equipment

Refer to Chapter 6, Construction, Equipment Costs.

Inventory In-house operation

A record should be kept of all equipment assigned to LSR&T and should include the type, model, location, condition and accumulated maintenance costs.

CHAPTER 8		
PERSONNEL		8-0
State Employees		
LSR&T Assignments		8-1
Temporary Reassignment		8-2
Outside Employment		8-2
Village Employees		
Local Hire		8-2
Local Control		8-2
State Control		8-2
Local Government Employees		8-2
Training of Community Residents		
State Employees Local 71		8-2
Community Employees		8-3

PERSONNEL

State Employees

LSR&T Assignments:

Program Administrator

Charles D. Karella - State LSR&T Engineer
P.O. Box 1467
Juneau, Alaska 99802 Phone: 364-2121

Central Region I

Robert L. Haneline - Regional LSR&T Engineer
Pouch 6900
Anchorage, Alaska 99502 Phone: 266-1542

Ruth Iverson - Secretary
John McClellan - Project Engineer
Charles Moyer - Project Engineer
Paul White - Project Engineer

Interior Region II

Donovan S. Ronken - Region LSR&T Engineer
2301 Pegc' Road
Fairbanks, Alaska 99701 Phone: 452-1911

Benedict Jones - Roving Foreman

Southeastern Region III

Olaf L. Bartness - Region LSR&T Engineer
P.O. Box 3-1000
Juneau, Alaska 99802 Phone: 789-0841

Western Region IV

Johnathan A. Widdis - Region LSR&T Engineer
P.O. Box 1048
Nome, Alaska 99762 Phone: 443-5266

Jeffrey S. Chandler - Project Engineer

Southcentral Region V

Bernard Murphy - Region LSR&T Engineer
P.O. Box 507
Valdez, Alaska 99686 Phone: 835-4322

Due to the multiple duties performed by the Southcentral Region LSR&T Engineer, his position is retained in the Region's engineering budget for Design and Construction.

Temporary Reassignment:

To insure that LSR&T retains program oriented personnel for the construction season all personnel assigned to LSR&T must be given temporary reassignment to other sections of DOT/PF when the workload dictates. Concurrence of the accepting sections must be obtained. Any deviation from this procedure must have the approval of the State LSR&T Engineer.

If the RSA method of finance is utilized, a memorandum must be sent to the affected employee with a copy to the LSR&T Engineer. If a PA is used, a memorandum is not necessary.

Outside Employment:

Any Department of Transportation and Public Facilities' employee who is in violation of S.O.P. 0900-11 policy on outside employment, will be subject to immediate dismissal for cause. Supervisors who condone these violations will be subject to the same action.

Village Employees

- Local Hire - Local residents receive priority hire.
- Local Control - As stipulated in project agreement.
- State Control - When the Department administers the project, the control of the project is by the Department utilizing an initial hire list from the village.

Local Government Employees

The local government has control of the local government employees.

Training of Local Community Residents

State Employee Local 71 Status.

In hiring of Local 71's in the LSR&T program, the following pay schedules will be in effect in our training programs and hiring policies.

Our laborers will be hired at a WG 58, the normal pay schedule. Much of our work for them is in land clearing (ax work), and they need no training there, other training necessary is minimal. All operators, that is, truck drivers, load operators, and dozer operators, will start at the WG 57, Step A. At the end of two (2) pay periods they will progress to WG 56, at the end of four (4) additional pay periods, they will go to WG 55, at the end of four (4) more pay periods, they will go to WG 54. At that point, the truck drivers will receive no more increases. They will be considered trained. Four pay periods after the loader or dozer operators receive their WC 54 pay, those operators will move to a WG 53.

In the event this schedule goes into two (2) seasons, all the time in the previous season will count toward grade increase upon returning to work the succeeding season.

The employees will be in temporary status as they complete the progression through the training period. At the time they finish their training, and achieve full pay, they will enter normal employee status, i.e. either permanent status or no more than eight (8) continuous pay periods of temporary status.

An agreement to the effect must be obtained between the employee and the supervisor at the time of hire.

Community Employee

Construction projects in communities utilizing locally hired crews are sometimes plagued with high rates of absenteeism. In an effort to induce crew members to show up regularly the following training scale is established, subject to certain stipulations noted below, for drivers and operators hired without previous experience:

Step 1	First two weeks	*\$ 6.00/hr
Step 2	Second two weeks	*\$ 8.00/hr
Step 3	Third two weeks	*\$10.00/hr
Step 4	After six weeks	*\$11.00/hr

* or prevailing wage

The principal stipulation is this: An employee must be at work punctually and every day of a continuous two week period to be eligible for a raise to the next step in the scale. In the event that he is not regularly punctual or misses a portion of a day in any week, he forfeits the opportunity to apply that week towards fulfillment of the two-week requirement.

Operators and drivers hired with previous experience may be started at a higher step depending on qualifications. Operators and drivers who are current union A card holders are paid prevailing union, provided they are never absent without cause. Mechanics, clerks and secretaries wages are commensurate with experience or union background.

The crew should be advised that absenteeism could be cause for applying the scale in reverse after an employee has reached steps 3 and 4.

CHAPTER 9

PROJECT MAINTENANCE

Road Powers

Maintenance Responsibility

Revenue Sharing

Routine Maintenance

9-0

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PROJECT MAINTENANCE

Road Powers

Local Government as defined in the LSR&T Act:

Article 3. Local Service Roads and Trails

Sec. 19.30.151. Local government participation. (A) Upon application by a local government, local service roads and trails constructed under the provisions of Sections 111-241 of this chapter and located within the jurisdictional boundaries of a local government, as defined in Section 241(3) of this chapter, may be transferred to that local government upon approval by the Department of Transportation and Public Facilities and after a vote of the people in the area. A local government which has assumed road powers before May 20, 1971 may not be required to hold an election as provided by this section.

(b) The commissioner is authorized to transfer funds to any local government which he determines to be qualified to construct local service roads or trails.

(c) Nothing in Sections 111-241 of this chapter prohibits a local government from participating financially in the construction of a local service road or trail.

Sec. 19.30.211. Maintenance. The department shall maintain, or cause to be maintained, any project constructed under the provisions of Sections 111-241 of this chapter, except that upon mutual agreement of the commissioner and the local government the responsibility for maintenance may be transferred to the local government if it is authorized to assume road maintenance powers.

Sec. 19.30.241. Definitions. In Sections 111-241 of this chapter (2) "home rule city" means a city defined in AS 29.08.010;

(3) "local government" means an organized borough of any class or a home rule city; (also home rule municipality).

Sec. 19.30.251. Local government powers. Road construction and maintenance authority is granted to local government, as defined in Section 241(3) of this chapter, which do not otherwise have that authority; however, the authority granted is limited to participation in the construction and maintenance of local service roads under Sections 111-241 of this chapter. (Section 2 ch 84 SLA 1971)

Maintenance Responsibility

Maintenance responsibility of projects constructed with LSR&T funds may be accepted by all local entities. Revenue sharing is available to all cities and boroughs incorporated under the laws of Alaska. Local governing bodies not incorporated may enter into road maintenance agreements with the Department of Transportation and Public Facilities. It is mandatory for home rule cities and home rule municipalities to maintain LSR&T funded projects.

Revenue Sharing

The Regional LSR&T Engineer should make every effort to explain the advantage of revenue sharing to all incorporated cities and organized boroughs.

For a local government to seek revenue sharing, the appropriate official must submit a release of maintenance responsibility to the Regional Highway Engineer for all roads constructed with LSR&T funds.

This document is to be sent to the State LSR&T Engineer who in turn will submit it with all supporting information to the Deputy Commissioner for Maintenance and Operations. When the request is approved, the mileage requested by the local government will be submitted by Maintenance and Operations to the Director of Transportation Planning to be deleted from the State Maintenance System.

The State LSR&T Engineer will be informed when the transaction is complete.

Routine Maintenance

Local service roads constructed by LSR&T funds and maintained by the Department shall be maintained as scheduled by the Regional Maintenance and Operations Division.

Reference is made to P & P No. 50-1000.

See Chapter 4 for projects maintained by State forces.

CHAPTER 10	10-0
CONTRACTS	10-1
Local Government Construction	10-2
DOT/PF Construction	10-2
Consultant Engineers	10-2
Negotiated Construction	10-2

CONTRACTS

Local Government Construction

All contracts with construction or engineering firms funded wholly or partially with LSR&T funds must be approved by the Deputy Commissioner for Design and Construction. No reimbursement of funds will be made for work performed prior to this approval.

All addendums, change orders, extra work orders must be approved by the Regional Engineer for Design and Construction.

Advertising and award of contract requires the approval of the Deputy Commissioner for Design and Construction.

Authority to Proceed will be issued by the Regional Engineer for Design and Construction upon receipt of the Department's concurrence in award.

The plan assembly submitted by a local government for approval must include the following information:

1. Plans
2. Specifications
 - a. Table of Contents
 - b. Notice to Bidders
 - c. Information to Bidders
 - d. Invitation for Bids
 - e. Contractor's Questionnaire
 - f. Proposal
 - g. Corporate Acknowledgement
 - h. Contract Bond (Payment)
 - i. Contract Bond (Performance)
 - j. Resident Agent's Affidavit
 - k. Non-Collusion Affidavit
 - l. Bid Schedule
 - m. Contract
 - n. Certification with Regard to the Performance of Previous Contracts or Subcontracts subject to the Equal Opportunity Clause and the filing of required reports.
 - o. Certification with Regard to Increased Participation by Minority Business Enterprise in Highway Construction.
 - p. Special Provisions. Specific Equal Employment Opportunity Responsibilities for all non-exempt State and State-Assisted Construction Contracts awarded in the State of Alaska.
 - q. Notice of Workmen's Compensation Insurance
 - r. Special Provisions
 - s. Current Alaska Minimum Wage Rates
3. Engineer's Cost Estimate
4. Department of Fish and Game permit, if applicable
5. Coast Guard or Corps of Engineers permit, if applicable
6. Assurance that a Bid Bond will be required

Documents may vary from the above, but results must be conformable.

Department of Transportation and Public Facilities Construction

All contracts require the approval of the Commissioner.

Authority to Proceed will be issued by the Deputy Commissioner for Design and Construction.

Consultant Engineers

Consultant Agreements with local governments and subsequent changes to the agreements require approval by the Deputy Commissioner for Design and Construction.

A minimum of three Consultants shall be contacted to perform the services or justify why it is not feasible.

Negotiated Construction Contracts

The following facts are necessary to consider a negotiated contract for construction of an LSR&T funded project:

1. Project less than \$100,000 may be negotiated at the discretion of the Department.
2. Projects exceeding \$50,000 require bonding.
3. Legal bonding required for a contract is bonding of 50% payment and 50% performance.
4. Difference between the low bidder and the negotiated price must result in substantial savings to the State.

CHAPTER 11
LSR&T FORMS
Required
Management

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REQUIRED LSR&T FORMS

<u>LSR&T Form No.</u>	<u>Page</u>	<u>Description</u>
10	11-2	Project Agreement - Local Government
20	11-6	Project Agreement - Local Governing Body
30	11-7	Funds Authorization
40	11-8	Employee Pay Agreement
50	11-10	Resolution - Local Government
60	11-11	Resolution - Local Governing Body
70	11-12	Equipment Rental
80	11-13	Wage Deductions Agreement
90	11-14	E.E.O. Report
100	11-15	Status Report
110	11-16	Reimbursable Services Agreement
120	11-17	Bridge Site Survey (DH-45)

NOTE: Each subsequent form revision will reflect in an increase of the form number by one, such as, the first revision to LSR&T Form No. 10 will read 11.

Region _____
 Allocation District _____
 Project No. _____
 Project Name _____
 Local Government _____
 Mailing Address _____
 Business Address _____
 Phone _____

PROJECT AGREEMENT
LOCAL SERVICE ROADS & TRAILS

PROJECT:

Under the provisions of AS 19.30.111 et. seq. this Project Agreement is entered into on this _____ day of _____, 19____, by and between the State of Alaska, Department of Transportation and Public Facilities (hereinafter the Department), and _____, (hereinafter the Local Government) for the purpose of constructing the following described project:

(A) OBLIGATIONS OF THE LOCAL GOVERNMENT

To further accomplish the construction of the project, it is agreed that the Local Government shall:

1. construct the above-described project, which is more fully described by Appendices "A" and "B" attached hereto and made a part hereof consisting of a location plat (Appendix "A") and a typical cross-section (Appendix "B");
2. provide all funds necessary to construct the project;
3. acquire and certify to the satisfaction of the Department that all necessary right-of-way, easements, permits and utility agreements have been obtained prior to commencement of construction;
4. award a contract for construction of the project to the lowest responsible bidder through the competitive bidding process unless the Commissioner of the Department expressly approves an alternate method;
5. execute a written contract with a contractor for the project;

6. maintain accurate daily written records of all costs for which reimbursement is requested, which records shall be made available for inspection by the Department upon request, and preserve such records for a period of three years following final acceptance of this project by the Department or termination of this agreement, after which they may be destroyed except for the project history, project engineer's diary, photos and the correspondence folder.

7. promptly pay in full all legitimate claims for services or material provided for construction of this project and hold the Department harmless from any claims or costs arising from the Local Government's failure or refusal to pay claims, whether or not those claims are legitimate;

8. indemnify, defend and hold harmless the Department from all liability, claims and demands arising from the work undertaken by the Local Government, its employees, agents or representatives, or arising out of any operation, no matter by whom performed or on whose behalf, whether or not in conjunction with work to be coordinated by the Department;

(B) OBLIGATIONS OF THE DEPARTMENT

1. If the Local Government has fulfilled its obligations, the Department shall reimburse the Local Government for the cost of materials and labor used in construction of the project in an amount which may not exceed the sum of \$ _____, the maximum amount of Local Service Roads and Trails funds allocated to this project.

2. The Department further agrees to provide assistance upon request of the Local Government in the form of advice and consultation for all phases of the project, including further design refinement and advice during construction. However, management of the project is the full responsibility of the Local Government.

(C) FURTHER CONDITIONS

1. Local Service Roads and Trails funds are obligated for this project in an amount as shown on the attached funds authorization which is made a part of this agreement. Maximum reimbursement for pre-construction engineering is limited to 15% of the Local Service Roads and Trails funds for this project.

2. Expenses incurred by the Local Government prior to the date of execution of this agreement will not be reimbursed by the Department.

3. The Department will not be obligated to reimburse the Local Government for costs which can not be adequately verified as a necessary cost of construction.

4. The following stages of this project are subject to the prior written approval of the Department;

- a. Plans, specifications and estimate prior to advertising
- b. Award of the construction contract.
- c. Any proposed contract for consulting engineering services.
- d. Any changes in plans or specifications occurring during the construction of the project.

5. The Department has the right to make periodic inspections during the course of work and a final inspection upon completion of construction for the purpose of certifying that the funds expended by the Local Government on this project qualify for reimbursement under this agreement.

6. When the construction of this project has been completed and duly accepted, the responsibility for maintaining the facility shall be borne by the _____.

If the Department agrees to perform the maintenance, the Department reserves the right to issue utility, driveway and encroachment permits. If the Local Government is to bear the maintenance responsibility, the Local Government agrees to indemnify, defend and hold harmless the Department and the State of Alaska from any and all claims arising in connection with or related to the maintenance function.

7. This agreement may not be modified except by written agreement between the Commissioner of the Department and an authorized agent of the Local Government.

8. The parties to this agreement shall obey all federal, state, and local laws and regulations.

9. This supersedes any and all other agreements expressed or implied between the Department and the Local Government with respect to this project and this agreement contains the entire agreement between said parties.

To witness this agreement, the Department and the Local Government have affixed hereto the signatures of the Commissioner of the Department and _____, the duly authorized signatory for said Local Government.

DATED this _____ day of _____, 19__.

(insert name of Local Government)

LOCAL GOVERNMENT

DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES

BY
Attach the completed resolution,
(attached) approved by the Local
Government

RECOMMENDED FOR APPROVAL:

REGIONAL ENGINEER

ATTEST:

(Title) _____

APPROVED AND EXECUTED BY:

Deputy Commissioner
Design & Construction

Date

Project Agreement
Local Service Roads and Trails
Local Hire

Under the authority granted by Chapter 84 Sessions Laws of Alaska 1971 this project agreement is entered into by and between the State of Alaska, Department of Transportation and Public Facilities; hereinafter called the Department, and _____, hereinafter called the Local Governing Body, that:

- 1/ The Department will upgrade local streets, access roads or trails, as shown on the attached plat with the typical section of improvement.
- 2/ A. In the event existing streets and platted street right-of-way do not agree, the platted right-of-way will govern.
B. Work will not begin until certification for the necessary right-of-way for this project is assured.
- 3/ A. The Department shall utilize local hire for on site project personnel excluding engineering personnel and project foreman.
B. Engineering personnel or project foreman may be hired locally at the discretion of the Department.
C. The Department will obtain initial local hire through the local governing body. Subsequent hiring is at the discretion of the Department.
- 4/ The cost of this project to the Department will not exceed _____.
- 5/ The Local Governing Body shall indemnify, defend and hold harmless the Department from all liability, claims and demands arising from the work undertaken by the Local Governing Body, its employees, agents, or representatives, or arising out of any operation, no matter by whom performed, or on whose behalf, whether or not in conjunction with work to be coordinated by the Department;
- 6/ The local Governing Body retains the right to provide additional unencumbered funds if they so desire.
- 7/ Project termination or work stoppage is dependent upon exhaustion of funds or completion of project, whichever occurs first.
- 8/ Following construction, maintenance will be provided by the _____.
- 9/ Any modification of this agreement are subject to mutual approval of both parties.

To witness the agreement, the Department and the Local Governing Body have affixed hereto the signatures of the Deputy Commissioner for Design and Construction and Village Council President, or duly authorized signatory of said Local Governing Body.

Local Governing Body

Date

Attach the completed resolution
(attached) approved by Local
Governing Body

RECOMMENDED FOR APPROVAL

Regional Engineer Date

APPROVED AND EXECUTED BY:

Deputy Commissioner
Design & Construction
Department of Transportation
& Public Facilities

HIGHWAY REGION

PROJECT NO. G-

NAME OF LOCAL GOVERNMENT

ALLOC. DISTRICT _____

PROJECT NAME _____

DESCRIPTION OF WORK AND REASON: _____

TYPE OF ACTION: _____

_____ INCREASE FUNDS _____ ORIGINAL FUNDING

_____ FINAL (to match actual cost)

IT IS REQUESTED THAT FUNDS FOR THIS PROJECT BE AUTHORIZED AS FOLLOWS:

FUNDS CURRENTLY AUTHORIZED \$ _____

CHANGE REQUESTED () _____

TOTAL AUTHORIZATION WITH THIS CHANGE: _____

_____ Regional Engineer _____ Date

Recommendation

Approved

Disapproved

Approved	Disapproved		
		_____	_____
		State LSR&T Engr.	Date
		Programming Officer	Date
		Director of Highway Design & Construction	Date

AUTHORIZATION GRANTED: _____

Deputy Commissioner for Design & Construction _____ Date

Department of Transportation & Public Facilities

NOTE: THIS DOCUMENT IS NOT VALID UNLESS APPROVED BY THE DEPUTY COMMISSIONER

CC: Region
Program Officer
Finance

Region _____
Allocation District _____
Project Number _____
Project Name _____

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES

LOCAL SERVICE ROADS & TRAILS
PROJECT REIMBURSEMENT AGREEMENT

This Agreement made and entered into this _____ day of _____,
19____, by and between

Party of the First Part: State of Alaska, acting by and through
its Department of Transportation and
Public Facilities, hereinafter called
the Department.

Party of the Second Part: The _____,
hereinafter called the Local Governing
Body.

WITNESSETH:

Whereas, the local governing body desires to participate in the
Local Service Roads and Trails program and,

Whereas, the local governing body has been allocated funds
for the purpose of constructing a Local Service Road, and

Whereas, the local governing body desires that the Department
administer the project in their behalf

THEREFORE, it is mutually agreed that:

- A. The local governing body agrees to employ people for the purpose of
constructing a Local Service Road Project.
- B. The local governing body agrees to pay employees utilized in con-
structing the Local Service Road Project the following hourly
rates:

<u>Title</u>	<u>Hourly Rate</u>
_____	_____
_____	_____
_____	_____
_____	_____

- C. The local governing body agrees that pay will be for the actual hours worked on the project.
- D. The local governing body agrees that:
 - 1. The Department will supervise the people working on the project, or will assign a local foreman to supervise the project.
 - 2. The Department will determine the days, shifts, hours and number of people to be used on the project.
 - 3. The Department reserves the right to judge the ability of any locally hired person on the project and, if found unacceptable by the Department, the local governing body will provide an immediate replacement.
 - 4. The local governing body requests that adequate Workman's Compensation and Liability Insurance be arranged by or on behalf of the local governing body for the duration of the project and that the Department pay the premiums for the local governing body.
- E. The local governing body agrees that under no circumstances does this agreement bind the Department to any expenses in excess of the amount specified in the project agreement.
- F. The local governing body specifically agrees to indemnify, defend and save harmless and exonerate the Department of and from all liability, claims and demands arising out of the work undertaken by the local governing body its employees, agents, representatives; or arising out of any operation, no matter by whom performed, or on behalf, or in conjunction with work to be coordinated by the Department.
- G. The Department agrees to reimburse the local governing body for the wages of employees utilized on the Local Service Roads project and pay the premium, on behalf of the Local Governing Body, for Workmen's Compensation and Insurance Liability from the project funds.
- H. It is mutually agreed that this agreement may be terminated by either party upon written notice in forty-eight (48) hours.
- I. This agreement shall become effective on the date of its approval by the Department.

 BY: _____

STATE OF ALASKA
 DEPARTMENT OF TRANSPORTATION
 AND PUBLIC FACILITIES

BY: _____
 Deputy Commissioner
 Design & Construction

Recommended for Approval:

 Regional Engineer for Design
 and Construction

(NAME OF LOCAL GOVERNMENT)

RESOLUTION NO.

A RESOLUTION OF THE _____ AUTHORIZING THE
CONSTRUCTION OF THE _____ TO BE PAID
FOR BY THE LOCAL SERVICE ROADS AND TRAILS FUND.

WHEREAS, the _____ desires to construct a road
known as the _____
at a cost of _____, and

WHEREAS, pursuant to AS Ch. 84, Sec. 19.30.111 through Sec. 19.30.241,
the State of Alaska has a fund for construction of local service roads and
trails.

WHEREAS, the Local Government agrees to secure all royalties for
road building materials from private or public lands necessary for the
construction of this project, at no cost to the Department of Transportation
and Public Facilities if a method other than the competitive bidding process
is approved by the Commissioner of the Department.

NOW, THEREFORE, BE IT RESOLVED by the _____
that application be made to the State of Alaska for funds
from the Local Service Roads and Trails Fund to be used in construction of
the _____.

PASSED, APPROVED AND ADOPTED by the _____ of
_____ on this _____ day of _____, 19____.

NAME & TITLE

A T T E S T :

, CLERK

(NAME OF LOCAL GOVERNING BODY)

RESOLUTION NO.

A RESOLUTION OF THE _____ AUTHORIZING
THE CONSTRUCTION OF THE _____ TO BE PAID
FOR BY THE LOCAL SERVICE ROADS AND TRAILS FUND.

WHEREAS, the _____ desires to
construct a road known as the _____
at a cost of _____, and

WHEREAS, pursuant to AS Ch. 84, Sec. 19.30.111 through Sec.19.30.241,
the State of Alaska has a fund for construction of local service roads
and trails.

WHEREAS, this local governing body requests that the State of Alaska
proceed with development of roads, trails and material sites in the vicinity
of _____, as generally shown on the map
attached hereto and made a part hereof.

WHEREAS, the proposed project was reviewed and found not to be
detrimental to the welfare of the individual natives of this area if
the right of way should fall within lands withdrawn under Section 11(2),
(1) of the Alaska Native Claims Settlement Act.

WHEREAS, this local governing body has no objections to the granting
of a Right-of-way by the Bureau of Land Management Townsite Trustee for
the above named and numbered project.

WHEREAS, this local governing body has no objections to furnishing
royalties (permission to use) for materials necessary to construct this
project at no cost to the Department of Transportation and Public Facilities.

NOW, THEREFORE, BE IT RESOLVED by the _____
that application be made to the State of Alaska for funds from the Local
Service Roads and Trails Fund to be used in construction of the _____

PASSED, APPROVED AND ADOPTED by the _____
of _____ on this _____ day of _____, 19 _____

NAME AND TITLE

A T T E S T :

, CLERK

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS
AGREEMENT FOR RENTAL OF EQUIPMENT

Project Name: _____ Date: _____

Project No.: _____ Activity: _____ M.P. _____ To _____

The State of Alaska hereby enters into an agreement with:

Owner-Lessor: _____ Phone No.: _____

Address: _____ Alaska Business License No.: _____

City, State: _____ Zip Code: _____

The Department of Highways, in order to augment equipment pool operations, requires the rental of the equipment described herein:

1. The Lessee, the State of Alaska, agrees to pay for all damages caused by the negligence of its employees. The Lessor shall assume liability for damages caused by the negligence of other parties or acts of God.
2. The State of Alaska, will make all normal operating repairs to the equipment except those repairs required due to usage prior to this contract. The Lessor further warrants that he will pay for all major repairs to the said equipment.
3. The owner-lessor agrees to deliver the specified equipment to _____ and the equipment shall be picked up at _____

The owner-lessor warrants that the equipment is in good working order, and available for the full time specified in this contract.

The owner-lessor further certifies he holds an Alaska business license, and has submitted his bid using the name appearing on that current Alaska business license

He further certifies he shall comply with all applicable State and Federal Laws and Regulations.

4. **Payment of Taxes:** As a condition of performance of this contract, the contractor shall pay all Federal, State and local taxes incurred by the contractor, sub-contractor or other person or persons in the performance of this contract, and proof of payment of these taxes is a condition precedent to payment by the State under this contract.

Failure to comply with this provision releases the lessee from the rental and transportation costs.

5. The lessee will assume the responsibility for Workmans Compensation Insurance, when the leased equipment is under the operation and control, by an employee of the State of Alaska.

6. Description of Equipment: _____ With : _____ Without: fuels, oils & operator.

7. Rental Fee: \$ _____ Shall be per _____

8. Specified time: _____ To _____

Signed: For Owner-Lessor

Signed: State of Alaska
Department of Highways

By: _____

By: _____

Title: _____

Title: _____

Project _____

Date _____

Name:

Social Security No. _____

Residence Address:

Mailing Address:

The undersigned hereby acknowledges that wages received for work performed in connection with the above project is taxable income, and as such will be reported by the undersigned on the applicable income tax forms when due, the undersigned is also responsible for payment of school taxes, F.I.C.A., Employment Security, and any other taxes required by state or federal law.

Signed _____

The above named individual, known to me, did personally appear before me, and of his/her own free will, did sign this document on the date above mentioned.

Notary Public (Postmaster)

Date _____

LSR&T WORKFORCE PROFILE BY PROJECT

LSR&T PROJECT Number: _____

LOCATION: _____

Dollar Amount of Project: \$ _____

LOCAL HIRE DATA	1ST QUARTER, 19__		2ND QUARTER, 19__		3RD QUARTER, 19__		4TH QUARTER, 19__	
	Male	Female	Male	Female	Male	Female	Male	Female
ALEUT								
CAUCASIAN								
BLACK								
SPANISH SURNAME								
ASIAN AMERICAN								
AMERICAN INDIAN								
ESKIMO								
ALASKA NATIVE*								
TOTAL MINORITY								
TOTAL MALE/FEMALE								
TOTAL EMPLOYEES								
Breakdown by CRAFT								
Truck Drivers								
Laborers								
Operating Engineers								
Dollar Amt. paid for Wages on this Project(\$)								
ADH Project/Adm. Personnel								

* Includes Athabaskan, Eyak, Haida, Tlingit, Tsimshian, Tyonek, etc.

NOTE: This report should be completed by the Project Engineer and should include each individual who worked on the project during that quarter. The report should be transmitted to Hdqtrs. upon completion of the project or at the end of the fourth quarter if it is a two year project. Additional comments can be listed on the back.
cc: Local Service Roads Engineer ; EEO Coordinator,

STATE OF ALASKA

REIMBURSABLE SERVICES AGREEMENT

The _____ (Agency) hereby requests the following services
to be performed by the _____ (Agency)

PROJECT OR PROGRAM TITLE: _____

DESCRIPTION AND TERMS OF REIMBURSEMENT: _____

(Attach additional information as required)

DATE COMPLETION REQUIRED: _____

ACCOUNT CODE TO BE CHARGED: _____

ESTIMATED COST OF PROJECT OR SERVICES REQUESTED

	<u>Estimate</u>	<u>Actual</u>
Planning and engineering	\$ _____	\$ _____
Land acquisition	\$ _____	\$ _____
Construction	\$ _____	\$ _____
Supervision	\$ _____	\$ _____
Equipment	\$ _____	\$ _____
Contingency	\$ _____	\$ _____
Other services (itemize)	\$ _____	\$ _____
	\$ _____	\$ _____
TOTAL	\$ _____	\$ _____

NOTE: Requesting agency must submit an encumbrance document with this agreement.

I certify, based on the above estimate, that sufficient funds are available to pay this obligation and that the appropriation code to be charged has a sufficient balance to cover this obligation through Interagency Charges Account (line item 900).

Signed: _____
REQUESTING AGENCY AUTHORIZED
REPRESENTATIVE

Approved: _____
SERVICING AGENCY AUTHORIZED
REPRESENTATIVE

Date: _____

Date: _____

DH 45
(4-66)

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS
BRIDGE SITE SURVEY
(To Accompany Contour Map)

Project No. _____ Bridge No. _____

Name of Stream: _____ Date of Survey: _____

Location: _____ Rt. No. _____ Sta. No. _____

Bench Mark _____ Do fish use stream for spawning? _____

Drainage Area: _____ How Determined: _____

General Description of Drainage Area: _____

Any Storage Basins Upstream? _____

Elev. of High Water _____ High Ice _____ Date of High Ice _____

Location of High Water Mark _____

What caused H.W. _____

To what elevation can water be backed up without local flood damage? _____

Slope or gradient of stream: 500 ft. upstream _____ 500 ft. downstream _____

In dry stream bed, give water table elev. _____

Describe ice and drift conditions _____

Scour conditions. _____

In general, is stream cutting or filling? _____

Should structure be skewed? _____

Should channel change be made? _____

Any special treatment for approach fills? _____

Sidewalks and Utilities _____

SUBMITTED BY _____

APPROVED BY _____
(District Engineer)

DATE _____

MANAGEMENT LIST OF FORMS

NUMBER	DESCRIPTION
1.	Application for Services
2.	Bid Tab
3.	Change Order
4.	Contractor's Release
5.	Certification for Local Government Project
6.	Daily Expenditure Account
7.	Daily Report of Contract Costs
8.	Documents Sent Out
9.	Equipment Rental
10.	Extra Work Order
11.	Final Estimate
12.	Final Estimate Review Report
13.	Foreman's Daily Report
14.	Reconnaissance Check List
15.	Reconnaissance Report
16.	Right-of-Entry and Easement
17.	Truck Load Measure Record
18.	Truck Measurements
19.	Vendor Payment Authorization
20.	Village Payment Form
21.	Weekly Equipment Expenditure Account
22.	Weekly Equipment and Labor Report

APPLICATION FOR SERVICES

(date)

Name of Applicant: _____

Address: _____

The above named hereby requests the Department of Transportation and Public Facilities to perform the following services; and does affirm that private or other State and government agencies are not available to perform the services requested:

The applicant understands that reimbursement of the total cost will be made to the State by money order or check upon receipt of billing by the State. It is further understood that the minimum service charge is \$10.00 (ten dollars). Performance of the work will depend upon availability of equipment and personnel.

It is further agreed that the applicant will hold the State blameless for any damages or injuries that may occur during or after the performance of the work.

Applicant's Signature

Title

Recommended by LSR&T (date)

Approved by Regional Engineer (date)

Region

Location

Original to Finance (with G//)
1st copy to Regional Engineer
2nd copy to Local Service Roads & Trails

14 21F
(2-74)

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

CHANGE ORDER NO. _____

_____ District

Sheet 1 of _____

Project No. _____

Contractor _____

Project Name _____

Address _____

The following change(s) in the above Contract are hereby ordered, in accordance with the terms of the Contract, and under the terms and conditions stated hereon

Receipt Acknowledged:

Recommended:

Contractor _____

Issued: _____

Contractor Representative _____ Title _____

Date _____

Date _____

DESCRIPTION

14-22a
(2-74)

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

CONTINUATION SHEET _____ Sheet _____ of _____

Project No. _____

14-17A
(3-74)

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

CONTRACTOR'S RELEASE

RE: Project No. _____	Final Project Amount	\$ _____
Project Name _____	Previous Payments, Progress Estimate(s). 1 thru _____	\$ _____
_____	Final Payment Due	\$ _____

CONTRACTOR: _____

Address: _____

Pursuant to the terms of the agreement in writing under date of _____, 19____ for the construction of _____ (Project Number)

and in consideration of the sum of _____

Dollars (\$ _____) which has been or is to be paid under the said agreement to _____

(Contractor's Name and Address) _____
(hereinafter called the Contractor) or its assignees, if any, the Contractor, upon payment of the said sum by the STATE OF ALASKA, does remise, release and discharge the STATE OF ALASKA, its officers, agents and employees, of and from all liabilities, obligations, claims, and demands whatsoever under or arising from said contract, whether known or unknown and whether or not ascertainable at the time of the execution of this instrument except specified claims in stated amounts or in estimated amounts where the amounts are not susceptible of exact statement by the Contractor, as follows:

The Contractor agrees, in connection with the claims which are not released as set forth above, that he will comply with all the provisions of the said contract, including without limitation those provisions relating to notification of the Contracting Officer and relating to the prosecution of claims.

IN WITNESS WHEREOF, this release has been executed this _____ day of _____, 19_____.

WITNESS _____

Contractor _____

WITNESS _____

BY _____

TITLE _____

(NOTE: In the case of a corporation, witnesses are not required, but certificate on reverse side must be completed.)

CERTIFICATE

I _____, certify that I am the _____
(official title) of the corporation named as Contractor in the foregoing release; that _____
who signed said release on behalf of the Contractor was then _____ (official title)
of said corporation; that said release was duly signed for and in behalf of said corporation by authority of its governing body
and is within the scope of its corporate powers.

Name: _____ (Signature)

Corporate Seal

CITY AND BOROUGH OF JUNEAU
DEPARTMENT OF PUBLIC WORKS

CONTRACTOR'S REQUEST FOR PAYMENT NO. _____ ON CONTRACT NO. _____

CONTRACTOR: _____

CONTRACT TITLE: _____

Partial Pay Request for Period Ending _____

Final Pay Request (attach contract release form).

Total Work Completed to Date*	\$	_____
Materials on Hand**	(+)	_____
NET EARNED ON CONTRACT TO DATE:		_____
Less _____ % Retained	(-)	_____
SUB-TOTAL		_____

Previous Payments

1.	_____
2.	_____
3.	_____
4.	_____
5.	_____
6.	_____
7.	_____
8.	_____
9.	_____
10.	_____
11.	_____
12.	_____
13.	_____
14.	_____
15.	_____
16.	_____
17.	_____
18.	_____
19.	_____
20.	_____

EXAMPLE

Total Previous Payments	(-)	_____
BALANCE DUE	\$	_____

The undersigned hereby certify that all items and amounts on this request for payment are correct and that the work has been performed and/or material supplied in full accordance with the contract.

_____ (Date)	_____ (Signature of Contractor's Representative)	_____ (Title)
-----------------	---	------------------

_____ (Date)	_____ (Signature of Project Engineer or Architect)
-----------------	---

*Attach Form 35 or Form 36.
**Attach invoices or other documentation.

CITY & BOROUGH OF JUNEAU
DEPARTMENT OF PUBLIC WORKS

CONTRACTOR'S PAY ESTIMATE NO. _____
(Lump Sum Contracts)

CONTRACT PRICE:

Original Contract Price: \$ _____

- Change Orders (indicate + or -) 1. _____
- 2. _____
- 3. _____
- 4. _____
- 5. _____
- 6. _____
- 7. _____
- 8. _____
- 9. _____
- 10. _____

ADJUSTED CONTRACT PRICE: \$ _____

WORK TO DATE THROUGH _____ :
(date)

Work completed on Contract (____%) \$ _____

- Work completed on change orders
- 1. _____
- 2. _____
- 3. _____
- 4. _____
- 5. _____
- 6. _____
- 7. _____
- 8. _____
- 9. _____
- 10. _____

Total work completed to Date \$ _____
(enter this amount on Form #34)

EXAMPLE

RELEASE

_____, being first duly sworn and under oath does say that he is the _____ of _____ (Owner, President, etc.)

_____ and that the Final Payment of _____ (Name of Contracting Firm)

\$ _____ by the City and Borough of Juneau to the _____ (Amount of Final Payment)

Contractor will satisfy in full all claims in any way connected with contract for _____ (Contract Number and Title)

which the Contractor has or may have against the City and Borough; and he further states that _____ (Name of Contracting Firm)

has satisfied all claims and indebtedness of every nature in any way connected with the contract, including but not limited to, all payables, amounts due subcontractors, accounts for labor performed and material furnished and liens and judgements.

Dated this _____ day of _____, 19__.

Signature

Title

Subscribed and sworn to before me this _____ day of _____, 19__.

Notary Public for Alaska.
My commission expires _____

EXAMPLE

**ALASKA DEPARTMENT OF TRANSPORTATION
DAILY EXPENDITURE ACCOUNT**

State Project No:	Project:	Date:
-------------------	----------	-------

LABOR COST (STATE PERSONNEL)							
Name	Class	Hours		Rate		Amount	Subsistence
		Regular	Overtime	Regular	Overtime		
TOTAL - LABOR and SUBSISTENCE							

EQUIPMENT COSTS				MATERIAL COST			
Vehicle No. or Type	Hr./Mi.	Rate	Amount	Description	Quantity	Price	Amount
TOTAL -				TOTAL -			

SUMMARY OF COSTS		
	Dollars	Cents
TODAYS LABOR COSTS		
TODAYS EQUIPMENT COSTS		
TODAYS MATERIAL COSTS		
TODAYS CONSTRUCTION COSTS (i.e. Contract Items, F.A. Sheets)		
TOTAL COST TODAYS WORK		
TOTAL COST BROUGHT FORWARD		
TOTAL COST TO BE FORWARDED		

REMARKS: _____

PROJECT ENGINEER

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION & PUBLIC FACILITIES
AGREEMENT FOR RENTAL OF EQUIPMENT

Date _____

Project _____

Project No. _____

The State of Alaska hereby enters into an agreement with:

for rental of the equipment described herein. The lessee, the State of Alaska, agrees to pay for all damages caused by the negligence of its employees. The lessee does not assume liability for damages caused by the negligence of other parties or acts of God (such as tornado, lightning, snowstorm, flood, etc., damage).

The lessee agrees to make the normal operating repairs.

The lessor-owner warrants that the equipment is in good working order.

Failure to comply with this provision releases the lessee from the rental and transportation costs. The lessor further warrants that he will pay for all major repairs to the said equipment.

Description of Equipment:

Rental Fee:

SIGNED: State of Alaska
Department of Transportation
& Public Facilities

BY: _____

SIGNED: _____
For Lessor-Owner

TITLE: _____

(NOTE: Letter of Agreement may be substituted.)

14-21G
(2-74)

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

EXTRA WORK ORDER NO. _____

District _____

Sheet 1 of _____

Project No. _____ Contractor _____

Project Name _____ Address _____

The above designated Contract is hereby modified in the manner described below. This order is supplemental to the above Contract, which is, by reference made a part hereof. All terms, conditions and provisions of the Contract, except as specifically modified herein, remain unchanged and in full force and effect.

Acceptance of this Extra Work Order constitutes agreement to the terms, conditions, and prices stated.

ACCEPTED:

Recommended:

Contractor _____

Issued: _____

Contractor Representative _____ Title _____

Date _____

Date _____

DESCRIPTION

DH-31
4-71

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

FINAL ESTIMATE REVIEW REPORT

Highway District _____

Project No. & Name _____ Contractor _____

Description of Work _____

Required Completion Date _____ Accepted for Traffic _____

Resident Engineer _____ Project Engineer _____

Project Final Acceptance Date _____

Days Overrun _____ Liquidated Damages Assessed \$ _____

Final Amount \$ _____ Bid Amount \$ _____

Material Certification Date _____ PR 47 Date _____

Classification of Costs

Per Project Estimate

Review Findings

Participating " , , .
Non participation " , , .
Deferred participating " , , .
Liquidated Damages " , , .

Participating " , , .
Non participating " , , .
Deferred participating " , , .
Liquidated Damages " , , .

Total

Total

Per Project Estimate

Review Findings

Participating " , , .
Non participation " , , .
Deferred participating " , , .
Liquidated Damages " , , .

Participating " , , .
Non participating " , , .
Deferred participating " , , .
Liquidated Damages " , , .

Total

Total

REMARKS: _____

I certify that my reviews of this project, in accordance with SOP 0201-01 indicate that all work has been substantially completed within the terms of the contract and authorized change documents, and also indicates that federal aid funds have been protected and properly classified unless otherwise noted above.

Signature of Reviewer

Date Submitted

LOCAL SERVICE ROADS AND TRAILS
FOREMAN'S DAILY REPORT

Project No. _____ Name: _____

DESCRIPTION OF WORK PERFORMED

<u>MEN</u>				
<u>Name</u>	<u>Started Work</u>	<u>Stopped Work</u>	<u>Work Performed</u>	<u>Actual Hours Worked.</u>

<u>EQUIPMENT</u>					
<u>Type of Equipment</u>	<u>Started Work</u>	<u>Stopped Work</u>	<u>* Down or Standby Time</u>	<u>Work Performed</u>	<u>Hours Worked</u>

* Down or standby time shall be explained in detail.
Note: If additional room for remarks is needed use back of this sheet.

Foreman's Signature: _____ Date: _____

FOREMAN'S DAILY REPORT INSTRUCTIONSGENERAL:

The Foreman's Daily Report in the course of a normal day's operation should list the work being performed and by who and what equipment is involved. The report, in the event men and or equipment changed type of work, should note hours spent on each item for the day. (e.g.) If a laborer was involved with culvert installation at the beginning of the shift then worked at drilling, his operation as noted on the "Daily Report" would show, say, 2 hrs. culvert, 6 hrs. drilling. The same would hold true for equipment changing basic jobs. In the event that for several days running, (e.g.) clearing and grubbing, a laborer would be noted on the first full day of this operation that he set chokers, operated chainsaw or whatever. As long as this operation continued the Foreman could note each day that "Men & Equipment the same." The first day of each week should be detailed. In all cases the remarks area on the "Daily Report" shall be filled out showing what work was accomplished for the day. The instructions for the equipment are the same as for the men. This report should contain enough information so that the job could be followed through on a day by day basis. This will be necessary for writing up the project history after completion of the work and might possibly show pitfalls that could be avoided in future operations.

SPECIAL:

- (1) Materials should be noted as to when ordered, expected arrival date and actual arrival date. If a delay is encountered it should be noted and what action was required if any.
- (2) The same holds true for Equipment.
- (3) Unforeseen problems encountered should be described in detail. (e.g.) Inclement weather, major equipment breakdowns, anything that might slow down or stop the work for an appreciable length of time.
- (4) State or Federal visitors, or legislatures or any one of note that might visit the project. If possible get their name, organization represented and any comments pertaining to the work.
- (5) Anything related to the project that might be of interest that might not be normally noted.

LSR&T

RECONNAISSANCE CHECK LIST

Date: _____

Attended by: _____

1. Project Description: (length, typical section, plan and location.)

2. Estimated Cost:

3. R.O.W. Clear - Letter of Certification: _____

4. Any utilities requiring relocation? _____

5. Photos.

6. Costs incurred prior to agreement are not reimburseable: _____

7. Local Government Participation.

1. Administration. _____
2. P.E. _____
3. Labor _____
4. Equipment _____
5. Material _____
6. Money _____
7. R.O.W. _____
8. Other _____

8. Pay Scales Used _____

9. Equipment Rental Rates: _____

10. Employment card if governing body not making deductions for payroll: _____

11. Equipment Rental Agreement to be completed prior to starting work: _____

12. Separate project checking account required to be countersigned by LSR&T Engineer: _____

13. Bank name and location: _____

14. Governing body signatures for account.

- 15. Local foreman available? _____

- 16. Daily and weekly report forms to be furnished by Department. _____

- 17. Maintenance by _____

- 18. Airport clearance required? _____

- 19. Historical Preservation Act: _____

- 20. Structures: _____
- 21. Materials: _____
- 22. Survey Required: _____
By whom: _____

District: _____ Name of Route _____

Termini: From _____ To _____ Length of Project _____

(INCLUDE MAP SHOWING PROJECT LIMITS)

Type of service to be provided: _____

Describe Existing Facility: _____

Proposed Facility: _____

Present System: _____ No. of persons served: _____

Location of materials sources: _____

Estimated Costs

P.E.	R/W	Construction	Constr. Engineering	Total

Type of R/W to be acquired: _____

Name of Head of Local Government: _____

Remarks (possible causes of delay, e.g. R/W, land claims, materials, etc.)

Construction Site Access: _____

WHEREAS, the STATE OF ALASKA, is willing to construct a Boardwalk and Foot Bridges through certain properties described below and the various owners and/or occupants are in agreement with the State's desire to construct said Boardwalk and Foot Bridges;

NOW THEREFORE, in consideration of the mutual benefits to be derived therefrom, the undersigned hereinafter called the Grantor states that he is the owner of said premises and is legally entitled to grant this right of entry and easement to the State of Alaska hereinafter called the Grantee, and by this instrument does herewith grant the State of Alaska the right to enter upon the following described real property for the purposes of constructing said Boardwalk and Foot Bridges over, through and across said lands, said property being situated in the Iliamna Recording Precinct and more particularly described as follows:

A fifty foot strip along the westerly edge of U.S. Survey 5577 and generally following the existing trail.

EXAMPLE

It is agreed by the Grantee herein, that said road will be constructed along that certain alignment previously agreed upon, insofar as it is possible and that at a later date when such alignment had been ascertained the Grantor herein agrees to execute an easement in perpetuity to the Grantee establishing the route and width of said road.

Signed this _____ day of _____, 19_____

Grantor

ACKNOWLEDGMENT OF GRANTOR__

STATE OF ALASKA)
) ss.
)

ON THIS _____ day of _____, 19____, before me, the undersigned, a Notary Public in and for the said State, personally appeared _____, the Grantor, known to me to be the identical person who executed the foregoing instrument and _____ he acknowledged to me that he signed the same as _____ free voluntary act and deed, with full knowledge of its contents, for the uses and purposes therein mentioned.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year above written.

My Commission Expires: _____
Notary Public

The Department of Transportation and Public Facilities hereby accepts the above easement on behalf of the STATE OF ALASKA.

Region Right of Way and
Land Acquisition Agent
For the Commissioner

Project Pedro Bay
Parcel USS 5577

ALASKA DEPARTMENT OF HIGHWAYS No 01
TRUCK MEASUREMENTS

PROJECT NO. _____ DATE _____

MEASURED BY:

MEASUREMENTS
AGREED TO BY:

Contractor's Representative

STATE OF ALASKA

DEPARTMENT OF TRANSPORTATION
and PUBLIC FACILITIES

JAY S. HAMMOND, GOVERNOR

CENTRAL REGION

4111 A. 10th AVENUE, POUCH 6900
ANCHORAGE 99502 (TELEX 25-103)

Date: _____

TO: State of Alaska
Department of Transportation
Division of Highways
Local Service Roads & Trails
Pouch 6900
Anchorage, Alaska 99502

Pay to _____, vendor, the amount
of \$ _____ for the items as stated below:

Vendor: _____

Title: _____

Approved for Payment: _____
Project Engineer
Department of Transportation

PROJECT: _____

Activity: _____

STATE OF ALASKA DEPARTMENT OF TRANSPORTATION LSR&T
WEEKLY EQUIPMENT EXPENDITURE ACCOUNT

PROJECT NAME _____ PROJECT NO. _____
WEEK FROM _____ THRU _____ HOURLY RATE _____

DATE		MON	TUE	WED	THU	FRI	SAT	SUN	WEEK TOTAL
NAME									
	HRS								
	AMOUNT								
	HRS								
	AMOUNT								
	HRS								
	AMOUNT								
	HRS								
	AMOUNT								
	HRS								
	AMOUNT								
	HRS								
	AMOUNT								
	HRS								
	AMOUNT								
	HRS								
	AMOUNT								
DAILY HOURS									
DAILY AMOUNT									

REMARKS _____

PROJECT ENGINEER

WEEKLY REPORT FORM

Location: _____

Project: _____

Date: _____

LABOR: Name	Job	Hours		Rate		Extensions	COST
		Reg.	O.T.	Reg.	O.T.		
							\$
Sub-Total:							\$

EQUIPMENT:	Hours	Rate	COST
Sub-Total:			\$

MATERIALS:	Unit	Cost/Unit	Invoice No.	COST
Sub-Total				\$

Computed By: _____
 Title: _____
 Date: _____

SHEET TOTAL \$ _____
 PREVIOUS TOTAL \$ _____
 TOTAL TO DATE \$ _____

NOTE: Foreman's Daily Report should be used for Backup.